



## STAFF REPORT TO THE ANTIOCH PLANNING COMMISSION

**DATE:** Regular Meeting of August 20, 2025

**SUBMITTED BY:** Kevin Valente, Contract Planner,  
Raney Planning and Management, Inc.

**APPROVED BY:** Zoe Merideth, Planning Manager

**SUBJECT:** Wildflower Station Townhomes 2 Multifamily Residential Project  
(TM-02 and AR-23-05)

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### **REQUESTS**

The project applicant, Kathryn Watt, with DeNova Homes Inc, is seeking approval of a Vesting Tentative Subdivision Map and Design Review for the Wildflower Townhomes Project (proposed project). Necessary entitlements from the City of Antioch include the following:

1. *Vesting Tentative Subdivision Map.* The proposed project requires approval of a Vesting Tentative Subdivision Map for condominium purposes that would subdivide the project site for the development of 19 townhome buildings totaling 159 residential units.
2. *Design Review.* The proposed project requires Design Review approval of the project's architecture, design, and landscaping.

### **STAFF RECOMMENDATION**

Staff recommends that the Planning Commission:

Adopt the resolution recommending City Council approval of a Vesting Tentative Subdivision Map and Design Review for the development of the Wildflower Station Townhomes 2 Multifamily Residential Project.

### **SITE LOCATION**

The proposed 10.35-acre vacant project site, identified by Assessor's Parcel Numbers (APNs) 052-140-013, -014, -015, and -016, is located east of Hillcrest Avenue and west of Wildflower Station Place. Surrounding existing uses include commercial uses to the north; townhomes to the east across the Wildflower Station Place roadway; commercial uses within a shopping center to the south, beyond Hillcrest Avenue; and single-family residences to the west, across Hillcrest Avenue. The project site is designated High Density Residential (HDR) in the Antioch general Plan and the site is zoned High Density Residential District (R-25).



A map of the San Francisco Bay Area and surrounding regions. The map shows major cities, highways, and geographical features. A red star is placed near Antioch, CA, with a black arrow pointing to it from a white box labeled "Project Location". The map includes labels for various locations such as San Francisco, Berkeley, Oakland, Alameda, Contra Costa, and Sacramento counties. Major highways like I-80, I-580, and I-680 are shown. The project location is situated in the eastern part of the Bay Area, near the border of Contra Costa and Sacramento counties.

This aerial map illustrates the Hillcrest area in San Diego, with the Project Site highlighted in red. The map is divided into several labeled zones: **Commercial Businesses** at the top center, **Single-Family Residences** in the upper left and middle right, **Townhomes** adjacent to the project site, a **Shopping Center** in the lower left, and **Vacant Land** in the lower right. Key streets shown include Hillcrest Ave, Hillcrest Rd, Hillcrest Terrace Ct, Hillcrest Dr, Hillcrest Ct, Hillcrest Pl, Hillcrest Ln, Hillcrest Way, Hillcrest Blvd, Hillcrest Pkwy, Hillcrest Hwy, Hillcrest Expy, Hillcrest Fwy, Hillcrest Bypass, Hillcrest Loop, Hillcrest Turnpike, Hillcrest Freeway, Hillcrest Expressway, Hillcrest Interchange, Hillcrest Junction, Hillcrest Roundabout, Hillcrest Overpass, Hillcrest Underpass, Hillcrest Tunnel, Hillcrest Bridge, Hillcrest Viaduct, Hillcrest Causeway, Hillcrest Ramp, Hillcrest On-ramp, Hillcrest Off-ramp, Hillcrest Exit, Hillcrest Entrance, Hillcrest Exit Ramp, Hillcrest Entrance Ramp, Hillcrest Exit Overpass, Hillcrest Entrance Overpass, Hillcrest Exit Underpass, Hillcrest Entrance Underpass, Hillcrest Exit Tunnel, Hillcrest Entrance Tunnel, Hillcrest Exit Bridge, Hillcrest Entrance Bridge, Hillcrest Exit Viaduct, Hillcrest Entrance Viaduct, Hillcrest Exit Causeway, Hillcrest Entrance Causeway, Hillcrest Exit Ramp, Hillcrest Entrance Ramp, Hillcrest Exit Overpass, Hillcrest Entrance Overpass, Hillcrest Exit Underpass, Hillcrest Entrance Underpass, Hillcrest Exit Tunnel, Hillcrest Entrance Tunnel, Hillcrest Exit Bridge, Hillcrest Entrance Bridge, Hillcrest Exit Viaduct, Hillcrest Entrance Viaduct, Hillcrest Exit Causeway, Hillcrest Entrance Causeway.



## **Background**

The project site was part of the larger 23-acre Wildflower Station project that includes single family homes, condominiums, and planned commercial development that was entitled in 2018. The single-family homes and townhomes were constructed, but the commercial parcels along Hillcrest Avenue (the current project site) were not constructed and the land sat undeveloped.

As part of the City's Sixth Cycle Housing Element Update (2023-2031) process, the City identified the proposed 10.35-acre project site for future multi-family housing, and therefore, amended the General Plan land use designation for the site from Mixed Use to HDR and rezoned the project site from Planned Development (P-D) to R-25 in January 2023.

In February 2023, the City of Antioch certified the Antioch Housing, Environmental Hazards, and Environmental Justice Elements Project Draft Environmental Impact Report (SCH No. 2021110146), hereafter referred to as the "Housing Element EIR."

The applicant submitted a Preliminary Application under SB 330 (The Housing Crisis Act of 2019). Through the Preliminary Application process put in place under SB 330, housing developments may only be subject to the ordinances and objective standards in effect at the time when a completed Preliminary Application is submitted. The Preliminary Application requires information regarding site characteristics, planned project, certain environmental concerns, the locations of recorded public easements, facts related to any potential density bonuses, certain coastal zone concerns, and the number of units to be demolished. The developer has 180 days from the submittal of the preliminary application to submit the full development application. The applicant submitted a complete Preliminary Application on April 25, 2023. The applicant submitted an entitlement application on April 25, 2023 as well.

At the July 16, 2025 Planning Commission meeting, staff requested that the project be continued to regular meeting of August 20, 2025. The Planning Commission opened the public hearing and one member of the public spoke on behalf of the Wildflower Station Place Homeowners Association (HOA) about the security concerns at the existing Wildflower Station Place condominium project, appreciation for DeNova for incorporating resident feedback into the proposal, and the HOA's acceptance of the offer to contribute their fair share towards maintenance of fencing and gates. The Planning Commission then voted 6-0, with one member absent, to continue the item to the August 20, 2025 meeting. The meeting minutes are included as Attachment F.

## **ANALYSIS**

The proposed project includes the development of 19 townhome buildings totaling 159 residential units on a 10.35-acre property. The proposed project would include a total of 2.8 acres of landscaping and open space, consisting of two open play areas and open space for bioretention. Each residential unit would include either a single-car or two-car



garage, and the two centrally located play areas would include 25 additional vehicle parking spaces with another 57 surplus shared parking spaces with the adjacent Wildflower Station development. The 159 residential units would consist of a mix of two- and three-bedrooms units, ranging in size from 1,135 to 1,381 square feet.

#### Consistency with the General Plan, Specific Plan, and Zoning

As described above, as part of the City's Sixth Cycle Housing Element Update, the 10.35-acre project site was designated HDR and rezoned from P-D to R-25 and identified for future multi-family housing. Consistent with the proposed project, the City's HDR land use designation is intended for multi-family dwellings.

The R-25 High Density Residential zoning district allows for multiple-family development at a minimum of 20 dwelling units per acre (du/ac) and a maximum of 25 du/ac. Consistent with the R-25 district, the proposed project has a proposed net density of 20.05 du/ac. In addition, the proposed project includes proposed building heights that range from 37 feet to 41 feet, consistent with the maximum building height of 45 feet for the R-25 district regulations. The proposed building setbacks, as well as the open space and parking requirements for the project, are also consistent with the R-25 development standards.

#### Vesting Tentative Subdivision Map

The proposed Vesting Tentative Subdivision Map would subdivide the project site for the development of 19 townhome buildings, the associated internal roadway network, and landscaping. The proposed map is for condominium purposes and would allow for the sale of each residential unit. Each townhome building would contain between eight (8) and ten (10) units, for a total of 159 residential units. Additionally, 17 separate parcels are proposed for the internal roadway network (Parcels A-K and M-Q), as well as common areas (Parcels J-L). Parcel I would be located along the northern project site boundary and contain a bioretention area.

Primary access to the project site would be provided by three driveways from Wildflower Station Place. Automatic gates are proposed as an option at the entrances, pending approval from the current homeowners on Wildflower Station Place, and their homeowner's association (HOA). All internal roadways would be privately owned and maintained by the HOA. Pedestrian access consisting of stairs and an accessible ramp would be provided along Hillcrest Avenue between buildings 10 and 11.

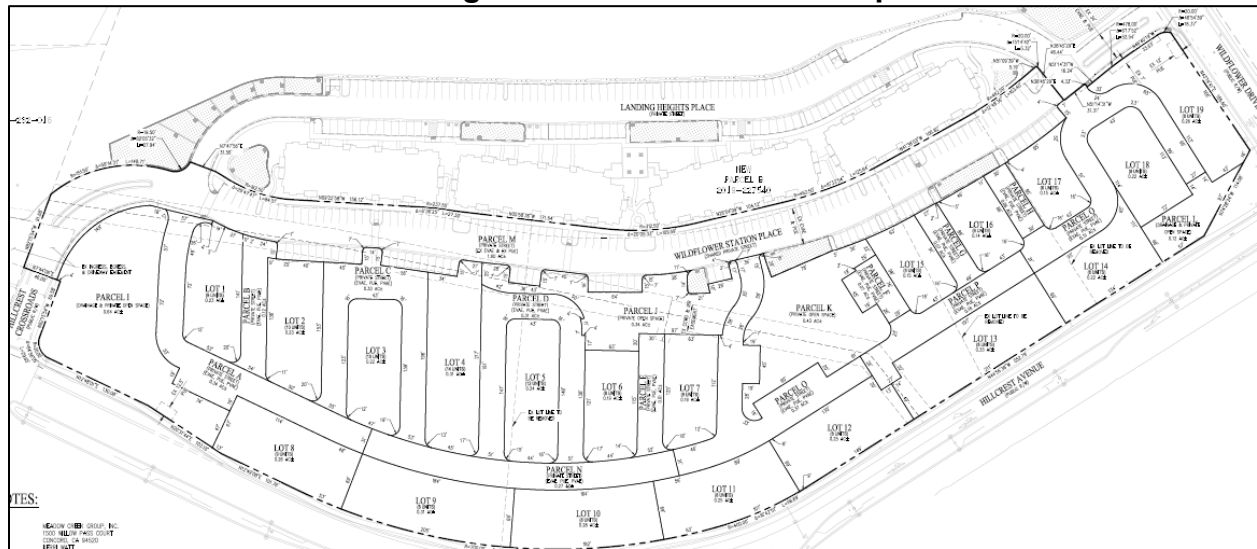
Existing utilities located in Hillcrest Avenue, Wildflower Station Place, and Wildflower Drive are adequately sized to serve the proposed development. As required by the City, all in-tract utilities (domestic water, water for fire service, sanitary sewer, storm drain, and bio-retention facilities) would be privately owned and maintained by the HOA. Eight-inch water lines would extend into the project site from Wildflower Station Place and from Hillcrest Avenue. Eight-inch sanitary sewer lines would also extend from the same connections to the existing utility lines in the Wildflower Station Place and Hillcrest Avenue roadways. In addition, eight-inch stormwater connections would be extended into



the project site from the surrounding roadways and would lead to the bio-retention facilities located at the northern area of the project site.

It should be noted that proposed street names also require Planning Commission approval. The proposed street names have been reviewed by Engineering staff and the Antioch Police Department. The proposed street names are included as a recommended condition of approval in Attachment A.

### Vesting Tentative Subdivision Map



### Design Review

Pursuant to the Antioch Municipal Code Section 9-5.2607(A)(1), proposed development of any new building or construction in the R-25 zoning district is subject to Design Review. The City of Antioch adopted citywide Multi-family Residential Objective Design Standards relating to all aspects of multi-family residential and mixed-use development on sites zoned for multi-family housing in the City of Antioch. The Multi-family Residential Objective Design Standards (ODS) are used to review projects including their building siting, architectural style, parking, and landscaping.

Each proposed townhome building would be three stories (approximately 37 to 41 feet tall), as measured to the roof ridge, consisting of either "Row Townhomes" units (63 units) or "Back to Back" (B2B) units (96 units). The eight Row Townhome buildings would be located along Hillcrest Avenue with units within each building designed in a side-by-side layout. The eleven B2B buildings would be sited generally in the central portion of the project site along Wildflower Station Place, with units within each building designed in a side-by-side and back-to-back layout. Both unit styles would include living areas primarily on the second and third levels. All units would have at least one 78-square-foot private deck, a storage area of at least 250 cubic feet, and an attached garage.





REAR ELEVATION



LEFT ELEVATION



RIGHT ELEVATION



FRONT ELEVATION

ROW TOWNHOMES





The Row Townhome buildings include entryways with covered porches at the front elevations and garages at the rear elevations. The B2B buildings include entryways with covered porches and garages at both the front and rear elevations. Each building would have eight (8) and ten (10) units. Five Row Townhome floorplan types would be offered with two-bedrooms and either a single-car garage or a two-car garage, ranging in size from 1,135 to 1,238 square feet. Two B2B floorplan types would be offered with two- and three-bedrooms and a two-car garage, ranging in size from 1,293 to 1,381 square feet.

The architectural style of the proposed residential buildings would be contemporary in design and would make use of stucco and siding finishes in several different colors on each building. The front façades of all buildings would be articulated to emphasize front entries and decks, while the sides and rears would include varying vertical and horizontal planes, and different finishes to provide articulation. The proposed residential buildings



complies with the articulation and planning requirements found in the ODS, including in section 2.2.1 Building Massing and Articulation.

The proposed project provides centrally located common open space areas with amenities consisting of seating, picnic tables, a large shade structure, a play area for children, and a multi-purpose turf area. Such amenities would serve as recreation areas for future residents, while making use of the otherwise unbuildable areas within the Contra Costa County Flood Control and Water Conservation District pipelines easement. Mailboxes and areas for bicycle parking would be located to the west of the common open space. The ODS 2.1.7.A requires a minimum of 200 square feet of usable open space per unit, with 50% of this provided as common open space and the remaining 50% provided as either private or common open space. The project exceeds these requirements by providing 385 square feet per unit, through a mix of private and common open space. The ODS requires a play area be provided because the project is more than 15 units of at least one bedroom. The applicant is meeting this requirement.

Street trees in the parkway strips would be located along all streets in addition to shrubs for privacy. The planting palette includes flowering trees such as the western redbud and the crape myrtle. Shrubs include dwarf bottlebrush, fortnight lily, New Zealand flax, and sage. Including the bioretention area in the northern portion of the site, the open space areas total approximately 32 percent of the project site, exceeding the City's 25 percent landscaping requirement.

On October 24, 2024 the Parks and Recreation Commission reviewed the park in-lieu fee payment as required by Antioch Municipal Code Section Title 9, Chapter 4, Article 10: Regulations for the Dedication of Land, the Payment of Fees, or Both, for Park and Recreation Lands, which requires either a dedication of land or payment of an in lieu fee for park and recreation lands. The ordinance requires the payment of in lieu fees based on the type and number of housing units in a project. The Parks and Recreation Commission recommended a payment of \$151,050, consistent with the ordinance. This fee has been added as a recommended condition of approval. The meeting minutes are included as Attachment B.

### Public Comments

The project application was routed to City Departments and outside agencies for review. Comment letters have been provided by City Engineering staff and the Contra Costa County Fire Protection District. These comments were incorporated into the proposed project where applicable or have been included as draft conditions of approval, which would be implemented as part of the improvement plan prior to building permit approval. The comment letters received from outside agencies are included as Attachment C. Staff also received a number of comment letters from residents in the Wildflower Station condominiums requesting this project provide gated access. These comments are included as Attachment D. As discussed above, the project plans show this as a possibility, but ultimately the HOAs will need to approve this.



## Traffic Impact Analysis

In compliance with the Antioch General Plan and the Contra Costa County Transportation Authority and TRANSPLAN, a Transportation Impact Analysis (TIA) was prepared for the project. The TIA is available here: <https://www.antiochca.gov/fc/community-development/planning/23-12-05-Wildflower-Townhomes-2-Project-TIA.pdf>

The goal of the City of Antioch and TRANSPLAN is to maintain Level of Service (LOS) of D or better at all intersections on Routes of Regional Significance in the project study area. The East County Action Plan establishes LOS D as the standard for Hillcrest Avenue and Deer Valley Road, which are identified as Routes of Regional Significance. Level of Service is a scale describes traffic flow with six ratings ranging from A to F, with “A” indicating relatively free flow of traffic and “F” indicating stop-and-go traffic characterized by traffic jams. The TIA found that all project study intersections would continue to operate at acceptable parameters and there would be no significant traffic or transportation impacts, and no off-site transportation improvements required.

## **ENVIRONMENTAL REVIEW**

As described above, the project site was included in the City’s Sixth Cycle Housing Element Update (2023-2031) and identified for future multi-family development. In February of 2023, the City of Antioch adopted the Housing Element EIR (SCH No. 2021110146), which analyzed adoption and implementation of the City’s Sixth Cycle Housing Element Update (2023-2031), including the adoption and implementation of rezoning and General Plan amendments to accommodate the City’s Regional Housing Needs Allocation (RHNA). Therefore, the proposed project has been adequately analyzed in the previous environmental review under the California Environmental Quality Act (CEQA) and further evaluation is not required. Per CEQA Guidelines Section 15183, a project that is consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified shall not require additional environmental review, except when it is necessary to examine whether there are project-specific significant effects which are peculiar to the project or its site.

Therefore, a Section 15183 Consistency Memorandum was completed for the proposed project to demonstrate that the project has been adequately analyzed in the previous environmental review under CEQA and that further evaluation is not required. Per the CEQA Guidelines, a project that is consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified shall not require additional environmental review, except when it is necessary to examine whether there are project-specific significant effects which are peculiar to the project or its site. This streamlines the review of such projects and reduces the need to prepare repetitive environmental studies.

The Wildflower Townhomes 15183 Consistency Memorandum indicates whether the proposed project would result in a significant impact that: (1) is peculiar to the project or the project site; (2) was not identified as a significant effect in the Housing Element EIR; or (3) are previously identified significant effects, which as a result of substantial new



information that was not known at the time that the Housing Element EIR was certified, are determined to have a more severe adverse impact than discussed in the Housing Element EIR.

The Housing Element EIR anticipated the project site would be developed in accordance with the standards established for the R-25 zoning district, which allows for a density of 20 to 25 du/ac. The proposed project would result in a density of approximately 20 du/ac, and therefore, be consistent with the development anticipated for the project site within the Housing Element EIR. It should be noted that the Housing Element EIR did not identify any significant impacts and associated mitigation measures. Therefore, the Housing Element EIR does not include any additional mitigation measures that would be applicable to the proposed project. Thus, with respect to the foregoing issue areas, the proposed project would result in similar impacts as those identified within the Housing Element EIR. Compliance with applicable federal, State, and local policies, regulations, and standards would ensure impacts related to the aforementioned issue areas would be reduced to a less-than-significant level.

On July 15, 2025, the City of Antioch received one comment letter on the proposed project that included comments regarding the 15183 Consistency Memorandum. The comment letter was received from Adams Broadwell Joseph & Cardozo on behalf of the Contra Costa Residents for Responsible Development. The comments were related to Air Quality and Greenhouse Gas Emissions, Noise, and Vehicle Miles Traveled (VMT). Minor revisions have been made to the 15183 Consistency Memorandum to address the comments received. None of the revisions result in new impacts or change the conclusions in the 15183 Consistency Memorandum. A condition of approval specifically requiring a Noise Mitigation Plan in conformance with General Plan Policy 11.8.2(o) has been added for clarity, even though compliance with this General Plan Policy was already required. Additionally, as described in the response to comments, a condition of approval requiring compliance with the Housing Element EIR Mitigation Measure AIR-3a has also been added. The comment letter and the detailed responses are included as Attachment E.

## **ATTACHMENTS**

- A.** Resolution recommending City Council approval of a Vesting Tentative Subdivision Map and Design Review to allow for the development of the Wildflower Station Townhomes 2 Multifamily Residential Project.
  - Exhibit A – Wildflower Townhomes 15183 Consistency Memorandum
  - Exhibit B – Vesting Tentative Subdivision Map and Proposed Architectural Plans
  - Exhibit C – Draft Conditions of Approval
- B.** Parks and Recreation Meeting Minutes from October 24, 2024
- C.** Agency Comments Received
- D.** Public Comments Received
- E.** Response to Comments
- F.** Draft Planning Commission Meeting Minutes of July 16, 2025



## **ATTACHMENT "A"**

### **PLANNING COMMISSION RESOLUTION NO. 2025-XX**

#### **RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ANTIOCH FORWARDING A RECOMMENDATION TO THE CITY COUNCIL TO ADOPT THE PROPOSED VESTING TENTATIVE SUBDIVISION MAP (TM-02) AND DESIGN REVIEW (AR-23-05) FOR THE DEVELOPMENT OF THE WILDFLOWER STATION 2 TOWNHOMES MULTIFAMILY RESIDENTIAL PROJECT**

**WHEREAS**, the City of Antioch ("City") received an application from DeNova Homes, Inc. ("Applicant") seeking City approval of a Vesting Tentative Subdivision Map and Design Review for the development of the Wildflower Townhomes Project ("Project"); and

**WHEREAS**, the Project site is in the northeastern section of the City of Antioch, on the eastern side of Hillcrest Avenue and west of Wildflower Station Place (APNs 052-140-013, -014, -015, and -016); and

**WHEREAS**, the Project consists of a multi-family project on approximately 10.35 acres consisting of 19 three-story buildings totaling 159 residential units; and

**WHEREAS**, in February 2023, the City of Antioch adopted the 6th Cycle Housing Element Update (2023-2031), which identified the project site as multi-family development; and

**WHEREAS**, the City, as lead agency under the California Environmental Quality Act ("CEQA"), certified the Antioch Housing, Environmental Hazards, and Environmental Justice Elements Project Final Environmental Impact Report (SCH No. 2021110146); and

**WHEREAS**, the City, as lead agency pursuant to CEQA, has prepared a Section 15183 Consistency Memorandum (attached hereto as Exhibit A) to demonstrate that the proposed project has been adequately analyzed in the previous environmental review under CEQA and that further evaluation is not required; and

**WHEREAS**, the proposed project requires approval of a Vesting Tentative Subdivision Map (attached hereto as Exhibit B) for condominium purposes and would; and

**WHEREAS**, the proposed project requires approval of Design Review for the development of the proposed 19 three-story buildings totaling 159 residential units; and

**WHEREAS**, the Planning Commission duly gave notice of a public hearing as required by law; and

**WHEREAS**, On July 16, 2025, the Antioch Planning Commission continued the public hearing to a date certain, August 20, 2025; and



**WHEREAS**, on August 20, 2025, the Antioch Planning Commission duly held a public hearing on the matter, received presentation by City staff, and considered evidence, both oral and documentary, and all other pertinent documents regarding the proposed request.

**NOW, THEREFORE, BE IT RESOLVED AND DETERMINED**, that the Antioch Planning Commission does hereby make the following findings for recommending City Council approval of the Vesting Tentative Subdivision Map, as conditioned:

1. That the subdivision, design and improvements are consistent with the General Plan, as required by Section 66473.5 of the Subdivision Map Act and the City's Subdivision Regulations. The site has a General Plan Land Use Designation of High Density Residential (HDR) and is zoned High Density Residential (R-25) and the subdivision will accommodate uses that are consistent with the General Plan on each of the lots created by the subdivision; and,
2. That the subdivision proposed by the Vesting Tentative Subdivision Map complies with the rules, regulations, standards and criteria of the City's Subdivision Regulations. The proposed subdivision meets the City's criteria for the map. The City's Planning and Engineering staff have reviewed the Vesting Tentative Subdivision Map and evaluated the effects of the subdivision proposed and have determined that the Vesting Tentative Subdivision Map, as conditioned, complies with and conform to all the applicable rules, regulations, standards, and criteria of the City's Subdivision Regulations.
3. The conditions of approval protect the public safety, health and general welfare of the users of the project and surrounding area. In addition, the conditions ensure the project is consistent with City standards.

**NOW, THEREFORE, BE IT RESOLVED AND DETERMINED**, that the Planning Commission has determined the proposed project is in compliance with the City's adopted Multi-Family Residential Objective Design Standards relating to all aspects of multi-family residential and mixed-use development.

**NOW THEREFORE BE IT FURTHER RESOLVED** that the Antioch Planning Commission does hereby recommend the Antioch City Council **APPROVE** the proposed Vesting Tentative Subdivision Map (TM-02) and Design Review (AR-23-05), for the Wildflower Townhomes Project, subject to the conditions of approval attached hereto as Exhibit C.



\* \* \* \* \*

**I HEREBY CERTIFY** that the foregoing resolution was adopted by the Planning Commission of the City of Antioch at a regular meeting thereof held on the 20th day of August 2025, by the following vote:

**AYES:**

**NOES:**

**ABSTAIN:**

**ABSENT:**

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**DAVID A. STORER, AICP**  
**SECRETARY TO THE PLANNING COMMISSION**



**CITY OF ANTIOCH  
COMMUNITY DEVELOPMENT DEPARTMENT**

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**Wildflower Townhomes**

**Section 15183 Consistency Memorandum**

**March 2024**

Prepared by



1501 Sports Drive, Suite A, Sacramento, CA 95834



## **A. INTRODUCTION AND SUMMARY**

The purpose of this Memorandum is to demonstrate that the Wildflower Townhomes Project (proposed project) has been adequately analyzed in the previous environmental review under the California Environmental Quality Act (CEQA) and that further evaluation is not required. As will be demonstrated below, consistent with CEQA Guidelines Section 15183, additional environmental review is not required.

## **B. PROJECT BACKGROUND**

In February 2023, the City of Antioch adopted the Antioch Housing, Environmental Hazards, and Environmental Justice Draft Environmental Impact Report (SCH# 2021110146), hereafter referred to as the “Housing Element EIR.” The Housing Element EIR was prepared pursuant to Title 14, Section 15070 of the California Code of Regulations.

The Housing Element EIR analyzed adoption and implementation of the City’s 6th Cycle Housing Element Update (2023-2031), including the adoption and implementation of rezoning and General Plan amendments to accommodate the City’s Regional Housing Needs Allocation (RHNA). An RHNA obligation represents the total number of housing units that must accommodate the housing needs of all residents during the eight-year planning period. RHNA obligation numbers are determined by a methodology established by the State of California’s Department of Finance (DOF) and Housing and Community Development (HCD) Department. RHNA obligation numbers are ascribed to each region of the State and further allocated to local communities by the designated regional planning entity for each region.

The City of Antioch’s “fair share” of this RHNA obligation is 3,016 units, as determined by the Association of Bay Area Governments (ABAG). The Housing Element demonstrates that the City has capacity to accommodate 1,559 housing units beyond its RHNA obligation of 3,016 housing units, for a total of 4,575 units. The Housing Element also includes a compilation of sites suitable for residential development, which are comprised of 182 sites totaling 230 acres. Of these 182 sites, 125 (69 percent) are non-vacant and under-utilized, and 57 (31 percent) are vacant. The aforementioned sites are anticipated to accommodate the potential future development of up to 4,575 residential units.

The Housing Element EIR also analyzed the City’s updates to the Environmental Hazards Element of its General Plan, as well as the development and adoption of an Environmental Justice Element. The Environmental Hazards Element is meant to implement policies that minimize the negative impacts and risks of natural and man-made hazards such as fires, floods, droughts, earthquakes, landslides, climate change vulnerability, adaptation, and resiliency. Pursuant to Senate Bill (SB) 1000, an Environmental Justice Element is intended to reduce the unique or compounded health risks experienced by disadvantaged communities, to encourage civic engagement in the public decision-making process within disadvantaged populations, and to prioritize improvements and programs that benefit disadvantaged populations. “Disadvantaged communities” are defined as a low-income area that is disproportionately affected by environmental pollution and other hazards that can lead to negative health effects, exposure, or environmental degradation. Additionally, “low-income area” is defined as an area with household incomes at or below 80 percent of the statewide median income (\$109,600 for a household of 4) or with household incomes at or below the threshold designated as low income by HCD’s list of State-adopted income limits.



## **C. PROJECT DESCRIPTION**

The following provides a description of the project site's current location and setting, as well as the proposed project components and the discretionary actions required for the project.

### **Project Location and Setting**

The project site is located east of Hillcrest Avenue and west of Wildflower Station Place in the City of Antioch, California (see Figure 1). The 10.35-acre project site, identified by Assessor's Parcel Numbers (APN) 052-140-013, -014, -015, and -016, is undeveloped (see Figure 2). The project site is comprised of ruderal grasses that are regularly mowed. Surrounding existing uses include commercial uses to the north; townhomes to the east across the Wildflower Station Place roadway; commercial uses within a shopping center to the south, beyond Hillcrest Avenue; and single-family residences to the west, across Hillcrest Avenue. A portion of Wildflower Drive borders the southern boundary of the project site. The project site is designated as High Density Residential by the Antioch General Plan, and the site is zoned High Density Residential (R-25).

It should be noted that the Wildflower Station Place drive aisle, as well as parking spaces and bio-retention facilities, are located within the project site boundaries. An Emergency Vehicle Access (EVA) and shared parking easement allow all current residents of the townhomes east of the project site to traverse Wildflower Station Place and use the parking spaces. The joint usable area of 1.90 acres has been netted out of the overall 10.35 gross acres of the site, given that the easement precludes any development of the private street and parking areas. Without the 1.90-acre area, the acreage of the project site totals 8.45 acres.

The site is traversed by several utility easements within the northern portion of A Street (see Figure 3) and are intended to be used for the benefit of the proposed project. The easements (storm drain, sanitary sewer, water, and fire service water) dictate the placement of buildings, certain amenities, and trees because such development may not be constructed or placed within the easement. The primary 100-foot-wide easement, located between Lots 12 and 13 and extending north through the site, includes one large water distribution line and one large irrigation distribution line owned and maintained by Contra Costa Water District (CCWD). Given that structures or trees may not be placed within the easement, the 0.5-acre portion of the easement area not located in a street could also be subtracted from the site's gross acreage, bringing the new total to 7.95 acres.

### **Project Components**

The project includes approval of a Vesting Tentative Subdivision Map for condominium purposes and Design Review. The project components are discussed in further detail below.

### **Vesting Tentative Subdivision Map**

The proposed Vesting Tentative Subdivision Map would divide the project site into 19 lots for condominium purposes (see Figure 4). Each lot would contain one townhome building, and associated sidewalks and landscaping, for a total of 159 units and a density of approximately 20.05 dwelling units per acre (du/ac). Separate parcels are proposed for the internal roadway network (Parcels A-K, M-Q) and private common areas (Parcels J-L). Finally, Parcel I would be located along the northern project site boundary and contain a bio-retention area.

Primary access to the project site would be provided by two new entrances: one from Wildflower Drive at the southern end of the project site onto Wildflower Station Place, and one from Hillcrest Avenue at the northeast corner of the site to the private street Hillcrest Commons.



**Figure 1**  
**Regional Vicinity**





## Project Site Boundaries





Figure 3  
Preliminary Utility Plan

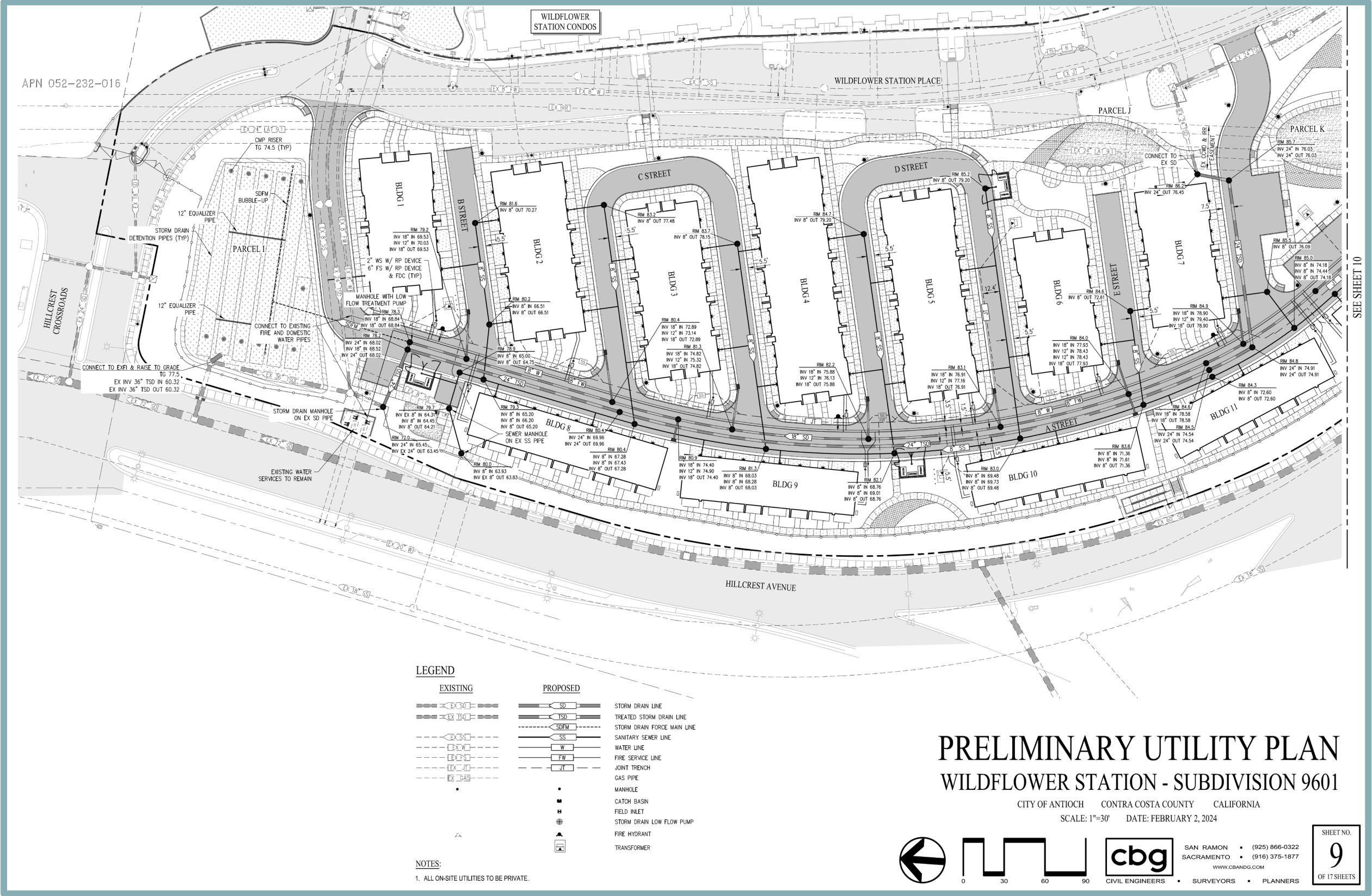
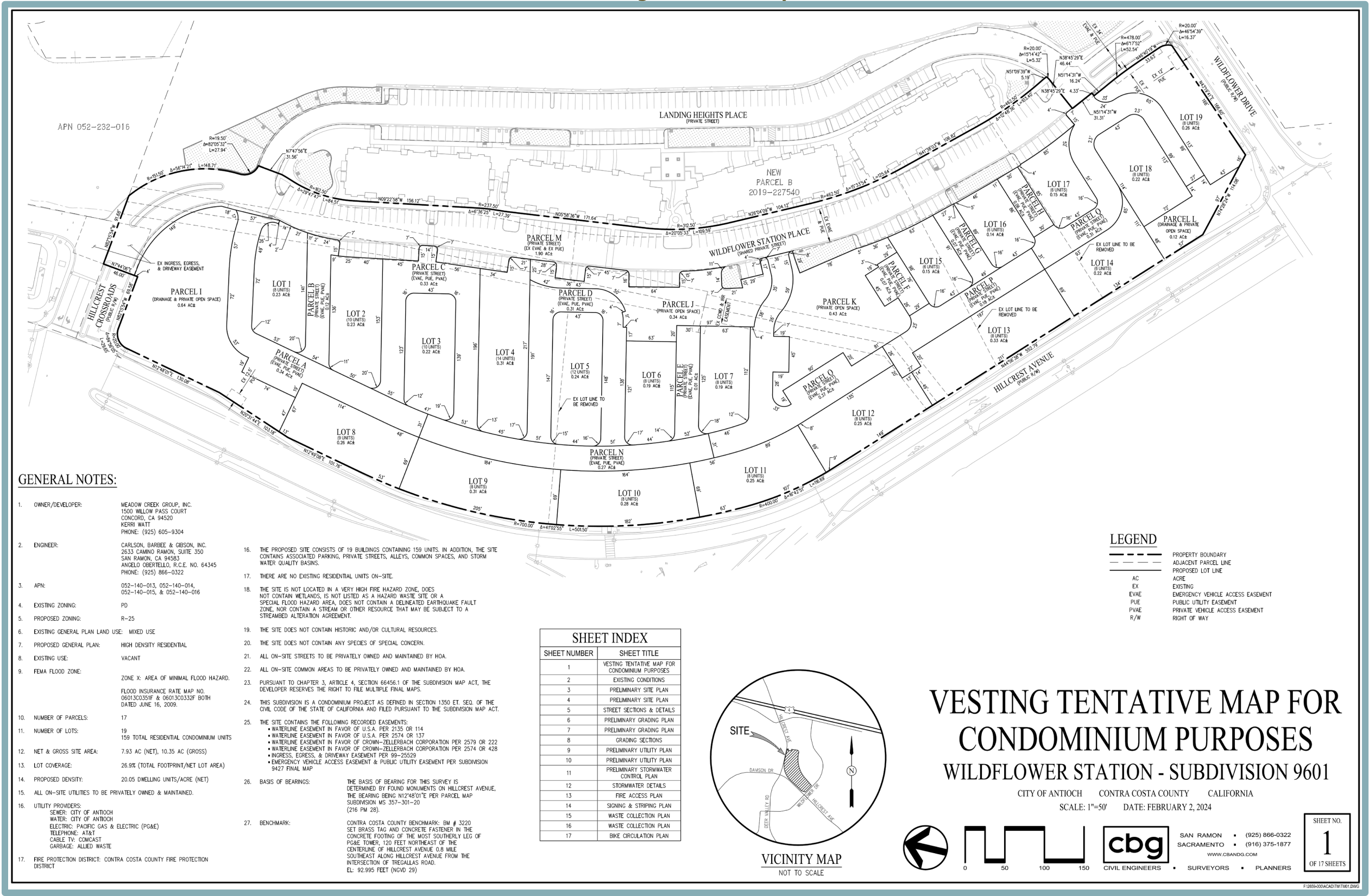




Figure 4  
Vesting Tentative Map





Automatic gates are proposed as an option at both entrances, pending approval from the current homeowners on Wildflower Station Place, and their homeowner's association (HOA). Similarly, the internal roadway network would be privately owned and maintained by the HOA. The entire site would be fenced with tubular steel, non-climb fencing located at the top of slope along Hillcrest Avenue and Wildflower Drive. A pedestrian/resident-access-only gate in the fencing would be provided between Buildings 10 and 11, with stairs and an accessible ramp to lead to the public sidewalk and the crosswalk on Hillcrest Avenue.

Existing utilities located in Hillcrest Avenue, Wildflower Station Place, and Wildflower Drive are adequately sized to serve the proposed development. As required by the City, all in-tract utilities (domestic water, water for fire service, sanitary sewer, storm drain, and bio-retention facilities) would be privately owned and maintained by the HOA. Eight-inch water and fire water lines would extend westward into the project site from Wildflower Station Place and eastward from Hillcrest Avenue. Similarly, eight-inch sanitary sewer lines would extend from the same connections to existing utility lines in the Wildflower Station Place and Hillcrest Avenue roadways. Finally, eight-inch stormwater connections would be extended into the project site from the surrounding roadways and would lead to the bio-retention facilities located at the northern area of the project site (see Figure 3).

## **Design Review**

Pursuant to the Antioch Municipal Code Section 9-5.2607(A)(1), proposed development of any new building or construction in the R-25 zoning district is subject to Design Review.

Each proposed townhome building would be three stories (approximately 37 to 40 feet tall), as measured to the roof ridge, consisting of "Row Townhomes" (63 units) and "Back to Back" (B2B) (96 units). All units would have at least one private deck, a storage area of at least 250 cubic feet, and an attached garage. Both styles would have living areas primarily on the second and third levels above parking.

The Row townhome buildings are designed with garages at the rear of the building and entry doors on the front façade of the building. Each building would have between six to nine units. Five floorplans are offered, ranging in size from 1,135 square feet (sf) to 1,238 sf. All five floorplans feature two bedrooms (en suite) and 2.5 bathrooms.

The B2B townhome buildings are designed with garages at the front and back of the building and front doors adjacent to the garages, in addition to front doors and covered porches on the short sides of each building. Each building would have between six to 14 units. Two floorplan types are offered: a two-bedroom (en suite) plan that is 1,293 sf, and a three-bedroom plan that is 1,381 sf. Both floorplans include 2.5 bathrooms.

The architectural style of the proposed residential buildings would be contemporary in design and would make use of stucco and siding finishes in several different colors on each building. The front façades of all buildings would be articulated to emphasize front entries and decks, while the sides and rears would include varying vertical and horizontal planes, and different finishes to provide articulation and interest.

Common open space of 0.43-acre is located at the center of the proposed development, at the main entry from Wildflower Station Place. Two buildings face the common area (Buildings 7 and 15) and portions of Buildings 12 and 13 have windows facing the common area. The central common area would include amenities with seating, picnic tables, a large shade structure, a play area for children, and a multi-purpose turf area. Such amenities would serve as recreation areas



for future residents, while making use of the otherwise unbuildable areas within the CCWD pipelines easement. Mailboxes and areas for bicycle parking would be located to the west of the common open space.

Street trees in the parkway strips would be located along all streets in addition to shrubs for privacy. All totaled, including the bio-retention area in the northern portion of the site, the private and common open space exceed the 25 percent landscaping requirement.

### **Requested/Required Entitlements**

The proposed project would require the following approvals from the City of Antioch:

- Vesting Tentative Subdivision Map; and
- Design Review.

### **D. DISCUSSION**

Pursuant to Public Resources Code (PRC) Section 21083.3 and Section 15183(b) of the CEQA Guidelines, a project that is consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified shall not require additional environmental review, except when it is necessary to examine whether there are project-specific significant effects which are peculiar to the project or its site. This streamlines the review of such projects and reduces the need to prepare repetitive environmental studies. More specifically, Section 15183(b) states the following:

- (b) In approving a project meeting the requirements of this section, a public agency shall limit its examination of environmental effects to those which the agency determines, in an initial study or other analysis:
  - (1) Are peculiar to the project or the parcel on which the project would be located,
  - (2) Were not analyzed as significant effects in a prior EIR on the zoning action, general plan, or community plan, with which the project is consistent,
  - (3) Are potentially significant off-site impacts and cumulative impacts which were not discussed in the prior EIR prepared for the general plan, community plan or zoning action, or
  - (4) Are previously identified significant effects which, as a result of substantial new information which was not known at the time the EIR was certified, are determined to have a more severe adverse impact than discussed in the prior EIR.

It should be noted that, according to Section 15183(f), an effect of a project on the environment shall not be considered peculiar to the project or the parcel if uniformly applied development policies or standards have been previously adopted by the City or County with a finding that the development policies or standards would substantially mitigate that environmental effect when applied to future projects, unless substantial new information shows that the policies or standards would not substantially mitigate the environmental effect.

As set forth by Sections 15168 and 15183 of the CEQA Guidelines, the program EIR, in this case the City's Housing Element EIR, serves as a basis for this 15183 Consistency Memorandum to determine if project-specific impacts would occur that are not adequately covered in the previously certified EIR. To the extent that the Housing Element policies and/or actions substantially mitigate



a particular project impact, the impact shall not be considered peculiar, pursuant to 15183(f), thus, eliminating the requirement for further environmental review.

This 15183 Consistency Memorandum indicates whether the proposed project would result in a significant impact that: (1) is peculiar to the project or the project site; (2) was not identified as a significant effect in the Housing Element EIR; or (3) are previously identified significant effects, which as a result of substantial new information that was not known at the time that the Housing Element EIR was certified, are determined to have a more severe adverse impact than discussed in the Housing Element EIR.

The Housing Element EIR anticipated the project site would be developed in accordance with the standards established for the R-25 zoning district, which allows for a density of 20 to 25 dwelling units per acre (du/ac). The proposed project would result in a density of approximately 20.05 du/ac, and therefore, would be consistent with the development anticipated for the project site within the Housing Element EIR.

The applicability of the Section 15183(b) criteria to the proposed project is described in the following sections.

### **Criterion 15183(b)(1)**

The proposed project would include approval of a Vesting Tentative Subdivision Map that would subdivide the project site into 19 lots for residential uses and condominium air space. The area proposed for development and the land uses proposed in the Vesting Tentative Subdivision Map are consistent with the City's updated Housing Element, and therefore consistent with what was analyzed in the Housing Element EIR. Design Review is also required in order to authorize the proposed building architecture, landscaping, and site design, which would ensure consistency with the City of Antioch General Plan, Zoning Ordinance, and Citywide Design Guidelines. The Design Review process would not alter the analysis or conclusions of the previously approved Housing Element EIR.

Additionally, the project site is comprised of ruderal grasses that are regularly mowed. Trees are not located on-site. Pursuant to the California Department of Conservation (DOC) California Important Farmland Finder, the project site does not contain Prime Farmland, Unique Farmland, or Farmland of Statewide Importance.<sup>1</sup> Pursuant to the Federal Emergency Management Agency (FEMA), the project site is located in Zone X, designated as an Area of Minimal Flood Hazard.<sup>2</sup> Based on the above, the project site does not contain any peculiar characteristics that could result in environmental effects through development of the proposed project. Thus, the proposed project would not result in any environmental effects peculiar to the project or the project site, and therefore would not result in environmental impacts beyond what was previously anticipated, analyzed, and mitigated by the Housing Element EIR.

### **Criterion 15183(b)(2)**

As discussed above, an EIR that assessed the full buildout of the City's updated Housing Element, including residential development of the project site, was certified in 2023. Because significant updates to local, State, and federal regulations have not been adopted since the certification of the previous EIR, and, as demonstrated in further detail in the Environmental Impact Analysis

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<sup>1</sup> California Department of Conservation. *California Important Farmland Finder*. Available at: <https://maps.conservation.ca.gov/dlrp/ciff/>. Accessed October 2023.

<sup>2</sup> Federal Emergency Management Agency. *FEMA's National Flood Hazard Layer (NFHL) Viewer*. Available at: <https://www.fema.gov/flood-maps/national-flood-hazard-layer>. Accessed October 2023.



section below, the proposed project would not result in any new environmental effects that were not analyzed as significant effects in the Housing Element EIR. As such, substantial changes would not occur with respect to the circumstances under which the project is undertaken.

**Criterion 15183(b)(3)**

The proposed project does not include off-site improvements, and, therefore, would not result in any impacts associated with off-site construction or operational activities. In addition, the proposed project is consistent with the City's updated Housing Element, and, therefore, development of the project site with the proposed uses was generally evaluated as part of the associated Housing Element EIR. Furthermore, given that the Housing Element EIR serves as a cumulative analysis, cumulative impacts related to development of the site with residential uses have already been anticipated and analyzed in the Housing Element EIR. Thus, the proposed project would not result in any off-site and/or cumulative impacts that were not evaluated in the prior EIR.

**Criterion 15183(b)(4)**

The question of "substantial new information" relates to the current CEQA requirements to assess impacts that were not required at the time the Housing Element EIR was certified. Because the Housing Element EIR was completed in accordance with the most recent version of the CEQA Appendix G Checklist, all required environmental impact areas were addressed in the Housing Element EIR. As previously discussed, changes would not occur with respect to development of the project site as a result of the proposed project, relative to what was anticipated for the site by the Housing Element EIR. Thus, the proposed project would not result in new significant impacts or substantially more severe significant impacts beyond what was anticipated in the Housing Element EIR. Therefore, the proposed project would not result in any more severe impacts than what was discussed in the prior EIR.

**E. ENVIRONMENTAL IMPACT ANALYSIS**

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The following discussion briefly evaluates each CEQA Appendix G environmental resource area. As noted above, the analysis below indicates whether the proposed project would result in a significant impact that: (1) is peculiar to the project or the project site; (2) was not identified as a significant effect in the Housing Element EIR; or (3) are previously identified significant effects, which as a result of substantial new information that was not known at the time that the Housing Element EIR was certified, are determined to have a more severe adverse impact than discussed in the Housing Element EIR.

**Air Quality**

The proposed project would be consistent with the Housing Element and, thus, was anticipated by the City and considered under the Housing Element EIR analysis. Accordingly, the proposed project would not result in any new significant effects related to air quality. However, the Housing Element EIR requires mitigation measures related to construction emissions of criteria air pollutant emissions from future housing developments (AIR-1), operational emissions of criteria air pollutant emissions from future housing developments (AIR-2), and health risks related to the generation of toxic air contaminants (TACs) and particulate matter (PM) 2.5 microns in diameter (PM<sub>2.5</sub>) during construction and operation of future housing developments (AIR-3a and AIR-3b).

Pursuant to Mitigation Measure AIR-1, construction of residential projects involving more than 114 single-family units or 240 multi-family units requires a quantitative air quality analysis to be conducted and measures identified to reduce the project's construction-related criteria air pollutant emission to below the applicable BAAQMD thresholds of significance. Given that the



proposed project consists of 159 multi-family residential units, Mitigation Measure AIR-1 as set forth in the Housing Element EIR is not applicable to the proposed project. Similarly, because the proposed project would not involve more than 325 single-family units or 451 multi-family units and would not include emergency generators, Mitigation Measures AIR-2 and AIR-3b would not apply to the project. In addition, the proposed project would be subject to the same regulations governing criteria air pollutants and emissions as identified for the projects evaluated in the Housing Element EIR.

However, a portion of the project site is located within a Bay Area Air Quality Management District (BAAQMD) Planning Healthy Place Map area defined as needing “Best Practices.”<sup>3</sup> Because project construction is anticipated to take longer than six months, the proposed project would be subject to Mitigation Measure AIR-3a. Given that the mitigation measure is structured so that development projects choose one option or the other, only part of the mitigation measure is applicable to the proposed project. Pursuant to Mitigation Measure AIR-3a, the proposed project shall equip all off-road diesel equipment with the most effective engine type as certified by the California Air Resources Board (CARB). Tier 4 engines would automatically meet the requirement. In addition, the project applicant shall prepare a Construction Emissions Minimization Plan (Emissions Plan) for all identified reduction measures. The Emissions Plan shall be submitted to the City (and BAAQMD upon request) for review and approval prior to the issuance of building permits. The City shall require compliance with mitigation measure AIR-3a as a Condition of Approval. Implementation of Mitigation Measure AIR-3a would ensure the proposed project would not result in a significant impact that is peculiar to the project or the project site, would not be identified as a significant effect in the Housing Element EIR, and would not result in a more severe adverse impact than the significant effects previously identified within the Housing Element EIR.

Overall, based on the above, the proposed project would not result in a significant impact that is peculiar to the project or the project site, was not identified as a significant effect in the Housing Element EIR, and would not result in a more severe adverse impact than the significant effects previously identified within the Housing Element EIR.

### **Greenhouse Gas Emissions**

Pursuant to the Housing Element EIR, the BAAQMD’s recommended plan-level thresholds of significance for GHG emissions include two options: Option A, to meet the State’s emission reduction goals, and Option B, to be consistent with a local GHG reduction strategy that meets the State criteria under CEQA Guidelines Section 15183.5(b). To demonstrate compliance with Option A of the plan-level thresholds, BAAQMD recommends implementing various design elements for typical residential, commercial, and retail land use projects. Such design elements would include, but are not limited to, a lack of natural gas appliances or plumbing, and a demonstrated reduction in project-generated vehicle miles traveled (VMT).

According to the Housing Element EIR, implementation of General Plan policies, such as Policy 11.7.2(c) and 11.7.2(d) (which were updated as part of the Housing Element Update), would comply with Options A and B of the BAAQMD’s recommend plan-level thresholds of significance and ensure that future development would not result in a cumulatively considerable contribution to global climate change. Furthermore, implementation of existing General Plan Policies 7.4.2, 10.6.2, 10.7.2, 10.8.2, and Housing Element Policy 4.1 would help to reduce GHG emissions from transportation, energy use, and water use.

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<sup>3</sup> Bay Area Air Quality Management District. *Planning Healthy Places*. Available at: <https://www.baaqmd.gov/plans-and-climate/planning-healthy-places>. Accessed October 2023.



Overall, the Housing Element EIR concluded that implementation of the existing and updated General Plan Policies related to GHG emissions would comply with the BAAQMD's recommended thresholds, and future development under the updated Housing Element would have a less-than-significant impact related to GHG emissions. Given that the project site was considered for residential development in the Housing Element, the proposed project would be consistent with the Housing Element and therefore evaluated within the Housing Element EIR. Thus, the proposed project would be required to comply with all of the applicable General Plan policies, and would not result in a significant impact that is peculiar to the project or project site, a significant effect that was not previously identified in the Housing Element EIR, or a substantially more severe significant effect related to GHG emissions during construction or operation.

## **Transportation**

Pursuant to Mitigation Measure TRANS-1 as set forth in the Housing Element EIR, individual housing project development proposals that do not screen out from a VMT impact analysis are required to provide a quantitative VMT analysis; however, the Housing Element EIR provides that any project that is exempt from CEQA is not required to conduct a VMT analysis. As demonstrated through this 15183 Consistency Memorandum, the proposed project would not result in a significant impact that is peculiar to the project or project site, a significant effect that was not previously identified in the Housing Element EIR, or a substantially more severe significant effect related to transportation beyond what was identified in the Housing Element EIR. Therefore, pursuant to Section 15183 of the CEQA Guidelines, the proposed project qualifies for exemption from further environmental review under CEQA. Because the proposed project would be considered exempt, Mitigation Measure TRANS-1 is not applicable. Thus, the proposed project would not result in a significant impact that is peculiar to the project or the project site, was not identified as a significant effect in the Housing Element EIR, and would not result in a more severe adverse impact than the significant effects previously identified within the Housing Element EIR.

## **Remaining Impact Areas**

In addition to the CEQA topics discussed in the previous sections of this 15183 Consistency Memorandum, the Housing Element EIR included analysis of the following issue areas:

- Aesthetics;
- Agriculture and Forestry Resources;
- Biological Resources;
- Cultural Resources;
- Energy;
- Geology and Soils;
- Hazards and Hazardous Materials;
- Hydrology and Water Quality;
- Land Use and Planning;
- Mineral Resources;
- Noise;
- Population and Housing;
- Public Services;
- Recreation;
- Tribal Cultural Resources;
- Utilities and Service Systems; and
- Wildfire.



As discussed previously, construction and operation activities associated with the proposed project would occur within a site previously analyzed as part of the Housing Element EIR and would not result in any increase to the area of disturbance previously anticipated by the Housing Element EIR. For these reasons, and given that site conditions, as well as conditions in the project vicinity, have remained the same since adoption of the Housing Element EIR, the proposed project would not result in new significant impacts or substantially more significant impacts related to the following environmental issue areas: aesthetics, agriculture and forestry resources, cultural resources, geology and soils, hazards and hazardous materials, hydrology and water quality, mineral resources, noise, public services, recreation, tribal cultural resources, and wildfire. For example, new scenic vistas have not appeared within the project vicinity subsequent to the adoption of the Housing Element EIR, and project design would be required to comply with applicable General Plan policies and City of Antioch regulations related to building height, setback, and neighborhood character. Similarly, the project site has not undergone changes related to farmland, subsurface conditions, or hydrology since adoption of the Housing Element EIR. The existing uses within the project vicinity are the same or similar to those that existed during preparation of the Housing Element EIR. As such, project construction would not be anticipated to result in substantial increases in impacts to existing sensitive receptors beyond the levels anticipated by the Housing Element EIR. Therefore, the proposed project would not result in new or substantially more significant impacts beyond what was identified in the Housing Element EIR.

Similarly, the biological resources in the project vicinity and at the project site have remained the same since adoption of the Housing Element EIR. The Housing Element EIR identified the project site as a Housing Site outside of any occurrences of special-status species protected under State and/or federal regulations. As such, the potential for adverse impacts to biological resources as part of the proposed project is low. Consistency with the General Plan policies identified in the Housing Element EIR would ensure that new or substantially more significant impacts beyond what was identified in the Housing Element EIR would not occur.

With respect to energy, the proposed project would be subject to the currently adopted 2022 California Green Building Standards Code (CALGreen Code) and the Building Energy Efficiency Standards (Title 24, Part 6 of the California Code of Regulations), which include more stringent requirements related to energy efficiency than previous iterations of the aforementioned regulations to move the State closer to its net-zero energy goals. The 2022 Building Energy Efficiency Standards are designed to move the State closer to its net-zero energy goals for new residential development by requiring all new residences to install enough renewable energy to offset all the electricity needs of each residential unit, as well as battery storage to maximize on-site use of solar energy and avoid electricity demand during peak consumption periods on the grid. Energy reductions relative to previous Building Energy Efficiency Standards are achieved through various regulations, including requirements for the use of high-efficacy lighting, improved water heating system efficiency, and high-performance attics and walls. Additionally, all construction equipment and operation thereof would be regulated per the CARB In-Use Off-Road Diesel Vehicle Regulation. The In-Use Off-Road Diesel Vehicle Regulation is intended to reduce emissions from in-use, off-road, heavy-duty diesel vehicles in California by imposing limits on idling, requiring all vehicles to be reported to CARB, restricting the addition of older vehicles into fleets, and requiring fleets to reduce emissions by retiring, replacing, or repowering older engines, or installing exhaust retrofits. The In-Use Off-Road Diesel Vehicle Regulation would subsequently help to improve fuel efficiency and reduce GHG emissions. Technological innovations and more stringent standards are being researched, such as multi-function equipment, hybrid equipment, or other design changes, which could help to reduce demand on oil and emissions associated with construction. Thus, the proposed project would not result in new significant impacts or



substantially more significant impacts related to energy beyond what were identified in the Housing Element EIR. Thus, the proposed project would not result in new significant impacts or substantially more significant impacts related to energy beyond what were identified in the Housing Element EIR.

Finally, with respect to land use and planning and population and housing, the proposed project would not physically divide an established community, and would be consistent with the uses anticipated in the updated Housing Element. New utility lines installed as part of the proposed project would be extended from existing lines in the adjacent roadway network and would be constructed consistent with the City's applicable engineering design standards. Additionally, any new utility lines associated with the proposed project would be sized to accommodate only the project, thereby ensuring the project does not induce substantial unplanned population growth. Furthermore, the proposed project would be subject to applicable development impact fees, ensuring the project's fair-share contribution for any improvements to various public services and utilities. Thus, the proposed project would not result in new significant impacts or substantially more significant impacts related to the aforementioned environmental issue areas beyond what were identified in the Housing Element EIR.

It should be noted that the Housing Element EIR did not identify any significant impacts and associated mitigation measures beyond those discussed above related to air quality and transportation. Therefore, the Housing Element EIR does not include any additional mitigation measures that would be applicable to the proposed project.

Thus, with respect to the foregoing issue areas, the proposed project would result in similar impacts as those identified within the Housing Element EIR. Compliance with applicable federal, State, and local policies, regulations, and standards would ensure impacts related to the aforementioned issue areas would be reduced to a less-than-significant level.

## **F. CONCLUSION**

As demonstrated by the discussions above, pursuant to CEQA Guidelines Section 15183, additional environmental review under CEQA would not be required for the proposed project.

## **G. APPLICABLE MITIGATION MEASURES**

The mitigation measures from the Housing Element EIR, as presented below, would be required to be implemented with approval of the proposed project.

### **Mitigation Measure AIR-3a: Residential Construction Controls for Diesel Particulate Matter.**

For construction of residential projects with a construction duration greater than 6 months that are located in an area defined as needing "Best Practices" or "Further Study" on the BAAQMD's Planning Healthy Places Map (<https://www.baaqmd.gov/plans-and-climate/planning-healthy-places>), the project applicant shall apply one of the following two measures:

1. The project applicant shall retain a qualified air quality consultant to prepare a Health Risk Assessment (HRA) in accordance with current guidance from the Office of Environmental Health Hazard Assessment to determine the health risks to sensitive receptors exposed to diesel particulate matter (DPM) from project construction emissions. The HRA shall be submitted to the City (and BAAQMD if specifically requested) for review and approval. If the HRA concludes that the health risks are at or below acceptable levels, then DPM reduction measures are not required. If the HRA concludes that the health risks exceed acceptable levels, DPM reduction measures shall be identified to reduce the health risks to acceptable



levels. Identified DPM reduction measures shall be submitted to the City for review and approval prior to the issuance of building permits and the approved DPM reduction measures shall be implemented during construction.

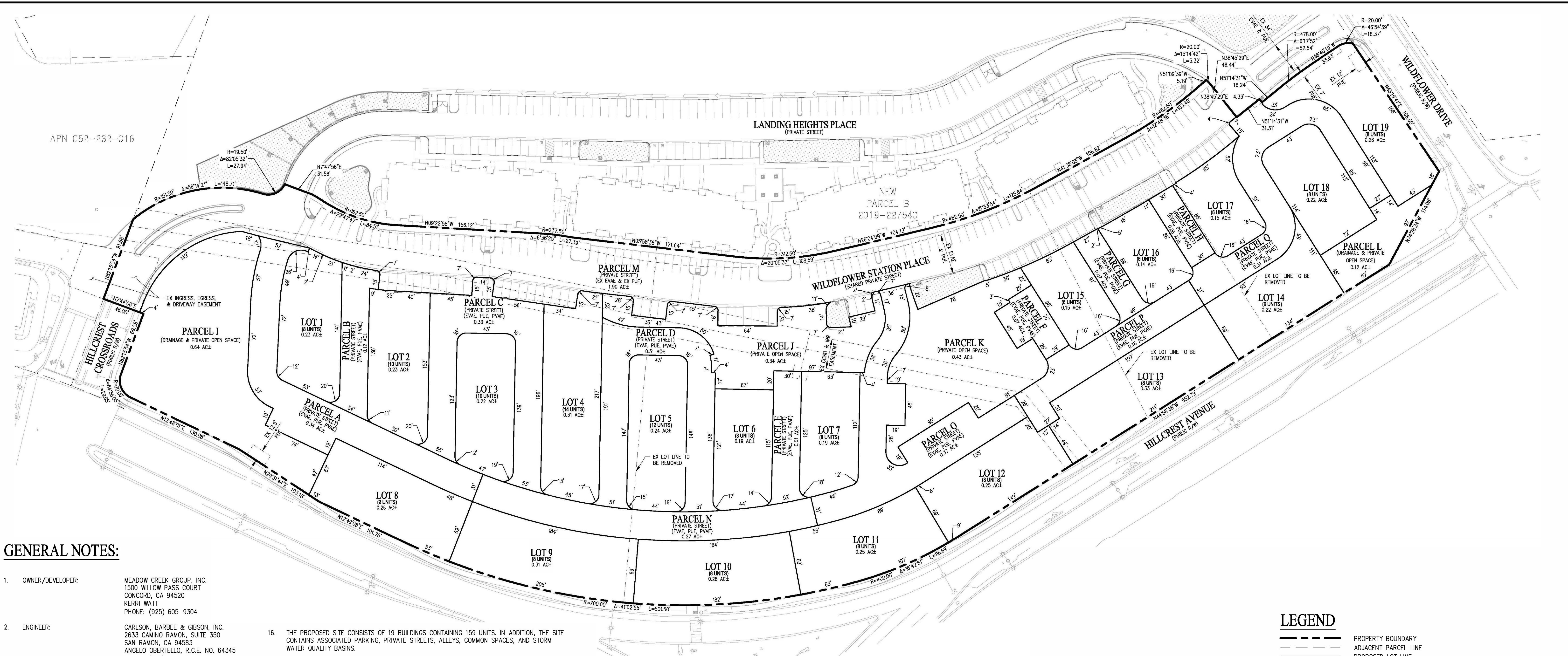
OR

2. All off-road diesel equipment shall be equipped with the most effective VDECS available for the engine type (Tier 4 engines automatically meet this requirement) as certified by CARB. The equipment shall be properly maintained and tuned in accordance with manufacturer specifications. In addition, the project applicant shall prepare a Construction Emissions Minimization Plan (Emissions Plan) for all identified DPM reduction measures (if any). The Emissions Plan shall be submitted to the City (and BAAQMD if specifically requested) for review and approval prior to the issuance of building permits. The Emissions Plan shall include the following:

- An equipment inventory summarizing the type of off-road equipment required for each phase of construction, including the equipment manufacturer, equipment identification number, engine model year, engine certification (tier rating), horsepower, and engine serial number. For all VDECS, the equipment inventory shall also include the technology type, serial number, make, model, manufacturer, CARB verification number level, and installation date.
- A Certification Statement that the Contractor agrees to comply fully with the Emissions Plan and acknowledges that a significant violation of the Emissions Plan shall constitute a material breach of contract.



EXHIBIT B



GENERAL NOTES:

1. OWNER/DEVELOPER:

MEADOW CREEK GROUP, INC.  
1500 WILLOW PASS COURT  
CONCORD, CA 94520  
KERRI WATT  
PHONE: (925) 605-9304
2. ENGINEER:

CARLSON, BARBEE & GIBSON, INC.  
2633 CAMINO RAMON, SUITE 350  
SAN RAMON, CA 94583  
ANGELO OBERTELLO, R.C.E. NO. 64345  
PHONE: (925) 866-0322
3. APN:

052-140-013, 052-140-014,  
052-140-015, & 052-140-016
4. EXISTING ZONING:

PD
5. PROPOSED ZONING:

R-25
6. EXISTING GENERAL PLAN LAND USE:

MIXED USE
7. PROPOSED GENERAL PLAN:

HIGH DENSITY RESIDENTIAL
8. EXISTING USE:

VACANT
9. FEMA FLOOD ZONE:

ZONE X: AREA OF MINIMAL FLOOD HAZARD.  
  
FLOOD INSURANCE RATE MAP NO.  
0601300351F & 0601300332F BOTH  
DATED JUNE 16, 2009.
10. NUMBER OF PARCELS:

17
11. NUMBER OF LOTS:

19  
159 TOTAL RESIDENTIAL CONDOMINIUM UNITS
12. NET & GROSS SITE AREA:

7.93 AC (NET), 10.35 AC (GROSS)
13. LOT COVERAGE:

26.9% (TOTAL FOOTPRINT/NET LOT AREA)
14. PROPOSED DENSITY:

20.05 DWELLING UNITS/ACRE (NET)
15. ALL ON-SITE UTILITIES TO BE PRIVATELY OWNED & MAINTAINED.
16. UTILITY PROVIDERS:

SEWER: CITY OF ANTIOCH  
WATER: CITY OF ANTIOCH  
ELECTRIC: PACIFIC GAS & ELECTRIC (PG&E)  
TELEPHONE: AT&T  
CABLE TV: COMCAST  
GARBAGE: ALLIED WASTE
17. FIRE PROTECTION DISTRICT:

CONTRA COSTA COUNTY FIRE PROTECTION DISTRICT

16. THE PROPOSED SITE CONSISTS OF 19 BUILDINGS CONTAINING 159 UNITS. IN ADDITION, THE SITE CONTAINS ASSOCIATED PARKING, PRIVATE STREETS, ALLEYS, COMMON SPACES, AND STORM WATER QUALITY BASINS.

17. THERE ARE NO EXISTING RESIDENTIAL UNITS ON-SITE.

18. THE SITE IS NOT LOCATED IN A VERY HIGH FIRE HAZARD ZONE, DOES NOT CONTAIN WETLANDS, IS NOT LISTED AS A HAZARDOUS WASTE SITE OR A SPECIAL FLOOD HAZARD AREA, DOES NOT CONTAIN A DELINEATED EARTHQUAKE FAULT ZONE, NOR CONTAIN A STREAM OR OTHER RESOURCE THAT MAY BE SUBJECT TO A STREAMBED ALTERATION AGREEMENT.

19. THE SITE DOES NOT CONTAIN HISTORIC AND/OR CULTURAL RESOURCES.

20. THE SITE DOES NOT CONTAIN ANY SPECIES OF SPECIAL CONCERN.

21. ALL ON-SITE STREETS TO BE PRIVATELY OWNED AND MAINTAINED BY HOA.

22. ALL ON-SITE COMMON AREAS TO BE PRIVATELY OWNED AND MAINTAINED BY HOA.

23. PURSUANT TO CHAPTER 3, ARTICLE 4, SECTION 66456.1 OF THE SUBDIVISION MAP ACT, THE DEVELOPER RESERVES THE RIGHT TO FILE MULTIPLE FINAL MAPS.

24. THIS SUBDIVISION IS A CONDOMINIUM PROJECT AS DEFINED IN SECTION 1350 ET. SEQ. OF THE CIVIL CODE OF THE STATE OF CALIFORNIA AND FILED PURSUANT TO THE SUBDIVISION MAP ACT.

25. THE SITE CONTAINS THE FOLLOWING RECORDED EASEMENTS:
  - WATERLINE EASEMENT IN FAVOR OF U.S.A. PER 2135 OR 114
  - WATERLINE EASEMENT IN FAVOR OF U.S.A. PER 2574 OR 137
  - WATERLINE EASEMENT IN FAVOR OF CROWN-ZELLERBACH CORPORATION PER 2579 OR 222
  - WATERLINE EASEMENT IN FAVOR OF CROWN-ZELLERBACH CORPORATION PER 2574 OR 428
  - INGRESS, EGRESS, & DRIVEWAY EASEMENT PER 99-25529
  - EMERGENCY VEHICLE ACCESS EASEMENT & PUBLIC UTILITY EASEMENT PER SUBDIVISION 9427 FINAL MAP

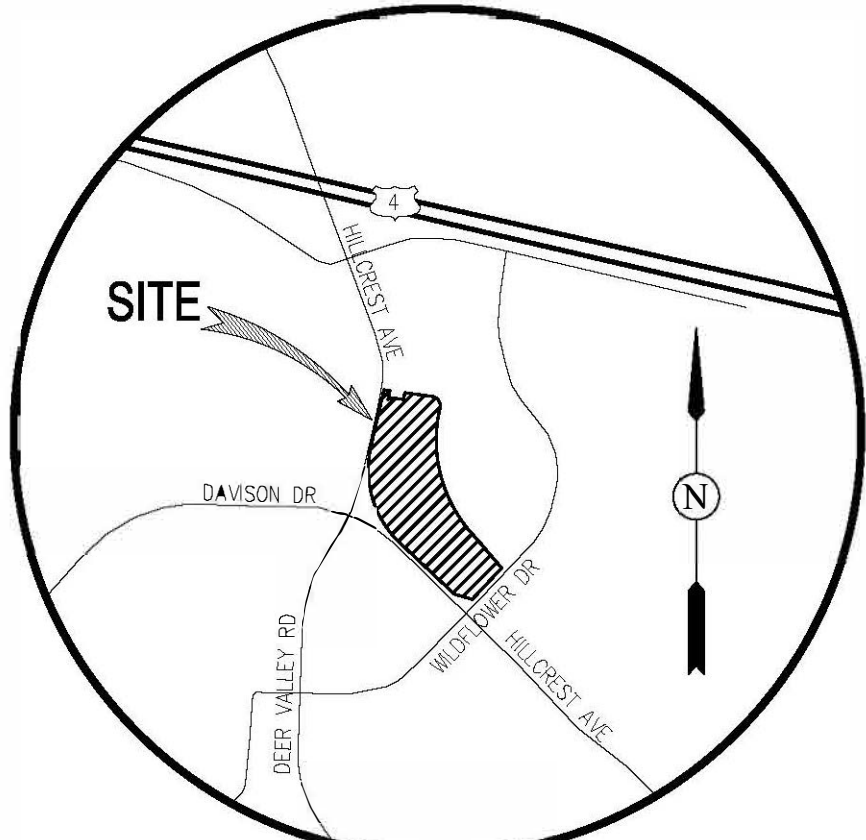
26. BASIS OF BEARINGS:

THE BASIS OF BEARING FOR THIS SURVEY IS DETERMINED BY FOUND MONUMENTS ON HILLCREST AVENUE, THE BEARING BEING N12°48'01"E PER PARCEL MAP SUBDIVISION MS 357-301-20 (216 PM 28).

27. BENCHMARK:

CONTRA COSTA COUNTY BENCHMARK: BM # 3220 SET BRASS TAG AND CONCRETE FASTENER IN THE CONCRETE FOOTING OF THE MOST SOUTHERLY LEG OF PG&E TOWER, 120 FEET NORTHEAST OF THE CENTERLINE OF HILLCREST AVENUE 0.8 MILE SOUTHEAST ALONG HILLCREST AVENUE FROM THE INTERSECTION OF TREGALLAS ROAD.  
EL: 92.995 FEET (NGVD 29)

SHEET INDEX	
SHEET NUMBER	SHEET TITLE
1	VESTING TENTATIVE MAP FOR CONDOMINIUM PURPOSES
2	EXISTING CONDITIONS
3	PRELIMINARY SITE PLAN
4	PRELIMINARY SITE PLAN
5	STREET SECTIONS & DETAILS
6	PRELIMINARY GRADING PLAN
7	PRELIMINARY GRADING PLAN
8	GRADING SECTIONS
9	PRELIMINARY UTILITY PLAN
10	PRELIMINARY UTILITY PLAN
11	PRELIMINARY STORMWATER CONTROL PLAN
12	STORMWATER DETAILS
13	FIRE ACCESS PLAN
14	SIGNING & STRIPING PLAN
15	WASTE COLLECTION PLAN
16	WASTE COLLECTION PLAN
17	BIKE CIRCULATION PLAN



VICINITY MAP  
NOT TO SCALE

LEGEND

- PROPERTY BOUNDARY
- ADJACENT PARCEL LINE
- PROPOSED LOT LINE
- ACRE
- EXISTING
- EMERGENCY VEHICLE ACCESS EASEMENT
- PUBLIC UTILITY EASEMENT
- PRIVATE VEHICLE ACCESS EASEMENT
- RIGHT OF WAY

AC

ACRE

EX

EXISTING

EVAE

EMERGENCY VEHICLE ACCESS EASEMENT

PUE

PUBLIC UTILITY EASEMENT

PVAE

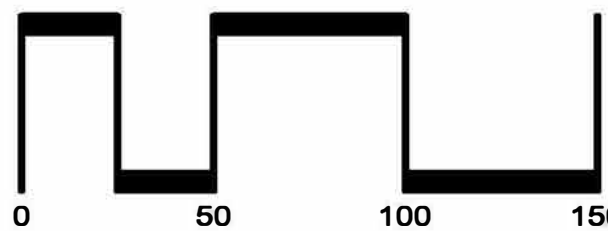
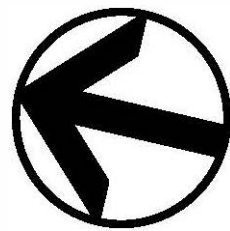
PRIVATE VEHICLE ACCESS EASEMENT

R/W

RIGHT OF WAY

VESTING TENTATIVE MAP FOR  
CONDOMINIUM PURPOSES  
WILDFLOWER STATION - SUBDIVISION 9601

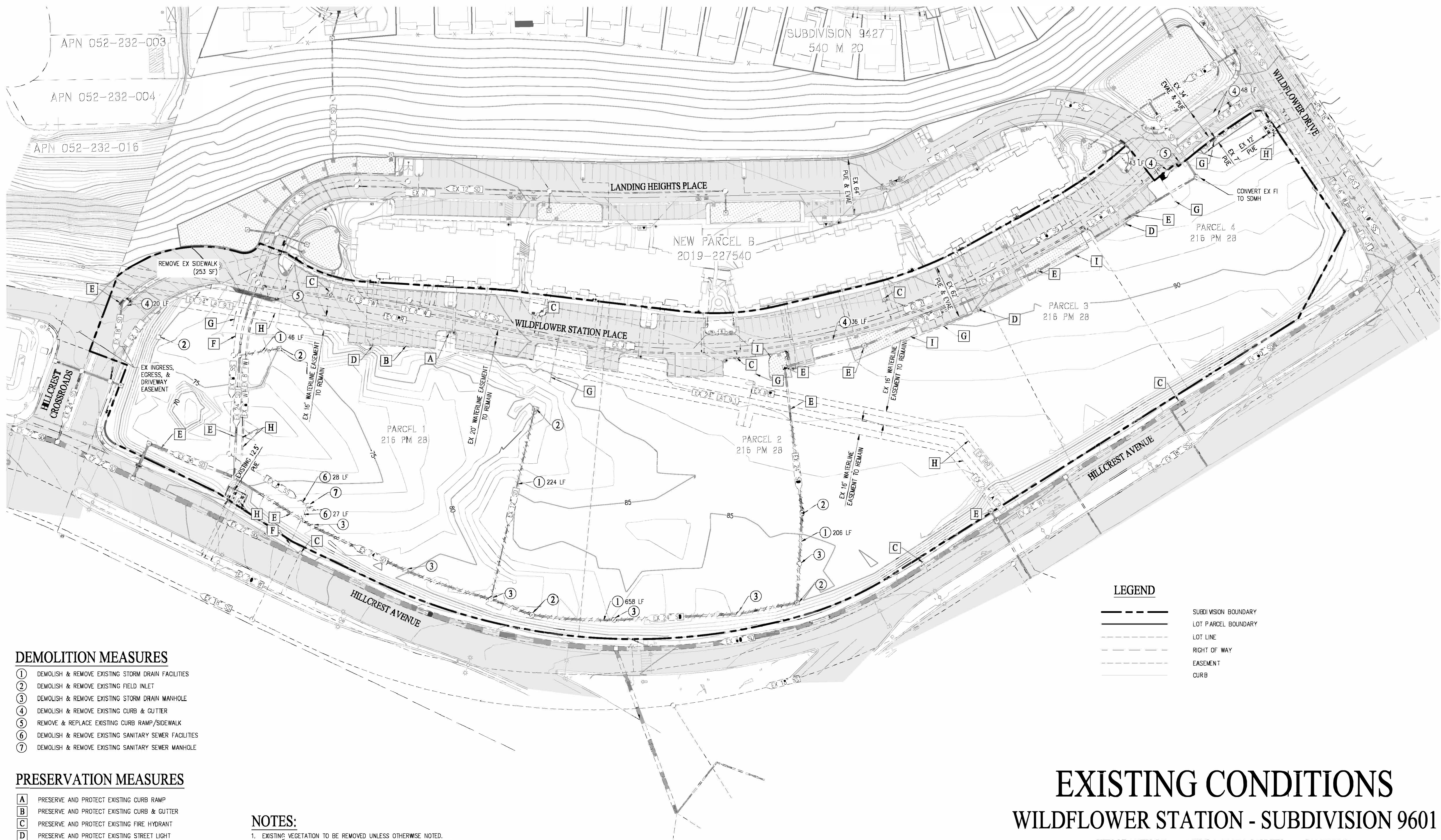
CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA  
SCALE: 1"=50' DATE: FEBRUARY 2, 2024



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1  
OF 17 SHEETS





**DEMOLITION MEASURES**

- ① DEMOLISH & REMOVE EXISTING STORM DRAIN FACILITIES
- ② DEMOLISH & REMOVE EXISTING FIELD INLET
- ③ DEMOLISH & REMOVE EXISTING STORM DRAIN MANHOLE
- ④ DEMOLISH & REMOVE EXISTING CURB & GUTTER
- ⑤ REMOVE & REPLACE EXISTING CURB RAMP/SIDEWALK
- ⑥ DEMOLISH & REMOVE EXISTING SANITARY SEWER FACILITIES
- ⑦ DEMOLISH & REMOVE EXISTING SANITARY SEWER MANHOLE

**PRESERVATION MEASURES**

- A PRESERVE AND PROTECT EXISTING CURB RAMP
- B PRESERVE AND PROTECT EXISTING CURB & GUTTER
- C PRESERVE AND PROTECT EXISTING FIRE HYDRANT
- D PRESERVE AND PROTECT EXISTING STREET LIGHT
- E PRESERVE AND PROTECT IN PLACE EXISTING STORM DRAIN FACILITIES
- F PRESERVE AND PROTECT IN PLACE EXISTING SANITARY SEWER FACILITIES
- G PRESERVE AND PROTECT EXISTING JOINT TRENCH FACILITIES
- H PRESERVE AND PROTECT EXISTING WATER FACILITIES
- I PRESERVE AND PROTECT EXISTING STORMWATER BASIN

**NOTES:**

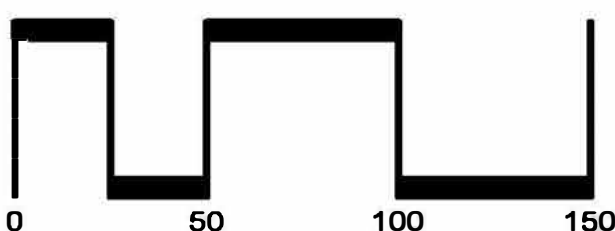
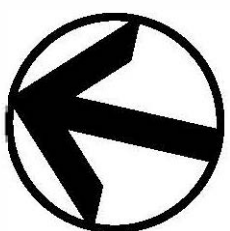
- 1. EXISTING VEGETATION TO BE REMOVED UNLESS OTHERWISE NOTED.
- 2. EXISTING UTILITIES TO REMAIN AND BE PROTECTED IN PLACE UNLESS OTHERWISE NOTED.
- 3. REFER TO JOINT TRENCH PLAN AND STREET LIGHT PLANS FOR ADDITIONAL INFORMATION.
- 4. CONTRACTOR SHALL SEQUENCE THE DEMOLITION ACTIVITIES AS NECESSARY TO MAINTAIN ROADS OPEN AND UTILITY SERVICE TO EXISTING CONDOS.

**LEGEND**

- SUBDIVISION BOUNDARY
- LOT PARCEL BOUNDARY
- LOT LINE
- RIGHT OF WAY
- EASEMENT
- CURB

**EXISTING CONDITIONS**  
**WILDFLOWER STATION - SUBDIVISION 9601**

CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA  
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**2**  
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LEGEND

- SUBDIVISION BOUNDARY
- ADJACENT PARCEL LINE
- PROPOSED EASEMENT
- PROPOSED LOT LINE
- BUILDING OVERHANG
- BIORETENTION AREA
- PROPOSED SIDEWALK/PATHWAY
- PROPOSED PAVEMENT
- EXISTING PAVEMENT TO REMAIN
- TRASH ENCLOSURE  
SEE SHEET 15 FOR DETAIL

ABBREVIATIONS

- AC ACRES
- BLDG BUILDING
- CL CENTERLINE
- D/W DRIVEWAY
- EVAE EMERGENCY VEHICLE ACCESS EASEMENT
- EX EXISTING
- FDC FIRE DEPARTMENT CONNECTION
- FF FINISHED FLOOR
- FS FIRE SERVICE
- GB GRADE BREAK
- HP HIGH POINT
- INV INVERT
- JT JOINT TRENCH
- LF LINEAR FEET
- LL LOT LINE
- LP LOW POINT
- LS LANDSCAPE
- PAD BUILDING PAD ELEVATION
- PUE PUBLIC UTILITY EASEMENT
- PVAE PRIVATE VEHICLE ACCESS EASEMENT
- RP REDUCED PRESSURE ASSEMBLY
- R/W RIGHT OF WAY
- SF SQUARE FEET
- S/W SIDEWALK
- TC TOP OF CURB
- TRC TOP OF ROLLED CURB
- TSM TOP OF SOIL MIX
- TVC TOP OF VERTICAL CURB
- TYP TYPICAL
- WS WATER SERVICE

UNIT COUNT	
	TOTAL
BACK-TO-BACK BUILDINGS	96
TOWNHOMES	63
TOTAL	159

OPEN SPACE SUMMARY		
	REQUIRED	PROVIDED
OPEN SPACE (AMC 9-5.706)	200 SF/UNIT	385 SF/UNIT
*TOTAL SF	31,800 SF	61,276 SF
PRIVATE (60 SF/UNIT MIN)	9,540 SF	16,022 SF
COMMON:		
CENTRAL OPEN AREA - PARCEL K		18,239 SF
PASSIVE PARK		3,312 SF
OPEN PLAY AREA - PARCEL J		15,694 SF
COMMON BETWEEN BLDG 9 & 10		4,297 SF
COMMON BETWEEN BLDG 13 & 14		3,712 SF
TOTAL SF		45,254 SF
*LANDSCAPING (AMC 9-5.706)	25% OF SITE (1.6 AC)	32% OF SITE (2.8 AC)

- NOTES:
- \*OPEN SPACE TOTAL DOES NOT INCLUDE BIORETENTION AREA
  - \*\*LANDSCAPING TOTAL INCLUDES BIORETENTION AREA

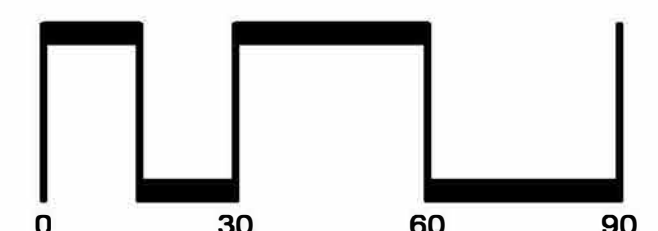
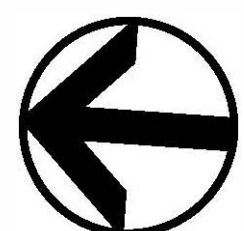
PARKING SUMMARY	
	PROVIDED
GUEST PARKING	20
SURPLUS SHARED PARKING (SEE NOTE 3)	57
ACCESSIBLE PARKING	2
ELECTRIC VEHICLE PARKING	3

- NOTES:
- EV PARKING STALLS ARE CALCULATED PER CALIFORNIA GREEN BUILDING CODE 2022 SECTION 4.106.4.2 (10% OF TOTAL PARKING SPACES).
  - ACCESSIBLE PARKING STALLS ARE CALCULATED PER 2022 CALIFORNIA BUILDING CODE SECTION 1109A.5 (5% OF TOTAL PARKING SPACES).
  - THE PARKING SPACES ON THE WEST SIDE OF WILDFLOWER STATION PLACE ARE TO BE SHARED AMONGST THE TOWNHOMES.

PRELIMINARY SITE PLAN  
WILDFLOWER STATION - SUBDIVISION 9601

CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA

SCALE: 1"=30' DATE: FEBRUARY 2, 2024

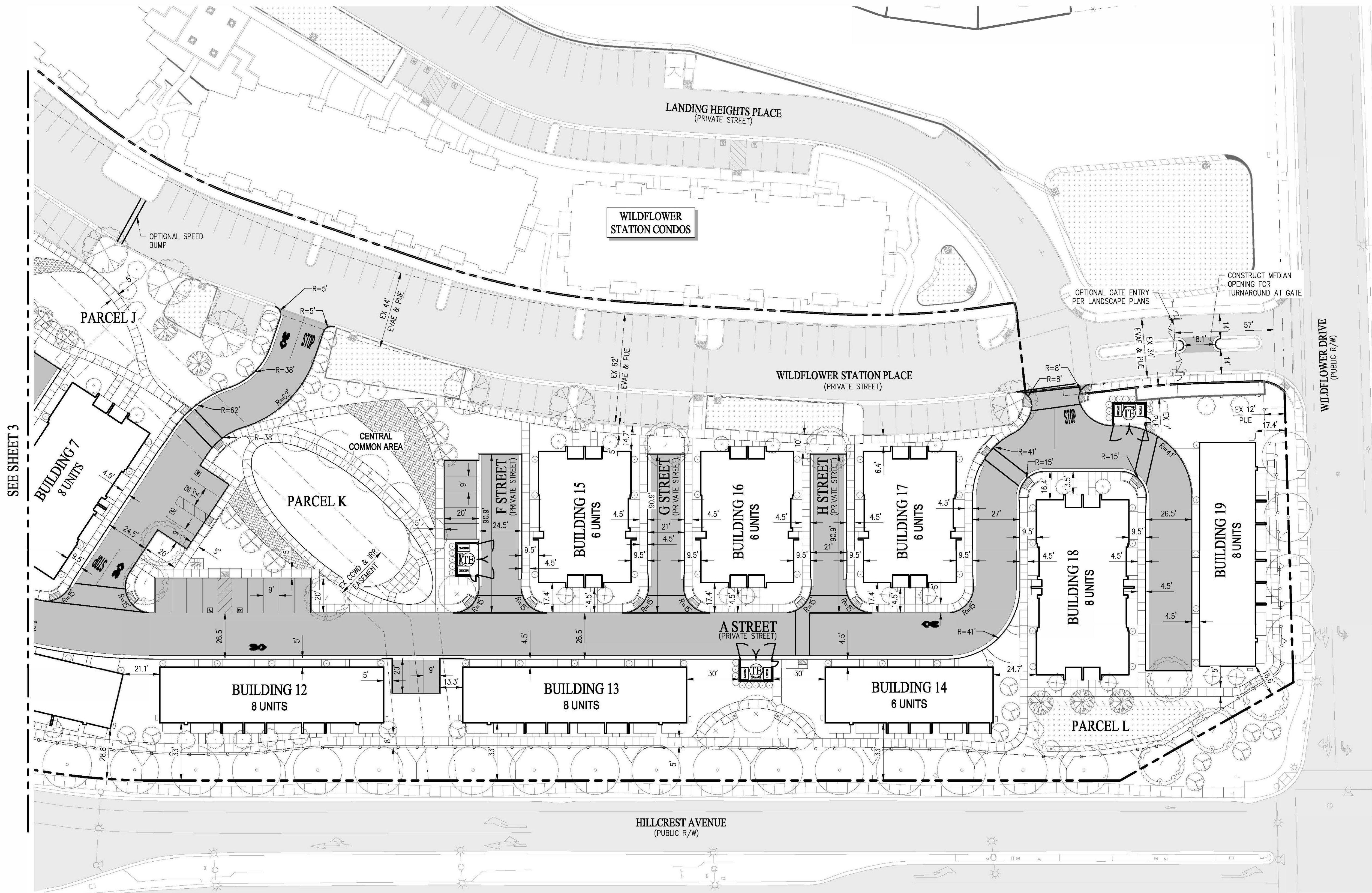


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SHEET NO.  
3  
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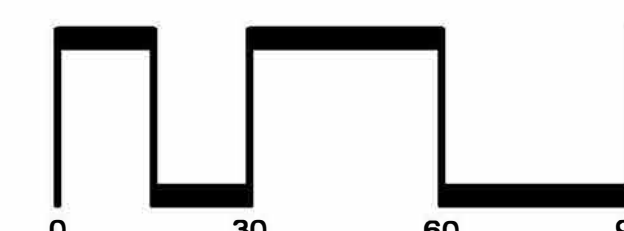
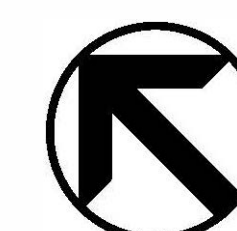




# PRELIMINARY SITE PLAN

## WILDFLOWER STATION - SUBDIVISION 9601

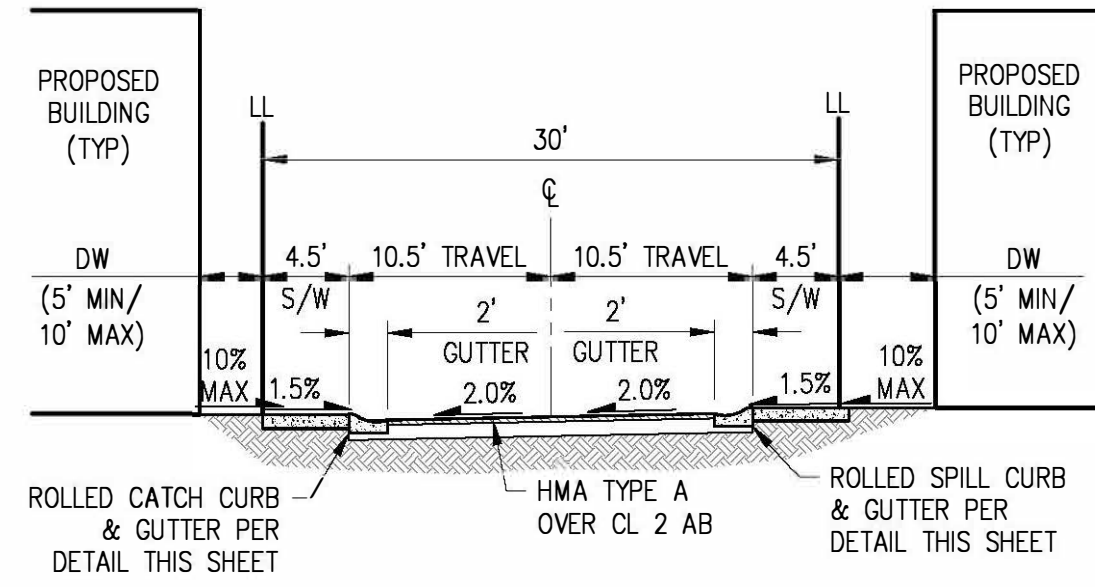
CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA  
SCALE: 1"=30' DATE: FEBRUARY 2, 2024



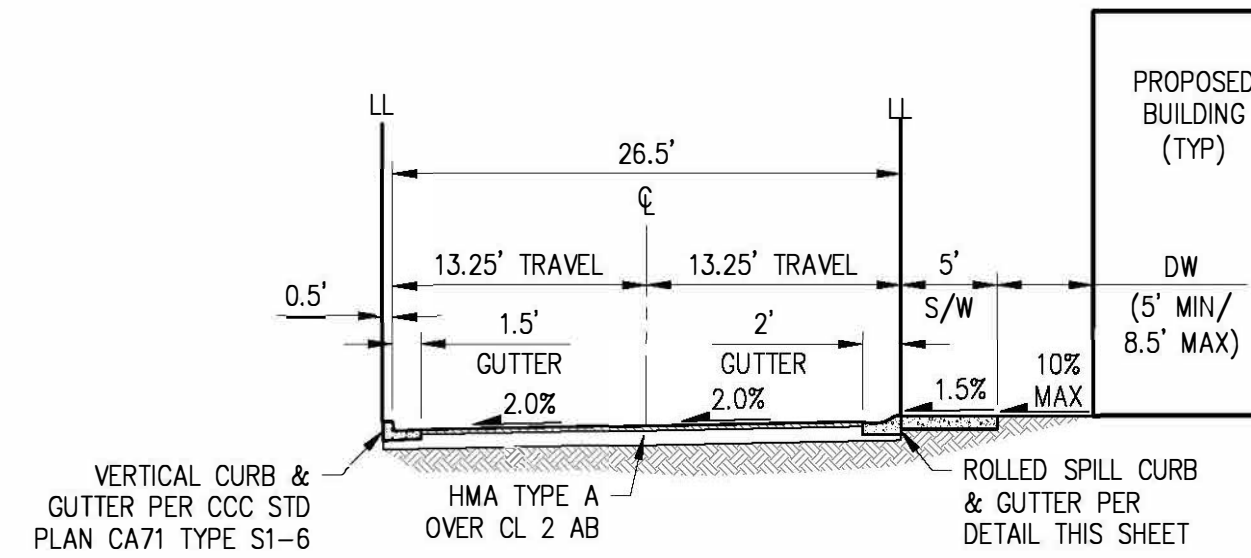
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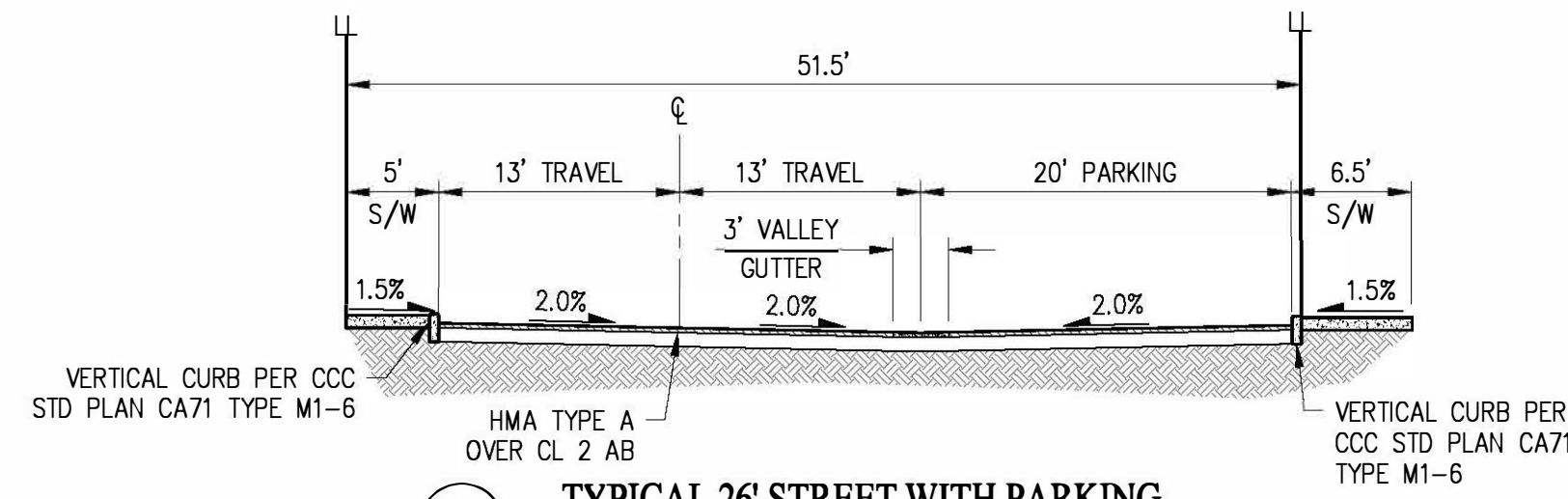




**A** TYPICAL 21' ALLEY  
(ROLLED CURB & GUTTER AND ROLLED CURB & GUTTER)



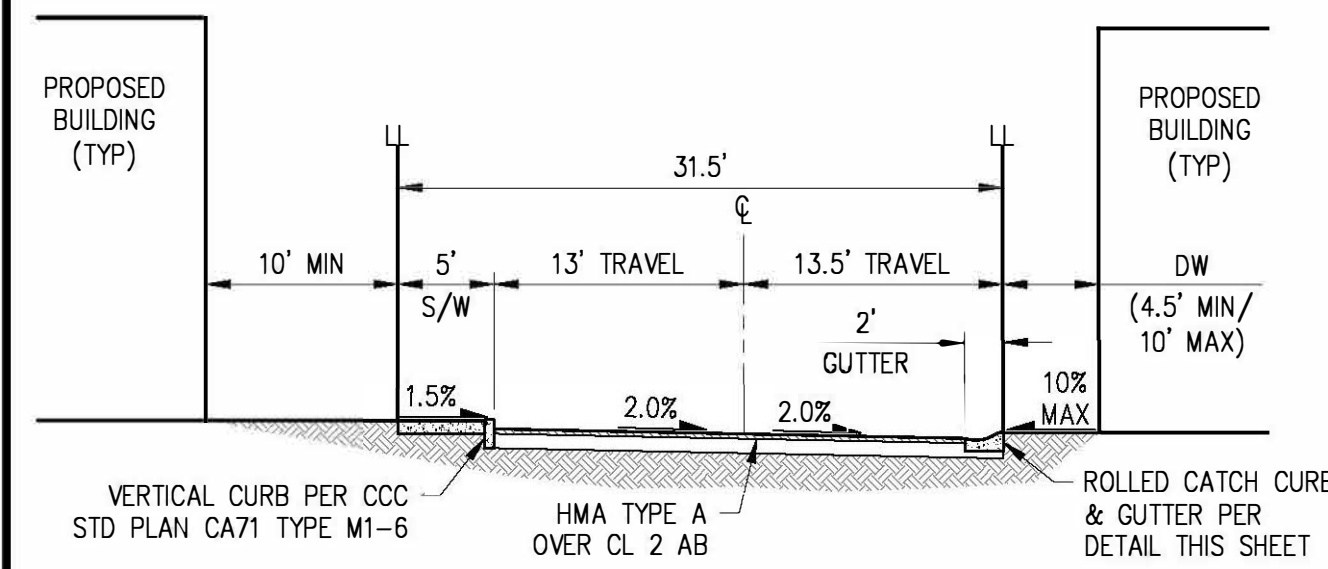
**B** TYPICAL 26.5' STREET  
(VERTICAL CURB & GUTTER AND ROLLED CURB & GUTTER)



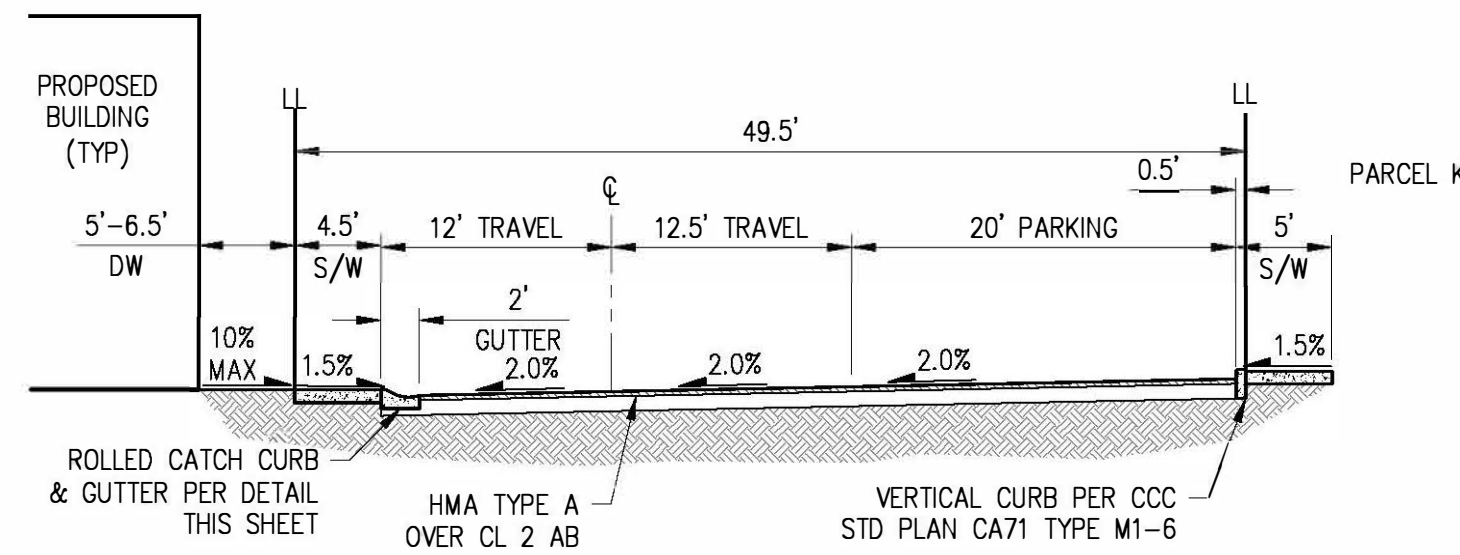
**C** TYPICAL 26' STREET WITH PARKING  
(VERTICAL CURB AND VALLEY GUTTER)



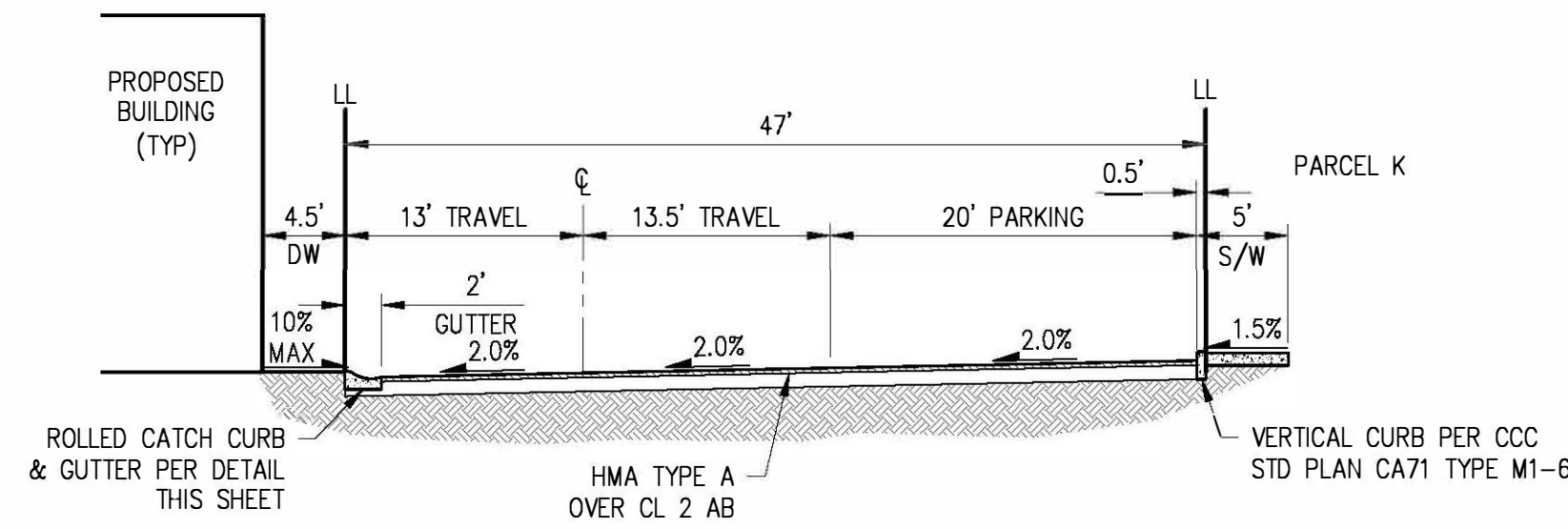
STREET KEY MAP  
NOT TO SCALE



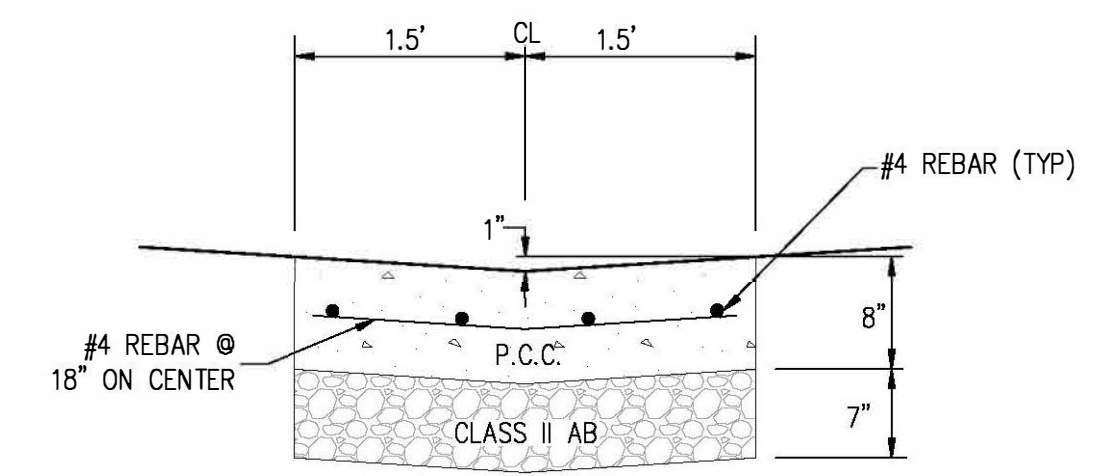
**D** TYPICAL 26.5' STREET  
(ROLLED CURB & GUTTER AND VERTICAL CURB)



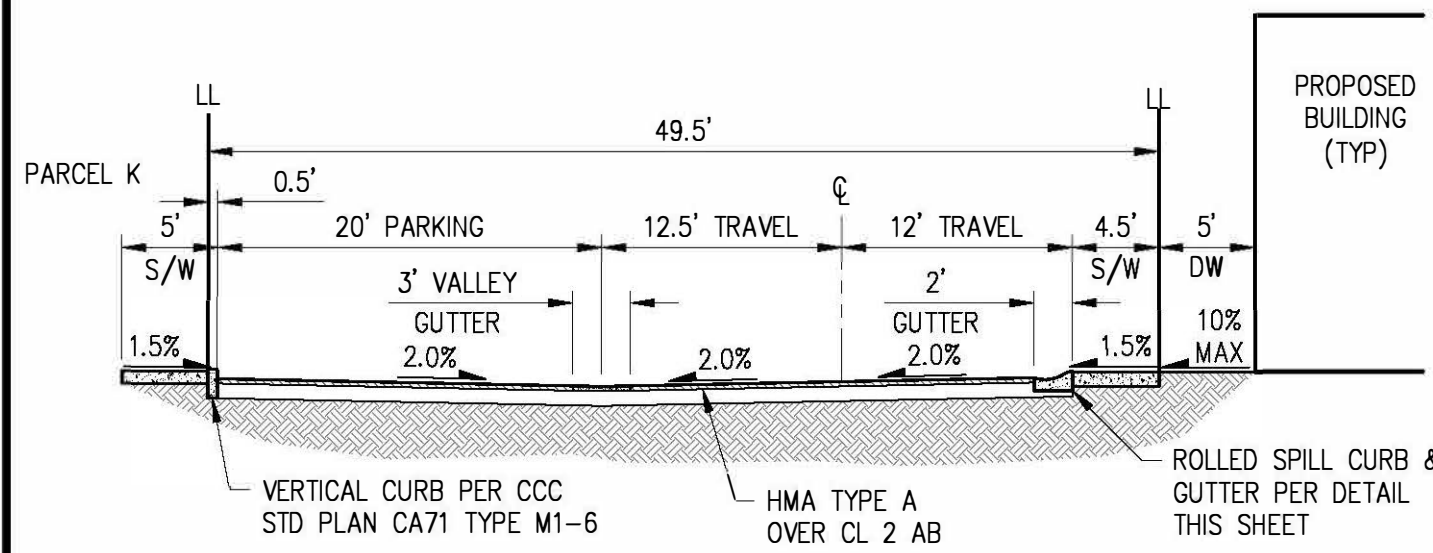
**E** TYPICAL 24.5' STREET WITH PARKING  
(ROLLED CURB & GUTTER AND VERTICAL CURB)



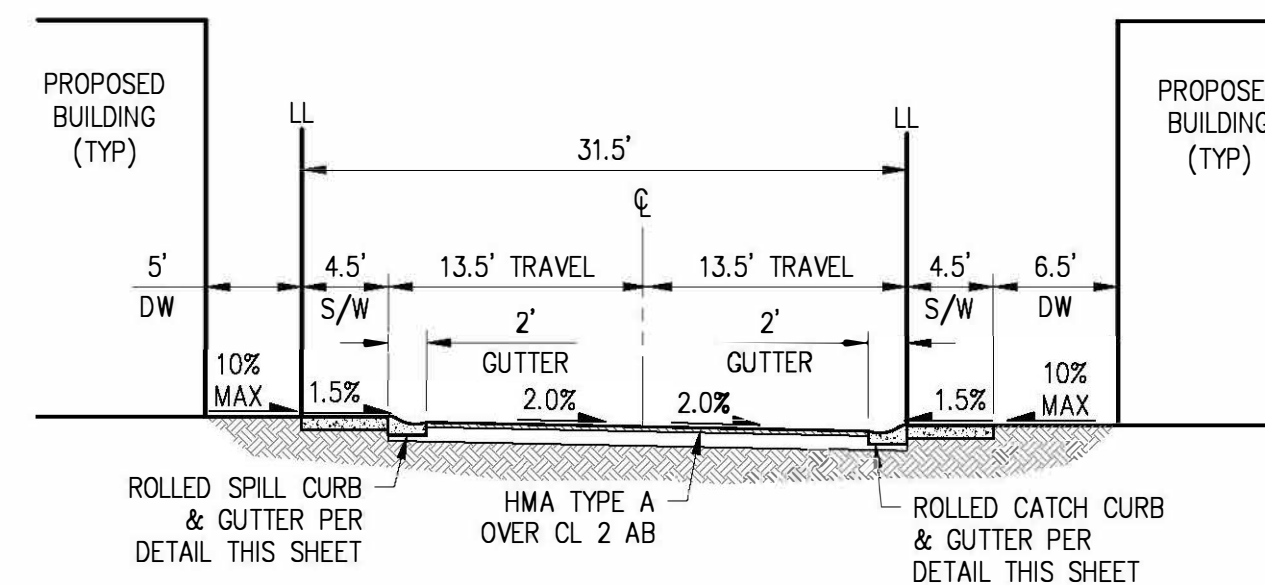
**F** TYPICAL 26.5' STREET WITH PARKING  
(ROLLED CURB & GUTTER AND VERTICAL CURB)



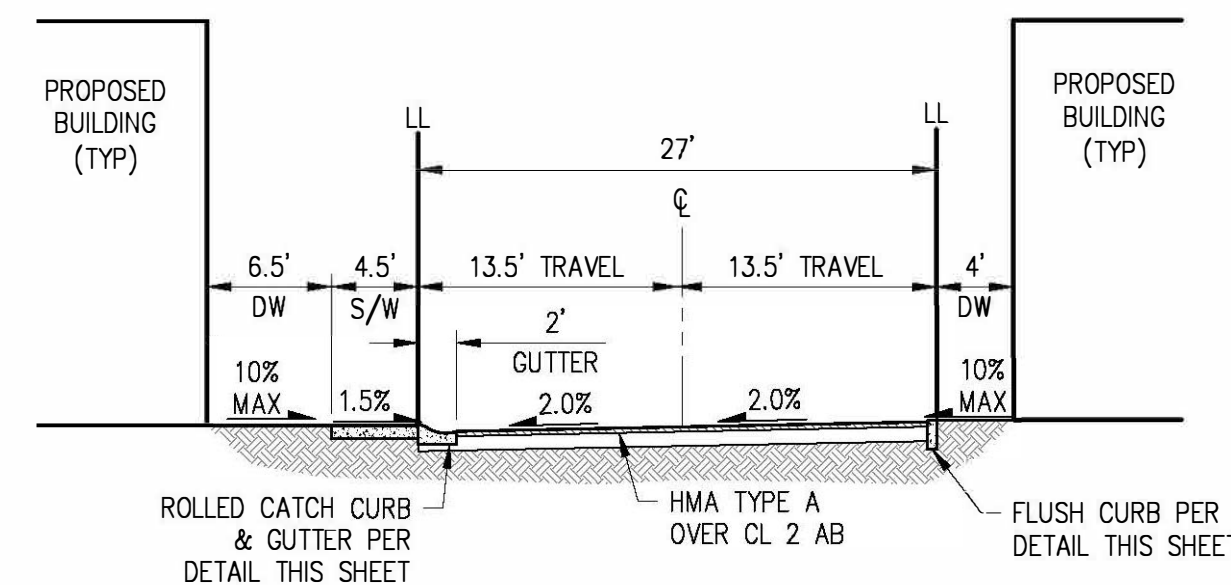
3' VALLEY GUTTER DETAIL  
NOT TO SCALE



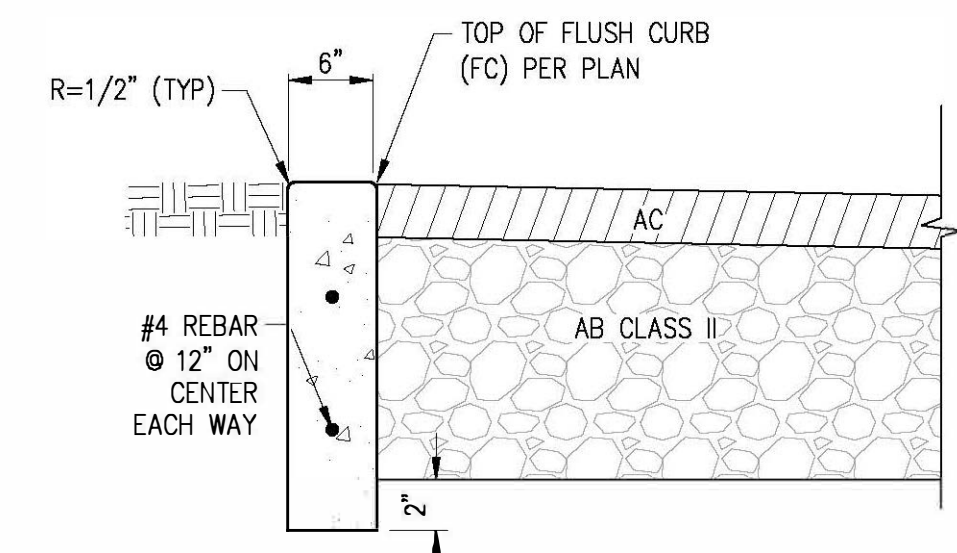
**G** TYPICAL 24.5' STREET WITH PARKING  
(ROLLED CURB & GUTTER AND VERTICAL CURB)



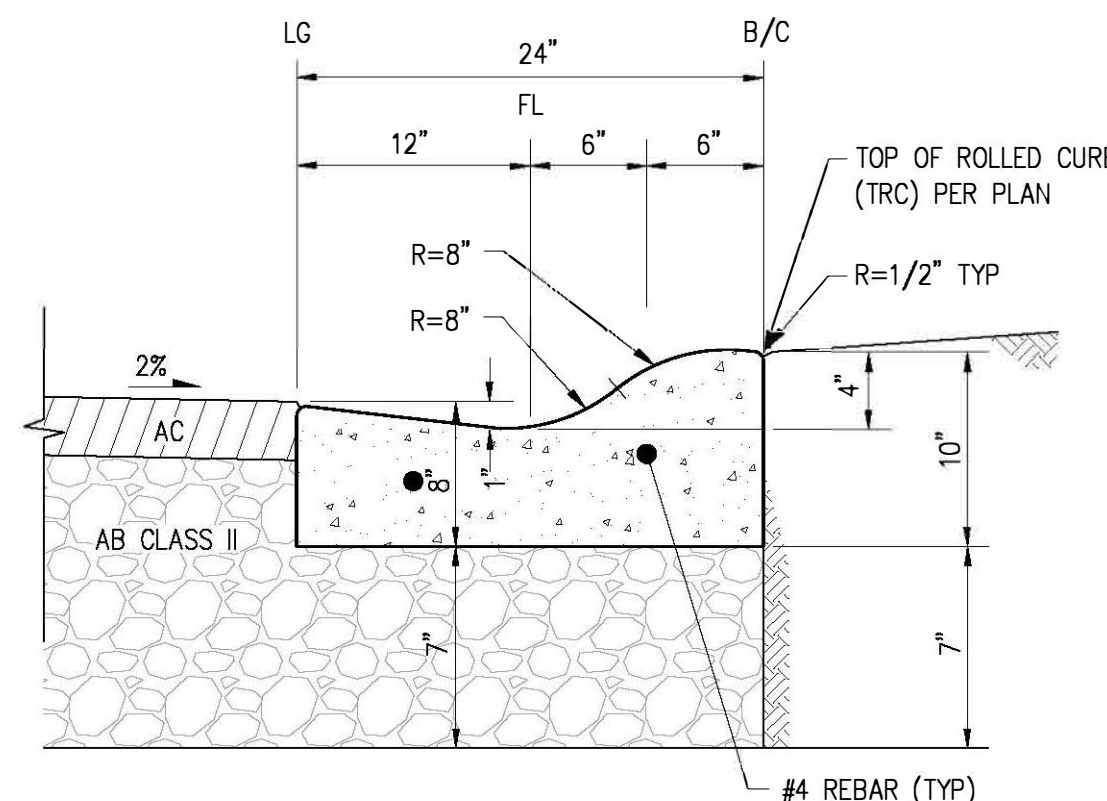
**H** TYPICAL 27' STREET  
(ROLLED CURB & GUTTER AND ROLLED CURB & GUTTER)



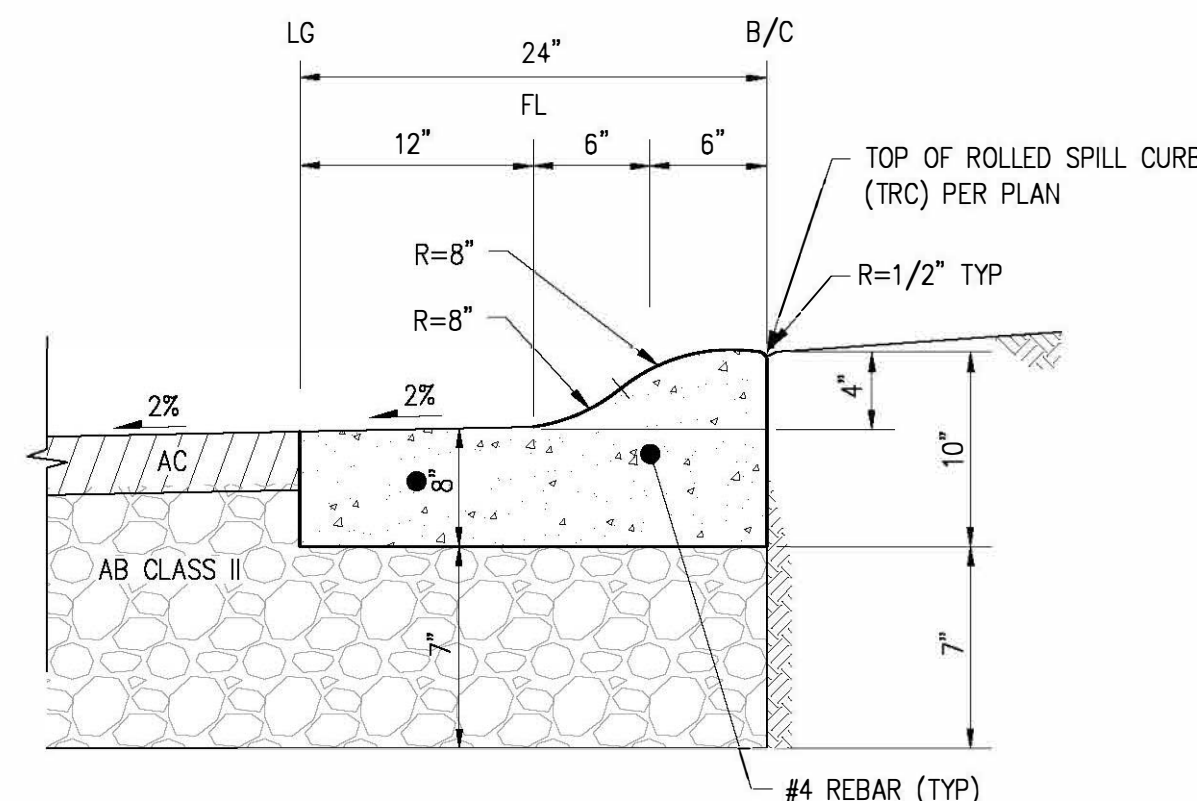
**I** TYPICAL 26.5' STREET  
(ROLLED CURB & GUTTER AND FLUSH CURB)



FLUSH CURB DETAIL  
NOT TO SCALE



ROLLED CATCH CURB DETAIL  
NOT TO SCALE

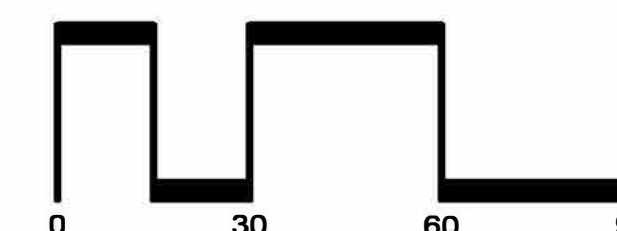
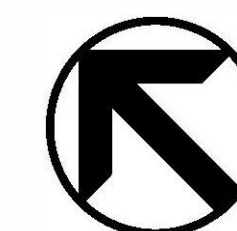


ROLLED SPILL CURB DETAIL  
NOT TO SCALE

# STREET SECTIONS & DETAILS

## WILDFLOWER STATION - SUBDIVISION 9601

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APN 052-232-016

WILDFLOWER STATION CONDOS

WILDFLOWER STATION PLACE

HILLCREST CROSSROADS

HILLCREST AVENUE

LEGEND

- EXISTING ADJACENT PROPERTY LINE
- PROPOSED PROJECT BOUNDARY
- PROPOSED STORM DRAIN
- STORM DRAIN FORCE MAIN
- EXISTING STORM DRAIN
- PROPOSED PAVEMENT
- EXISTING PAVEMENT TO REMAIN
- PROPOSED SIDEWALK/DRIVEWAY
- ACCESSIBLE UNIT
- ADA PATH OF TRAVEL
- BW BOTTOM OF WALL
- EG EXISTING GROUND
- FG FINISHED GROUND
- TW TOP OF WALL

EARTHWORK SUMMARY

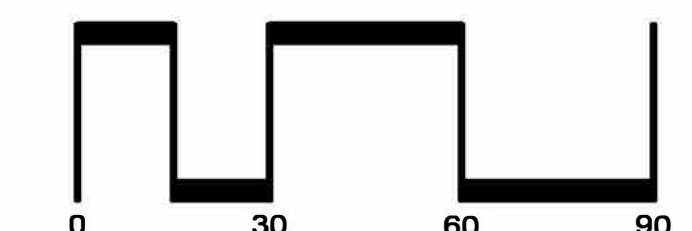
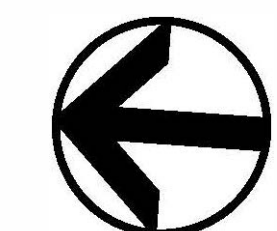
CUT	3,650 CY±
FILL	11,150 CY±
NET (FILL)	7,500 CY±

NOTES:

- DRIVEWAY APRON SLOPES ASSUMED TO BE 2-10%
- GARAGE SLOPES ASSUMED TO BE 1-2%
- FINISHED FLOOR TO BACK OF GARAGE STEP ASSUMED TO BE 0-12"
- FINISHED FLOOR TO PORCH STEP ASSUMED TO BE 0-6"
- PORCH SLOPE ASSUMED TO BE 2%
- HANDRAIL NEEDED AT ALL EXTERIOR STEPS EXCEEDING 1 RISER
- HANDRAIL EXTENSION ASSUMED 1' BEYOND TOP AND BOTTOM RISER
- PATHWAY GRADING ASSUMED NOT TO EXCEED 4.5%

PRELIMINARY GRADING PLAN  
WILDFLOWER STATION - SUBDIVISION 9601

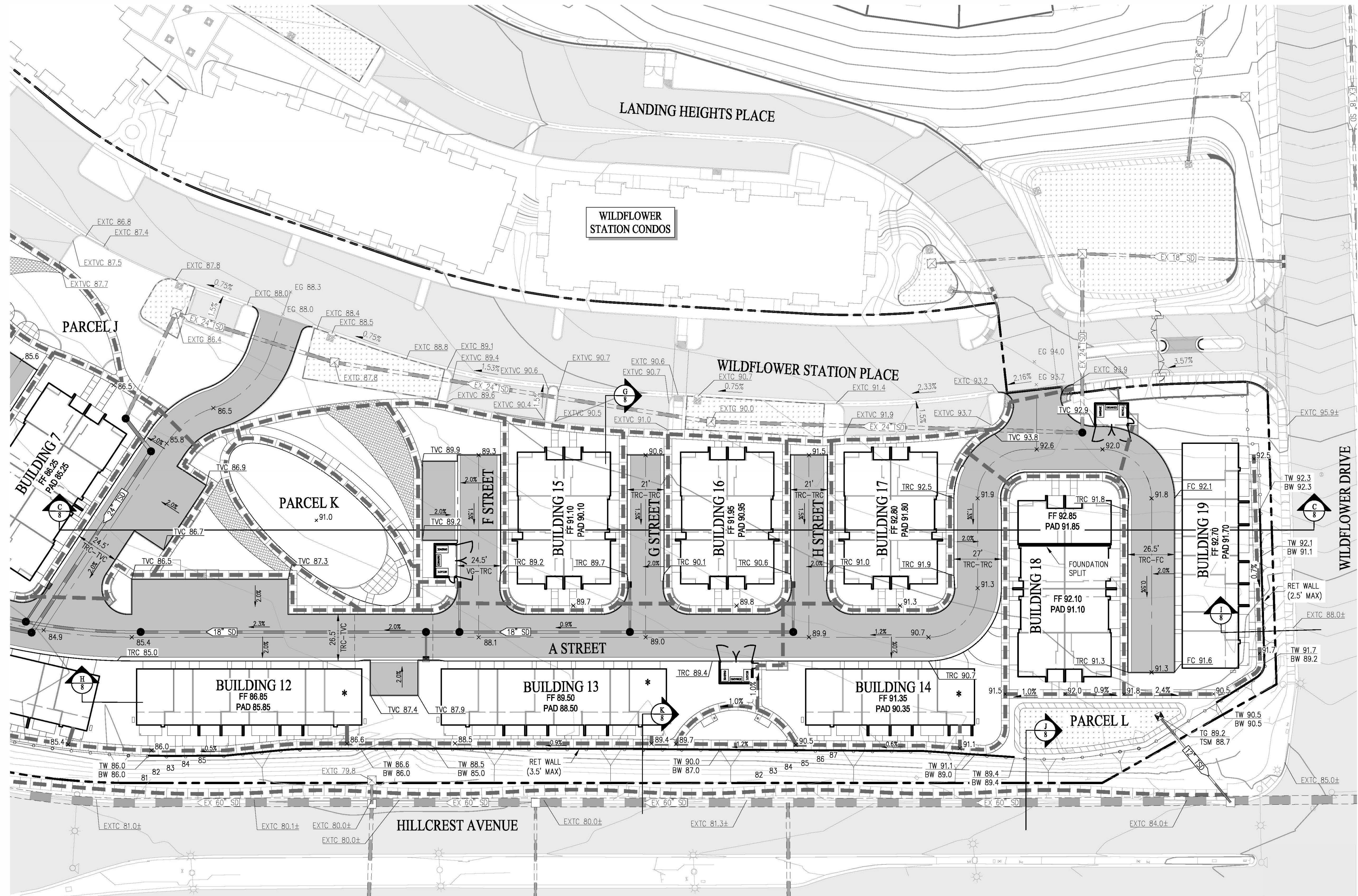
CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA  
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6  
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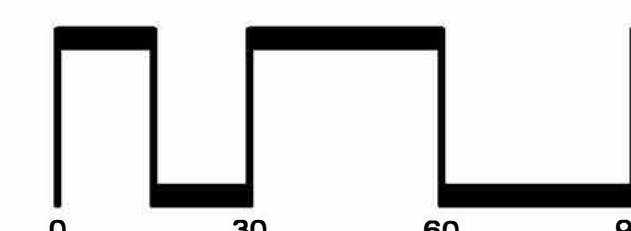
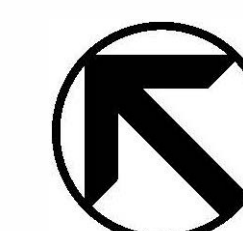


SEE SHEET 6

# PRELIMINARY GRADING PLAN

## WILDFLOWER STATION - SUBDIVISION 9601

CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA  
SCALE: 1"=30' DATE: FEBRUARY 2, 2024

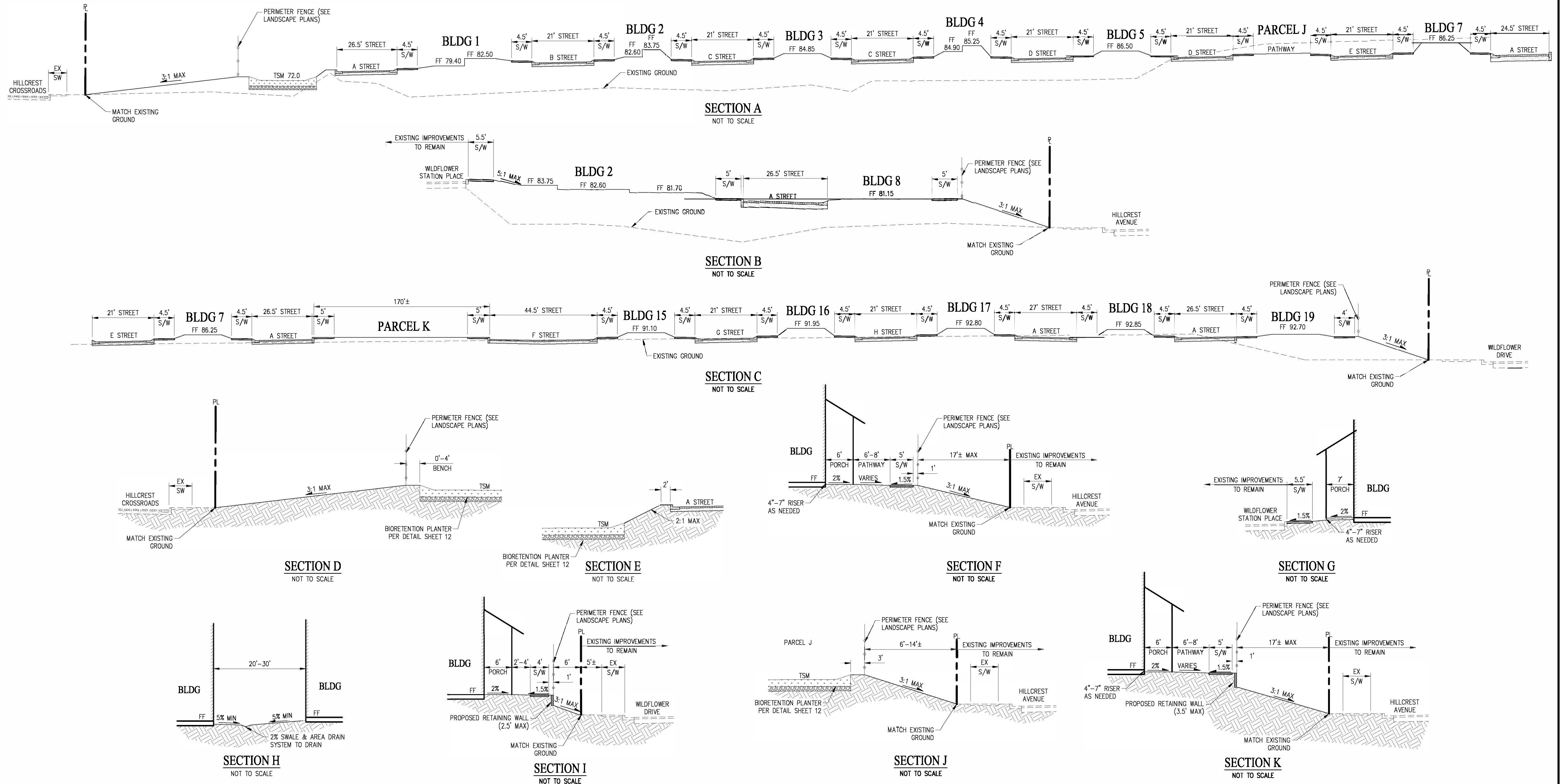


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**7**  
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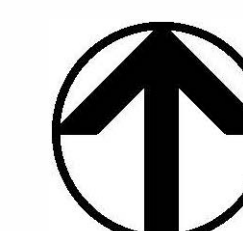




# GRADING SECTIONS

## WILDFLOWER STATION - SUBDIVISION 9601

CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA  
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APN 052-232-016

WILDFLOWER  
STATION CONDOS

WILDFLOWER STATION PLACE

PARCEL J

PARCEL K

PARCEL I

HILLCREST  
CROSSROADS

STORM DRAIN  
DETENTION VAULT

FLARED END CONNECTION TO BIORETENTION  
INV 18" IN 72.00

CONNECT TO EXISTING  
FIRE AND DOMESTIC  
WATER PIPES

CONNECT TO EXFL & RAISE TO GRADE  
TG 72.5  
EX INV 36" TSD IN 60.32  
EX INV 36" TSD OUT 60.32

STORM DRAIN MANHOLE  
ON EX SD PIPE

EXISTING WATER  
SERVICES TO REMAIN

BLDG 8

BLDG 9

BLDG 6

BLDG 11

BLDG 7

BLDG 5

BLDG 4

BLDG 3

BLDG 2

BLDG 1

### LEGEND

#### EXISTING

EX SD  
EX TSD  
EX SS  
EX W  
EX FS  
EX JT  
EX GAS

#### PROPOSED

SD  
TSD  
SDFM  
SS  
W  
FW  
JT

STORM DRAIN LINE  
TREATED STORM DRAIN LINE  
STORM DRAIN FORCE MAIN LINE  
SANITARY SEWER LINE  
WATER LINE  
FIRE SERVICE LINE  
JOINT TRENCH  
GAS PIPE  
MANHOLE  
CATCH BASIN  
FIELD INLET  
STORM DRAIN LOW FLOW PUMP  
FIRE HYDRANT  
TRANSFORMER

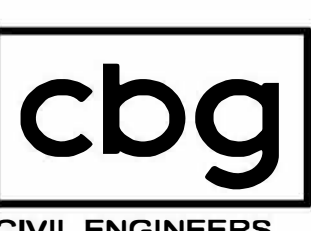
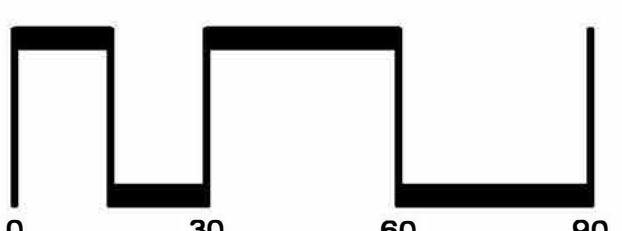
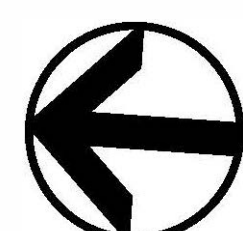
### NOTES:

1. ALL ON-SITE UTILITIES TO BE PRIVATE.

# PRELIMINARY UTILITY PLAN WILDFLOWER STATION - SUBDIVISION 9601

CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA

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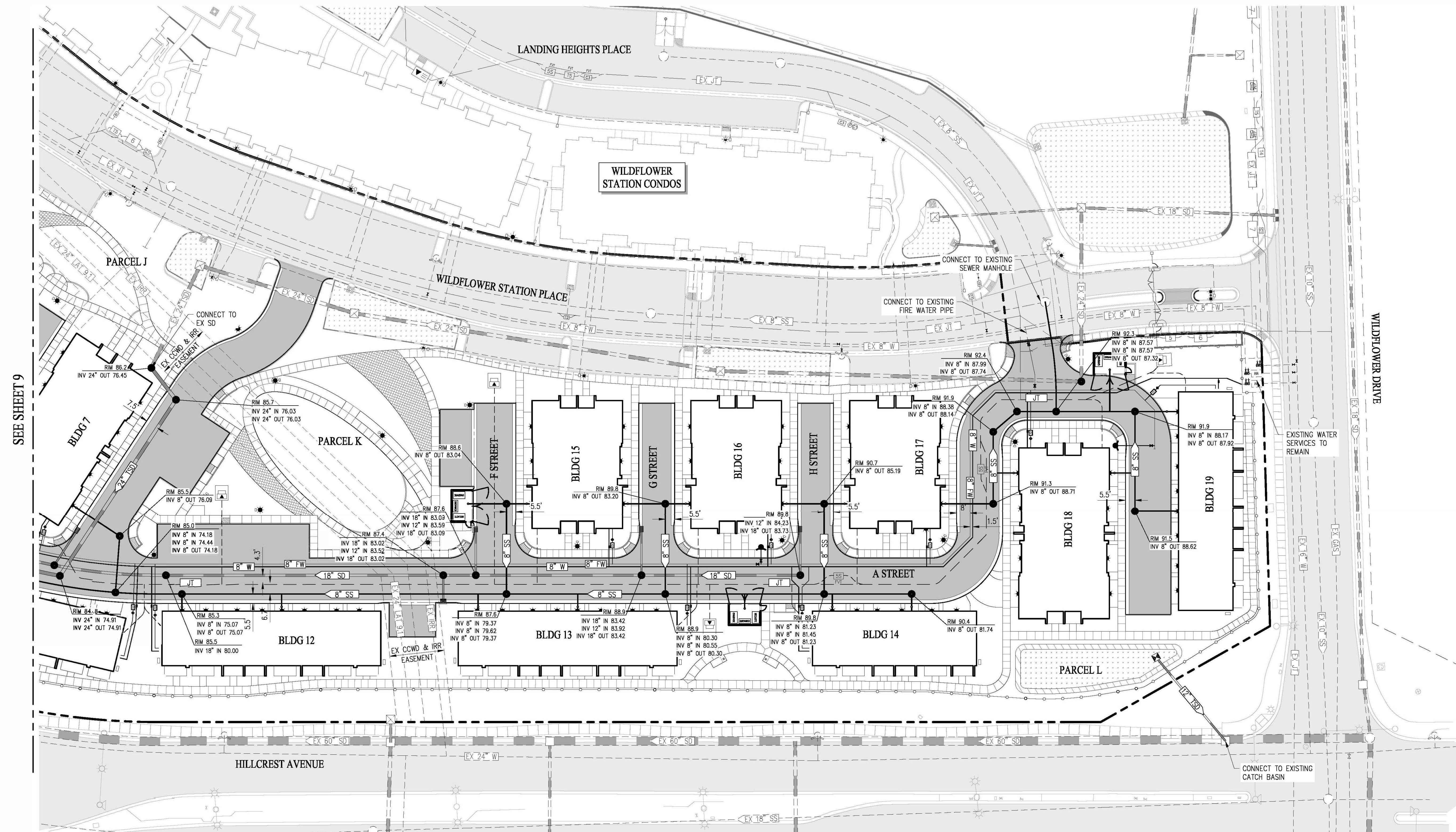
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**9**  
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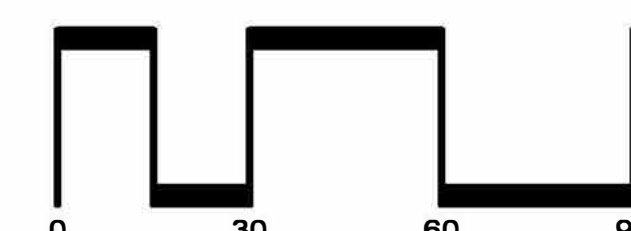
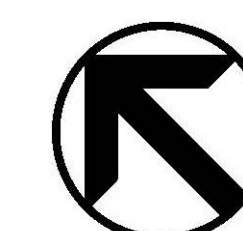


# PRELIMINARY UTILITY PLAN

## WILDFLOWER STATION - SUBDIVISION 9601

CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA

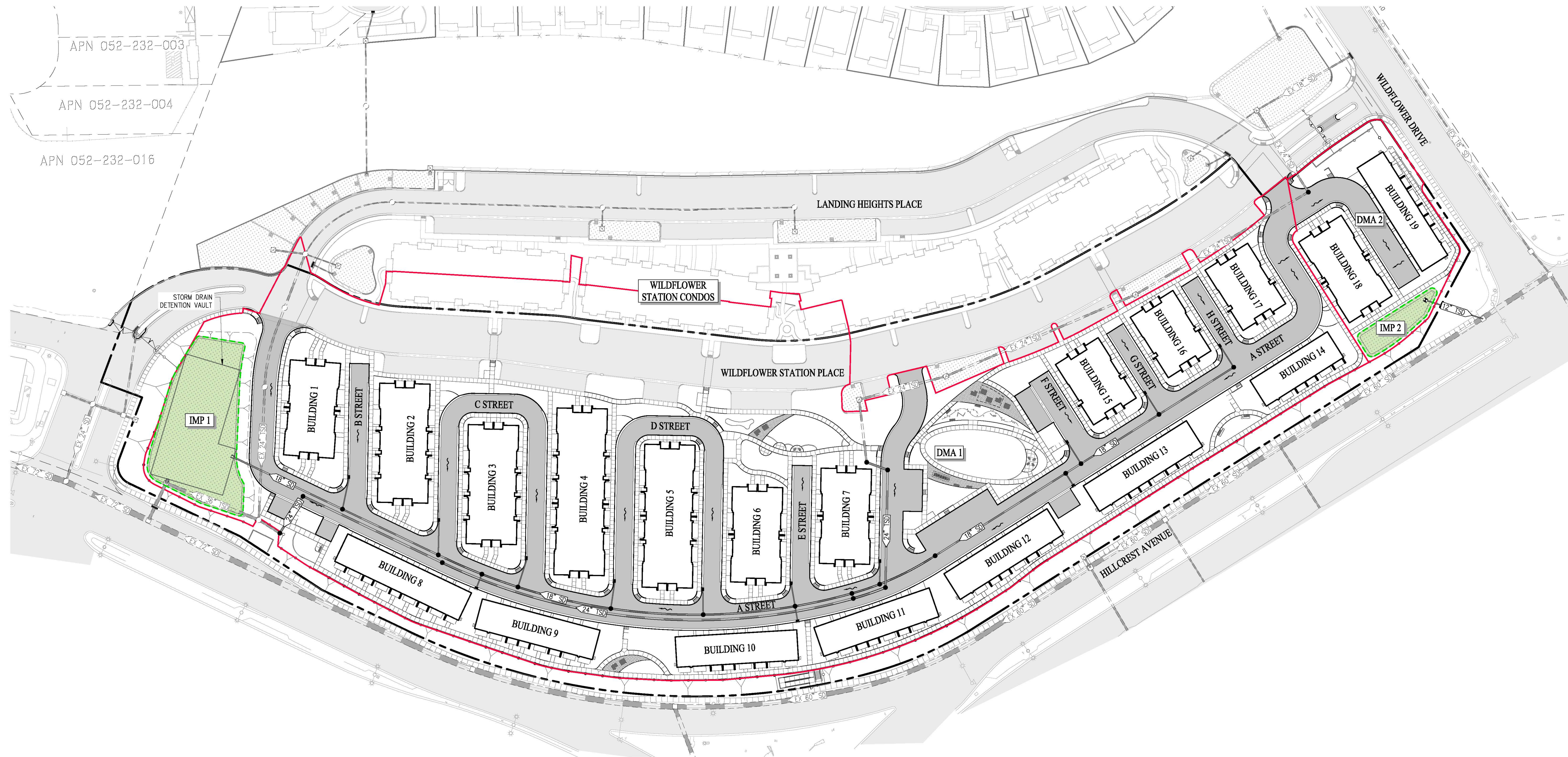
SCALE: 1"=30' DATE: FEBRUARY 2, 2024



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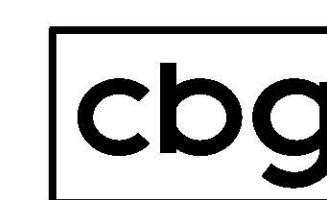
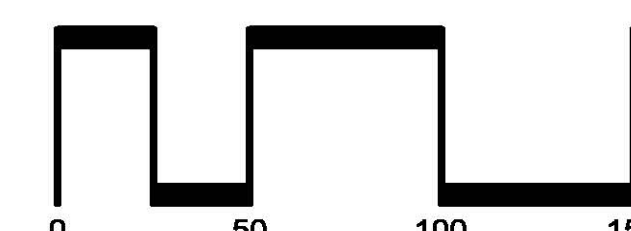
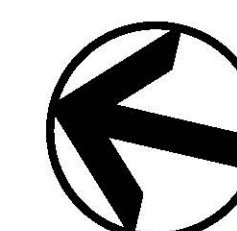
LEGEND

- DMA BOUNDARY
- PROPOSED BIORETENTION
- PROPOSED STORM DRAIN LINE
- PROPOSED TREATED STORM DRAIN LINE
- EXISTING STORM DRAIN LINE

# PRELIMINARY STORMWATER CONTROL PLAN

## WILDFLOWER STATION - SUBDIVISION 9601

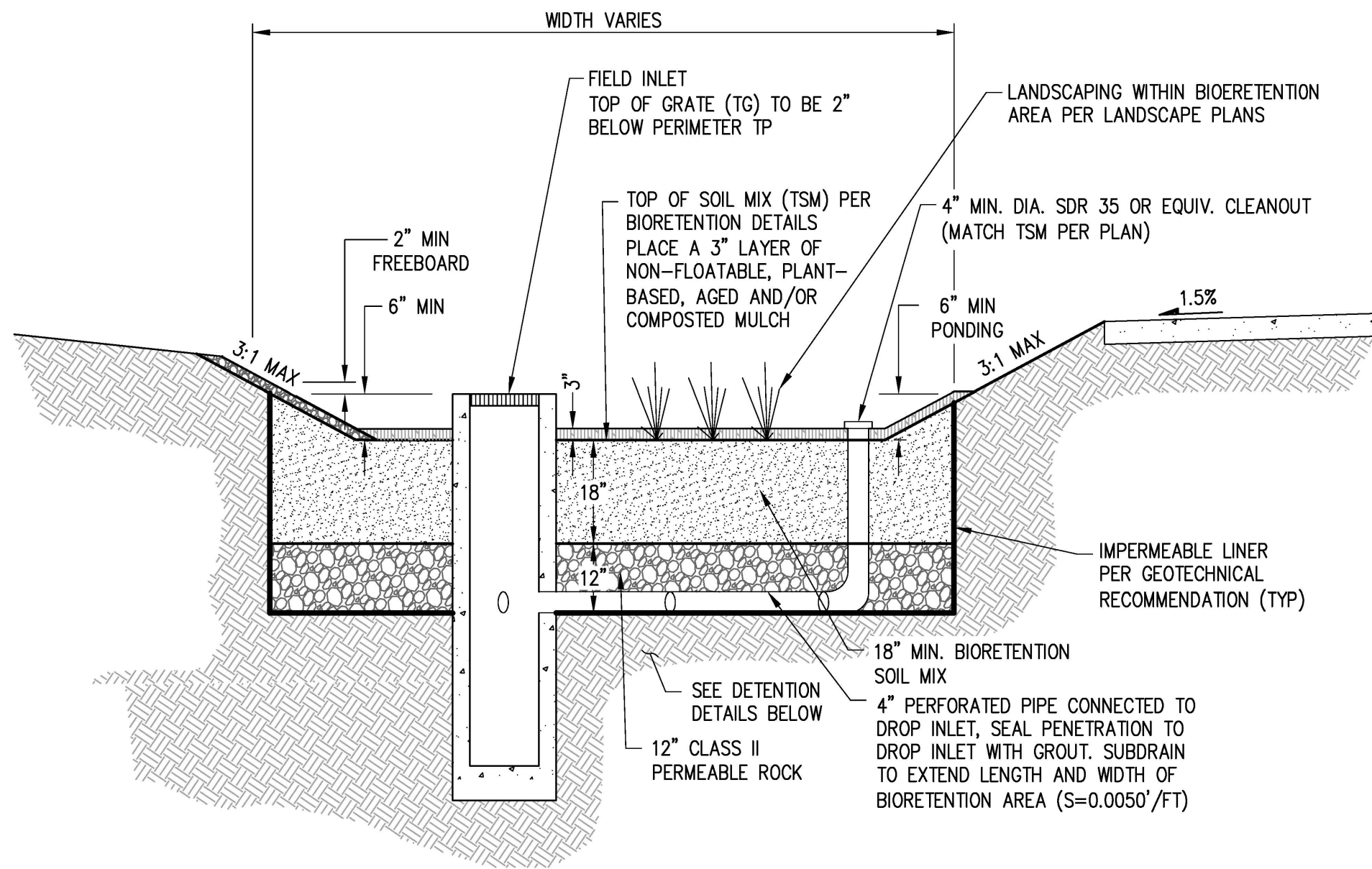
CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA  
SCALE: 1"=50' DATE: JUNE 27, 2025



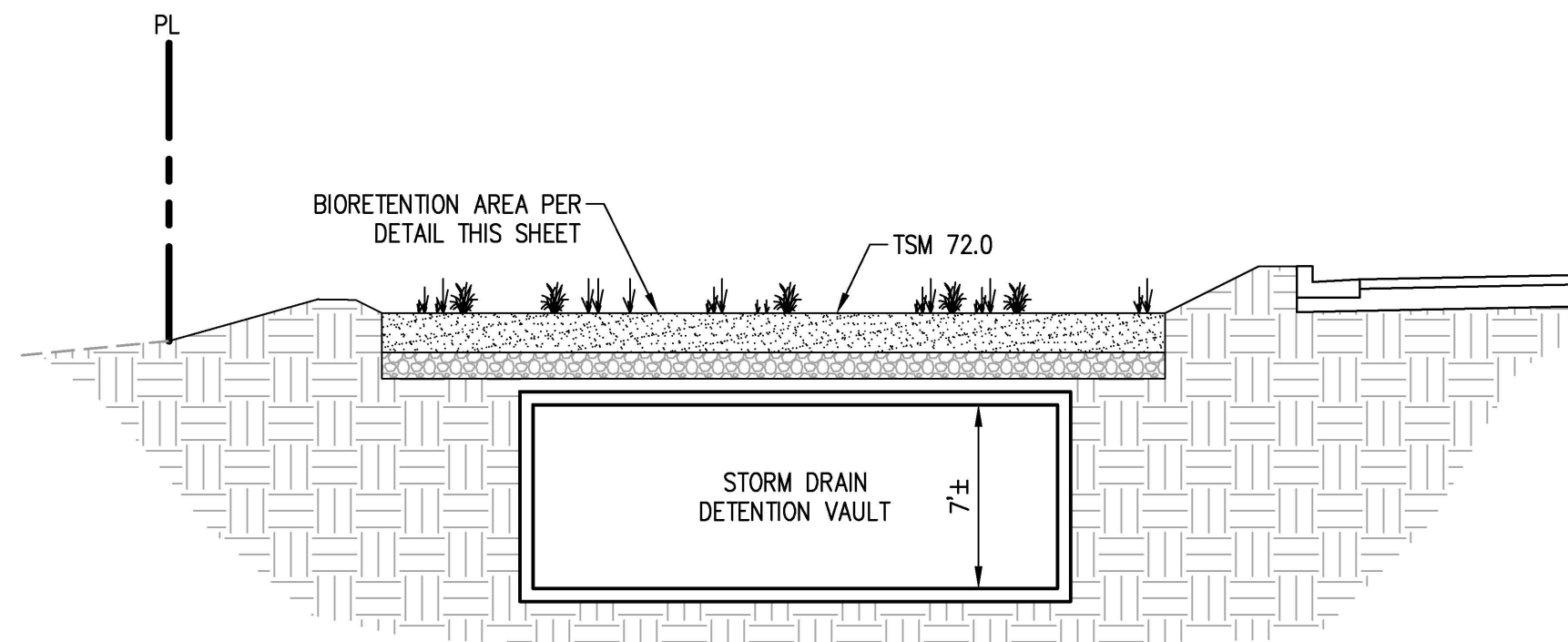
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SHEET NO.  
**11**  
OF 17 SHEETS

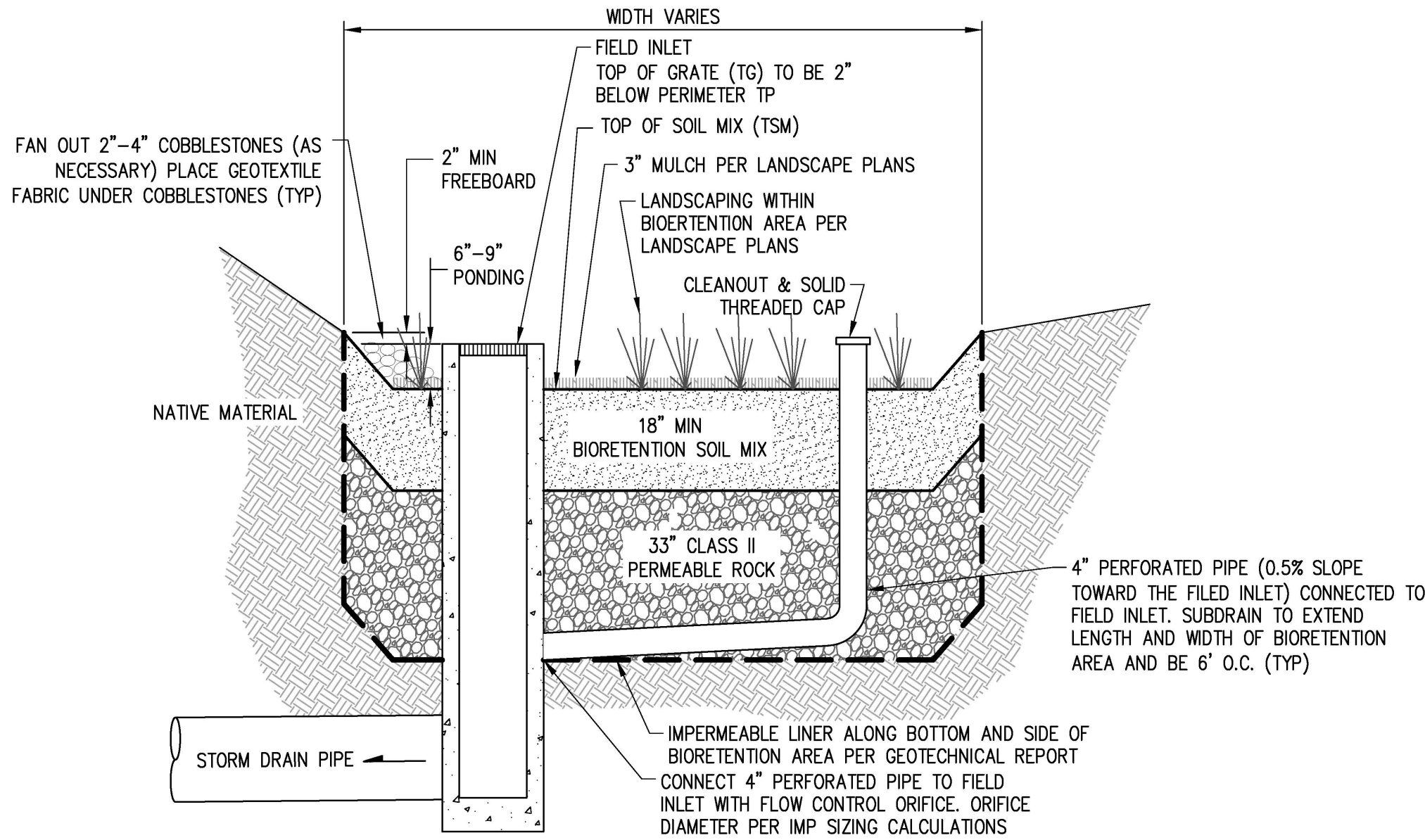




BIORETENTION AREA  
(IMP 1)  
NOT TO SCALE



DETENTION BELOW BIORETENTION AREA  
(IMP 1)  
NOT TO SCALE



BIORETENTION AREA IN LANDSCAPE AREA  
(IMP 2)  
NOT TO SCALE

Project Name: Wildflower Townhomes  
Project Type: Treatment and Flow Control  
APN: 052-0140-013 Thru 16  
Drainage Area: 398,146  
Mean Annual Precipitation: 13.6

IV. Areas Draining to IMPs

IMP Name: IMP1  
IMP Type: Bioretention + Vault  
Soil Group: IMP1

DMA Name	Area (sq ft)	Post Project Surface Type	DMA Runoff Factor	DMA Area x Runoff Factor	IMP Sizing	Rain Adjustment Factor	Minimum Area or Volume	Proposed Area or Volume
DMA 1A	112,910	Conventional Roof	1.00	112,910	IMP Sizing Factor	1.000	12,770	12,826
DMA 1B	168,268	Concrete or Asphalt	1.00	168,268				
DMA 1C	76,168	Landscape	0.50	38,084				
Total				319,262				
Area				0.040	IMP Sizing Factor	1.242	60,272	60,868
Volume				0.152				
							Maximum Underdrain Flow (cfs) Orifice Diameter (in)	0.29
								2.48

IMP Name: IMP2  
IMP Type: Bioretention Facility  
Soil Group: IMP2

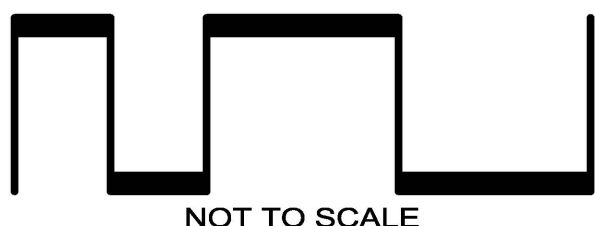
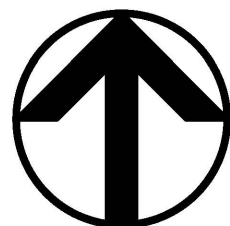
DMA Name	Area (sq ft)	Post Project Surface Type	DMA Runoff Factor	DMA Area x Runoff Factor	IMP Sizing	Rain Adjustment Factor	Minimum Area or Volume	Proposed Area or Volume
DMA 2A	10,950	Conventional Roof	1.00	10,950	IMP Sizing Factor	1.242	1,689	1,691
DMA 2B	8,100	Concrete or Asphalt	1.00	8,100				
DMA 2C	7,233	Landscape	0.50	3,617				
Total				22,867				
Area				0.060	IMP Sizing Factor	1.242	1,408	1,410
Surface Volume				0.050				
Subsurface Volume				0.066			1,858	1,861
							Maximum Underdrain Flow (cfs) Orifice Diameter (in)	0.02
								0.94

IMP CALCULATOR OUTPUT  
NOT TO SCALE

# STORMWATER DETAILS

## WILDFLOWER STATION - SUBDIVISION 9601

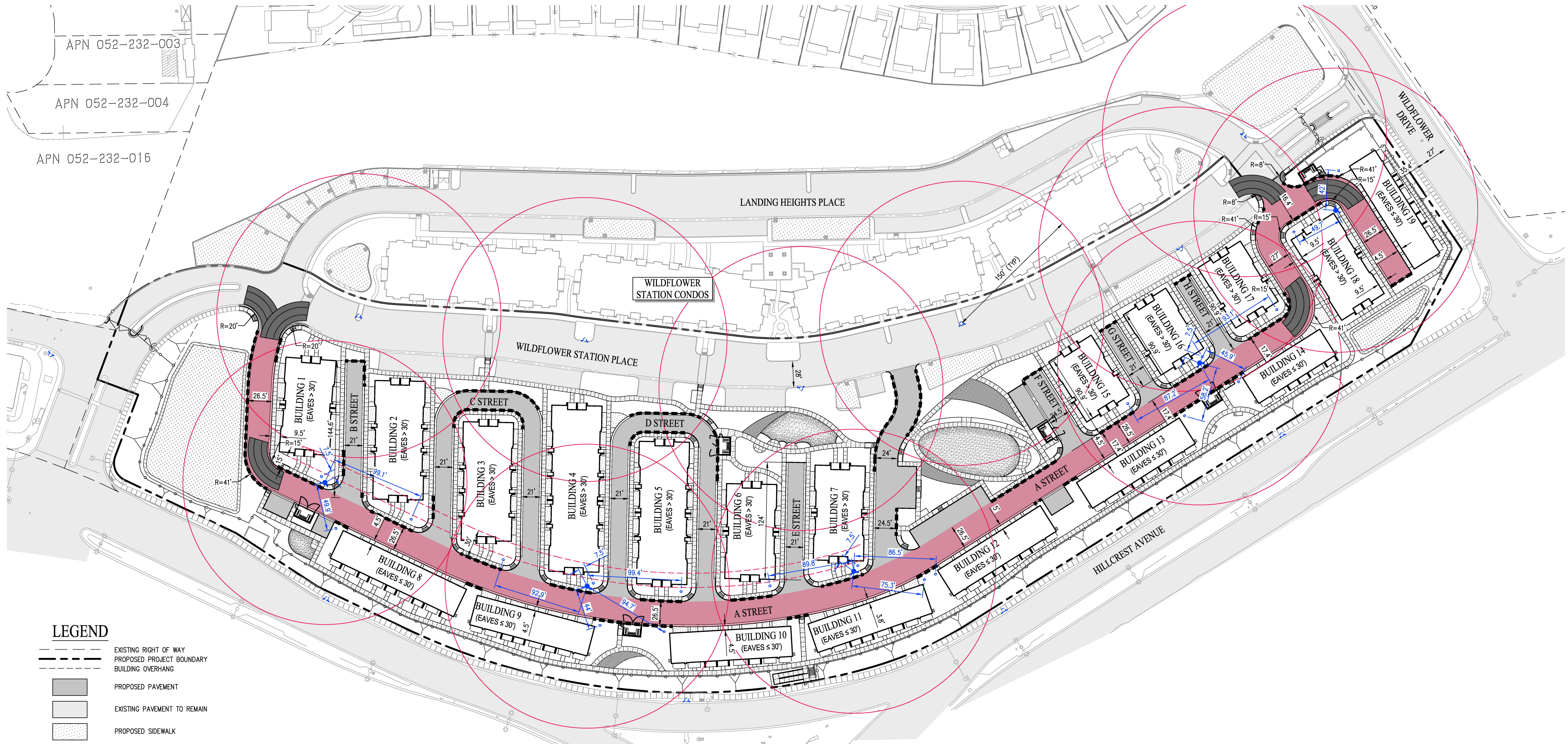
CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA  
SCALE: NTS DATE: JUNE 27, 2025



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SHEET NO.  
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OF 17 SHEETS



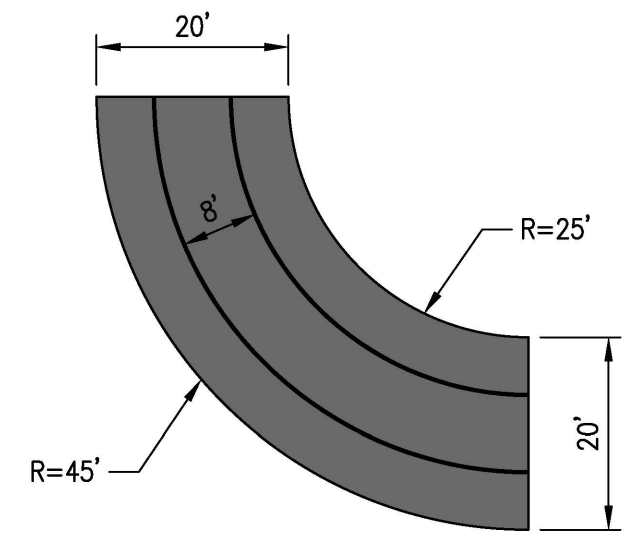


LEGEND

- EXISTING RIGHT OF WAY
- PROPOSED PROJECT BOUNDARY
- BUILDING OVERHANG
- PROPOSED PAVEMENT
- EXISTING PAVEMENT TO REMAIN
- PROPOSED SIDEWALK
- 26' FIRE LANE
- EXISTING FIRE HYDRANT
- PROPOSED FIRE HYDRANT
- PROPOSED FIRE DEPARTMENT CONNECTION (FDC)
- RED CURB PAINTED WITH THE WORDS "NO PARKING - FIRE LANE"

NOTES:

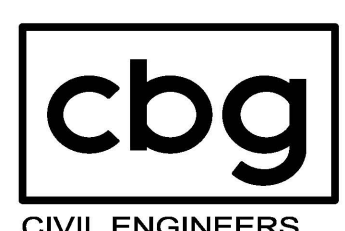
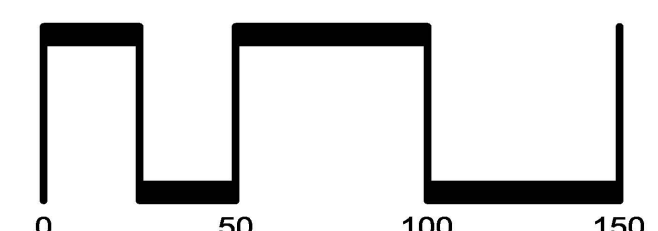
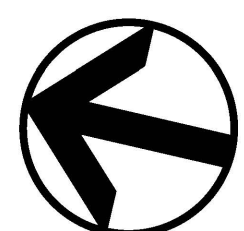
- BUILDING EAVE HEIGHT IS PER THE ARCHITECTURAL DRAWINGS AND NOTED HERE IN. FOR BUILDINGS WITH THE EAVE HEIGHT GREATER THAT 30', AN AERIAL FIRE APPARATUS ACCESS ROAD IS LOCATED NOT LESS THAN 15 FEET AND NOT GREATER THAN 30 FEET FROM EACH BUILDING AND IS POSITIONED PARALLEL TO ONE SIDE OF EACH BUILDING PER THE CA FIRE CODE. REFER TO ARCHITECTURAL DRAWINGS FOR MORE DETAILED INFORMATION.
- ACCESS ROADWAYS OF LESS THAN 28' UNOBSTRUCTED WIDTH SHALL HAVE SIGNS POSTED OR RED CURBS PAINTED WITH THE WORDS "NO PARKING - FIRE LANE" CLEARLY MARKED.
- LADDER PADS ARE TO BE CLEAR OF OBSTRUCTIONS AND LOCATED WITH FINAL BUILDING PERMIT PLANS AND SITE CONSTRUCTION DOCUMENTS.



CCCFPD FIRE ACCESS TURNING TEMPLATE  
NOT TO SCALE

FIRE ACCESS PLAN  
WILDFLOWER STATION - SUBDIVISION 9601

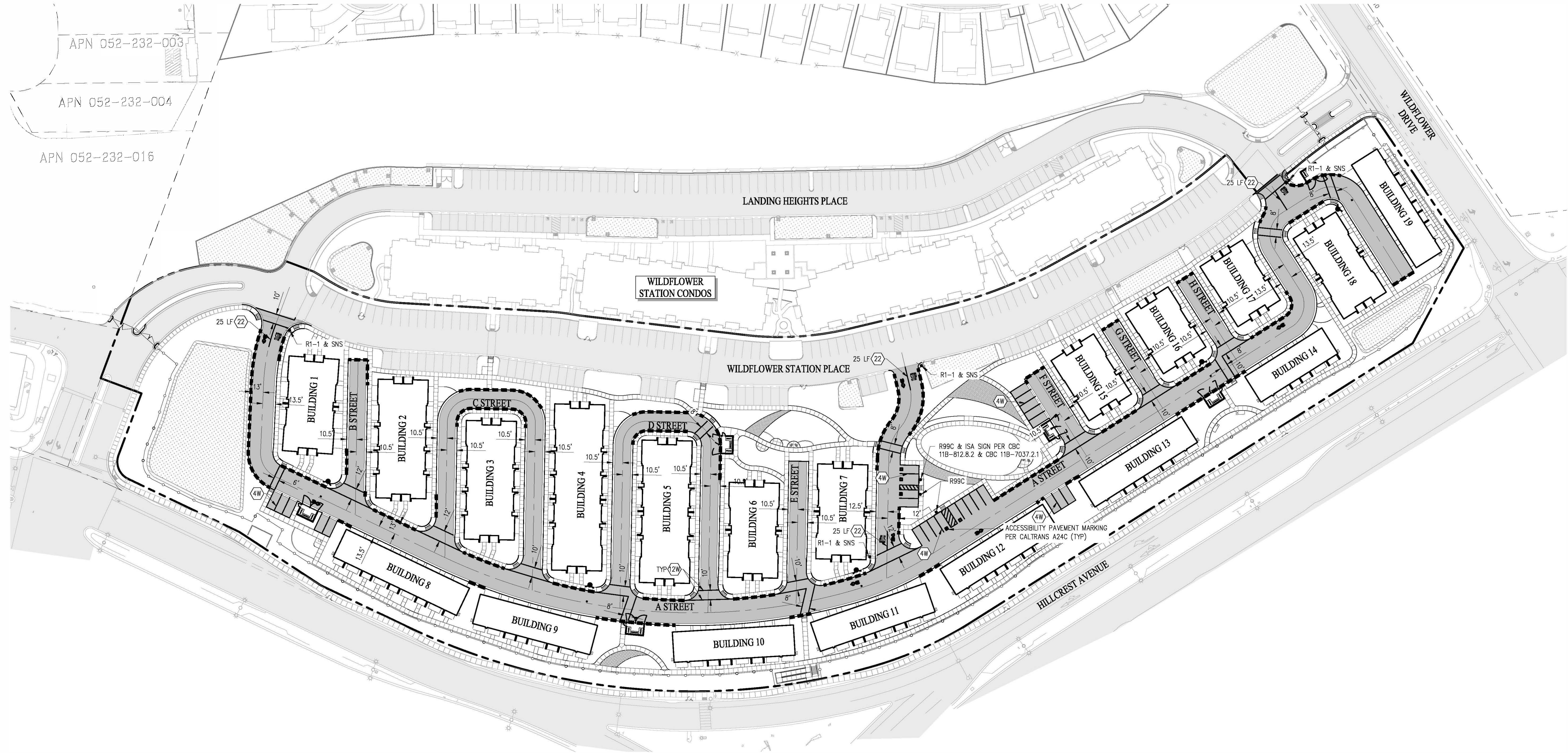
CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA  
SCALE: 1"=50' DATE: FEBRUARY 2, 2024



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SHEET NO.  
13  
OF 17 SHEETS



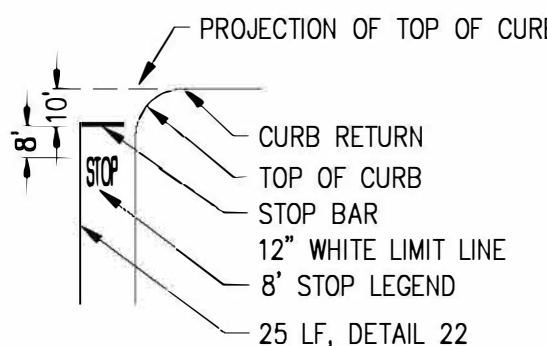


LEGEND

	STOP SIGN
	NO PARKING ANYTIME SIGN
	ACCESSIBLE PARKING ONLY - MINIMUM FINE \$250 SIGN
	STREET NAME SIGN PER COUNTY STANDARD.
	DETAIL 22 - DOUBLE YELLOW STRIPE PER CALTRANS STD. PLANS
	4" WHITE STRIPE
	12" WHITE STRIPE
	EXISTING FIRE HYDRANT
	PROPOSED FIRE HYDRANT
	RED CURB PAINTED WITH THE WORDS "NO PARKING - FIRE LANE"
	FIRE HYDRANT MARKER
	BIKE LANE SHARROW PAVEMENT MARKINGS PER CALIFORNIA MUTCD

STRIPING NOTES

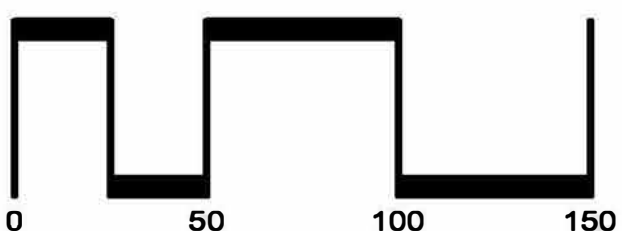
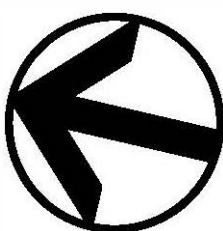
- ALL STRIPING, PAVEMENT MARKING, AND SIGNING SHALL CONFORM TO THE LATEST EDITION OF THE CALTRANS CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CA MUTCD) AND MOST CURRENT STANDARD DETAILS AND SPECIFICATIONS.
- ALL DETAIL NUMBERS REFER TO CALTRANS STANDARD PLANS STRIPING DETAILS.



STRIPING DETAIL

SIGNING & STRIPING PLAN  
WILDFLOWER STATION - SUBDIVISION 9601

CITY OF ANTIOCH    CONTRA COSTA COUNTY    CALIFORNIA  
SCALE: 1"=50'    DATE: FEBRUARY 2, 2024



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SHEET NO.  
**14**  
OF 17 SHEETS



APN 052-232-016

WILDFLOWER  
STATION CONDOS

WILDFLOWER STATION PLACE  
(PRIVATE STREET)

PARCEL J  
NORTHERN  
COMMON AREA

PARCEL I

HILLCREST  
CROSSROADS  
(PUBLIC R/W)

B STREET  
(PRIVATE STREET)

C STREET  
(PRIVATE STREET)

D STREET  
(PRIVATE STREET)

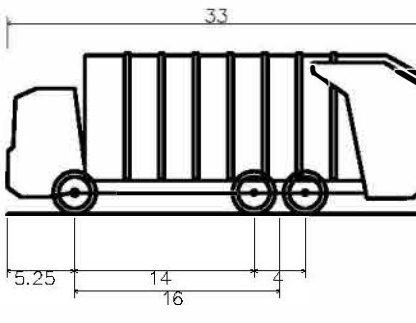
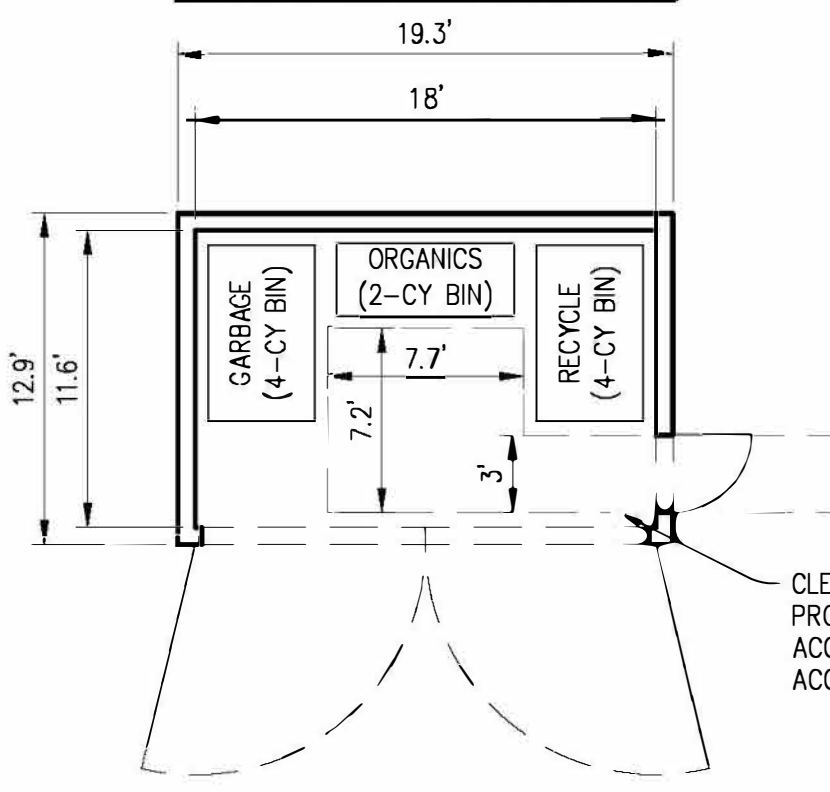
E STREET  
(PRIVATE STREET)

PASSIVE PARK

A STREET  
(PRIVATE STREET)

HILLCREST AVENUE  
(PUBLIC R/W)

REFUSE, RECYCLABLES, & GREEN  
WASTE COLLECTION ENCLOSURE



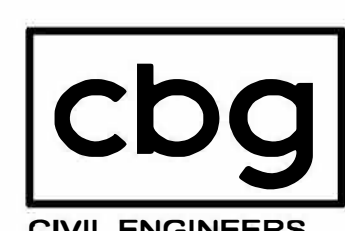
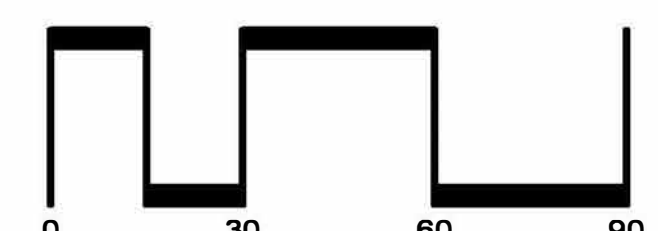
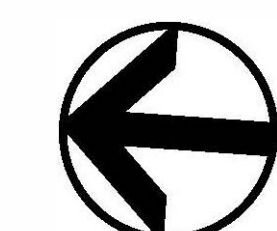
REPUBLIC SERVICES GARBAGE TRUCK  
Overall Length 33.000ft  
Overall Width 9.000ft  
Overall Body Height 12.558ft  
Min Body Ground Clearance 1.227ft  
Track Width 8.500ft  
Lock-to-lock time 6.00s  
Curb to Curb Turning Radius 37.500ft

### NOTE

THE TRUCK TURNING MOVEMENTS ON THE PLAN SHOW THE TRUCK WHEEL MAY BE WITHIN THE GUTTER (REPRESENTED BY THE LINE NEXT TO THE GREY HATCH), BUT THE TRUCK WILL NOT BE DRIVING OVER THE CURB.

# WASTE COLLECTION PLAN WILDFLOWER STATION - SUBDIVISION 9601

CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA  
SCALE: 1"=30' DATE: FEBRUARY 2, 2024

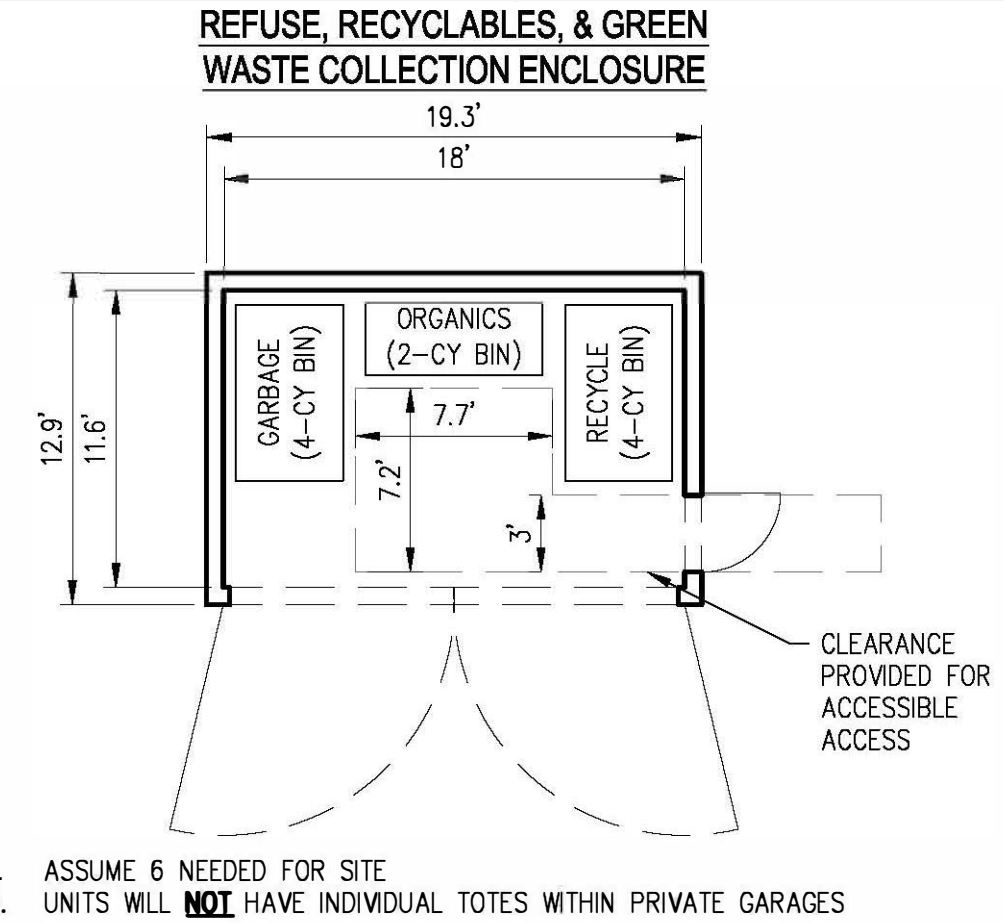
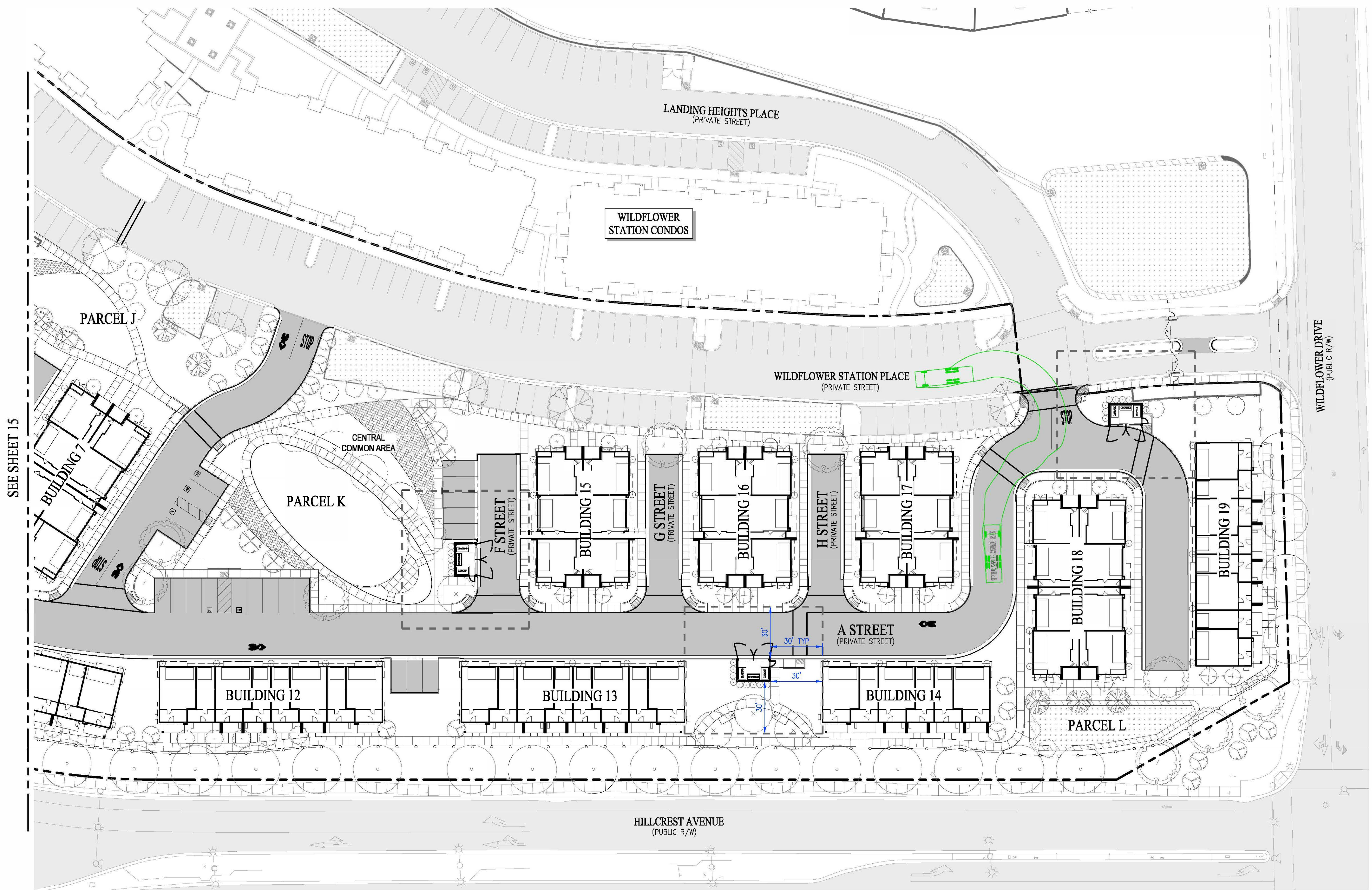


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SHEET NO.  
**15**  
OF 17 SHEETS

1. ASSUME 6' NEEDED FOR SITE
2. UNITS WILL **NOT** HAVE INDIVIDUAL TOTES WITHIN PRIVATE GARAGES

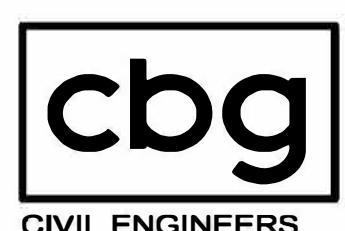
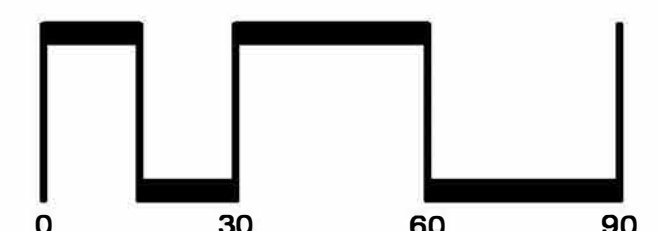
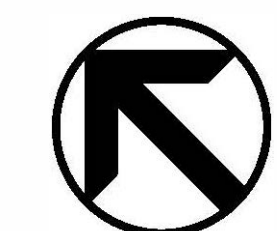




# WASTE COLLECTION PLAN

## WILDFLOWER STATION - SUBDIVISION 9601

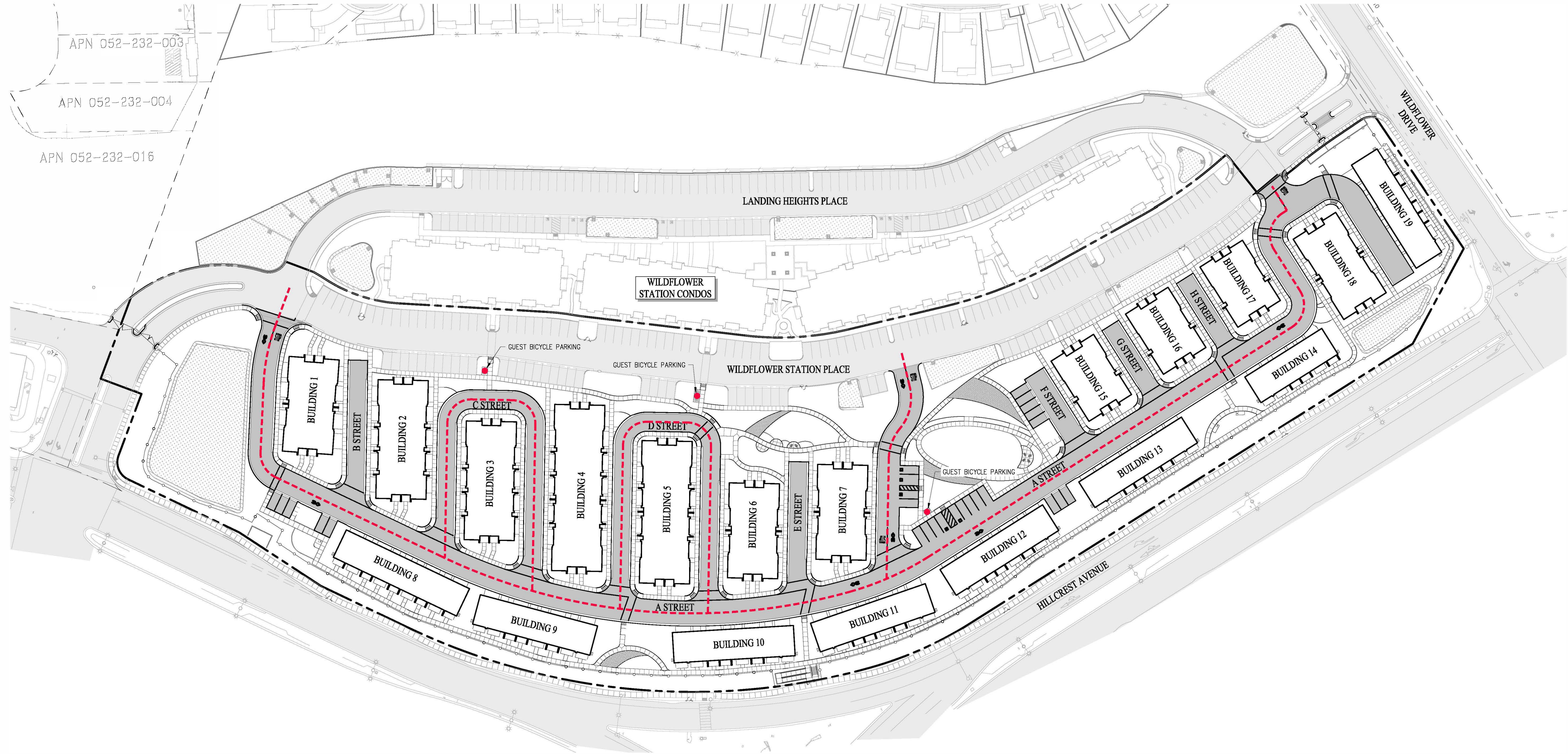
CITY OF ANTIOCH CONTRA COSTA COUNTY CALIFORNIA  
SCALE: 1"=30' DATE: FEBRUARY 2, 2024





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SHEET NO.  
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OF 17 SHEETS





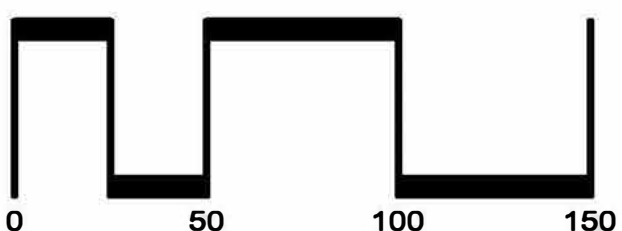
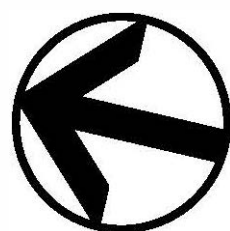
**LEGEND**

-  CLASS III SHARED BIKE LANE
-  GUEST BICYCLE PARKING  
(SEE LANDSCAPE PLANS FOR TYPE & QUANTITY)

# BIKE CIRCULATION PLAN

## WILDFLOWER STATION - SUBDIVISION 9601

CITY OF ANTIOCH    CONTRA COSTA COUNTY    CALIFORNIA  
SCALE: 1"=50'    DATE: FEBRUARY 2, 2024

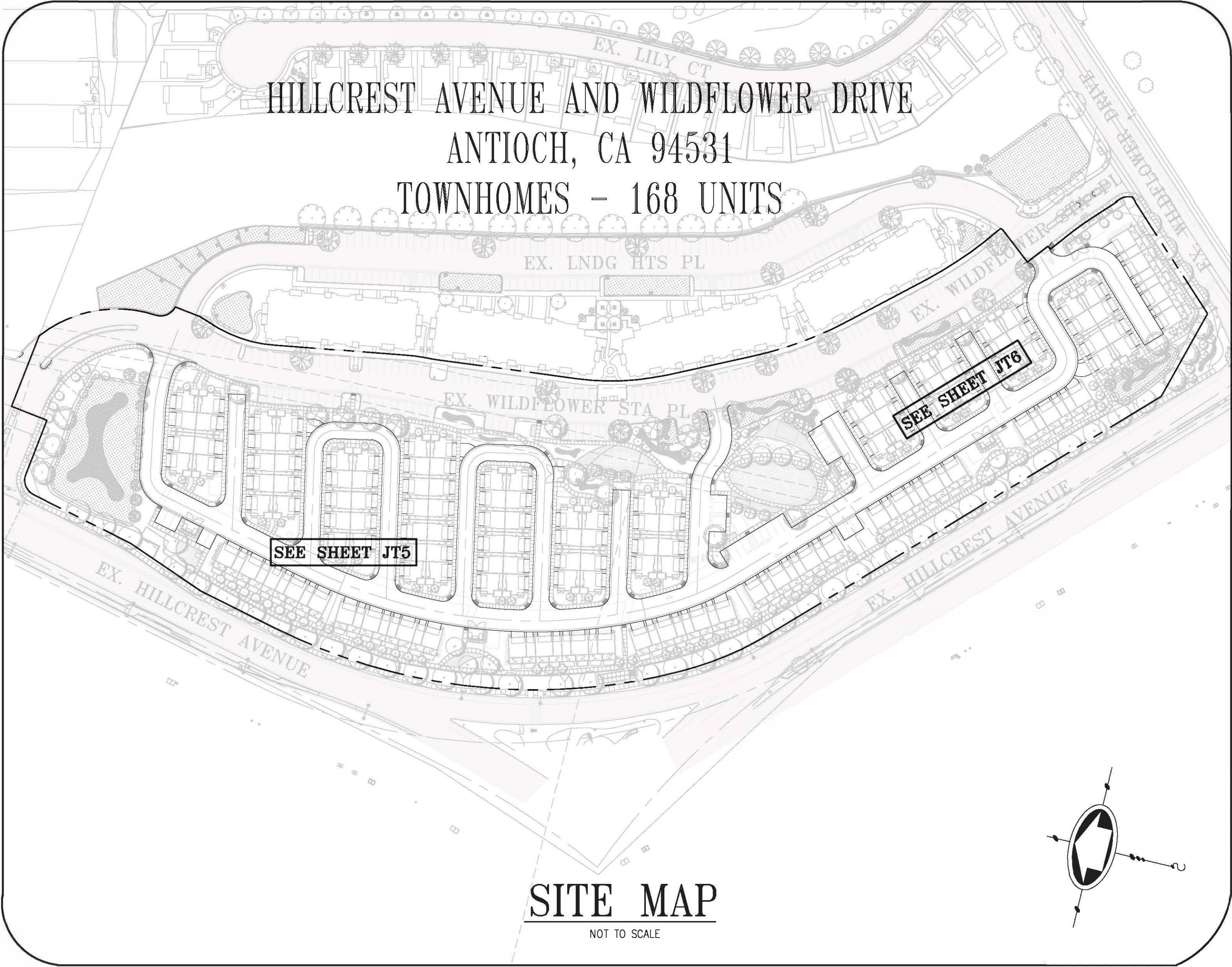
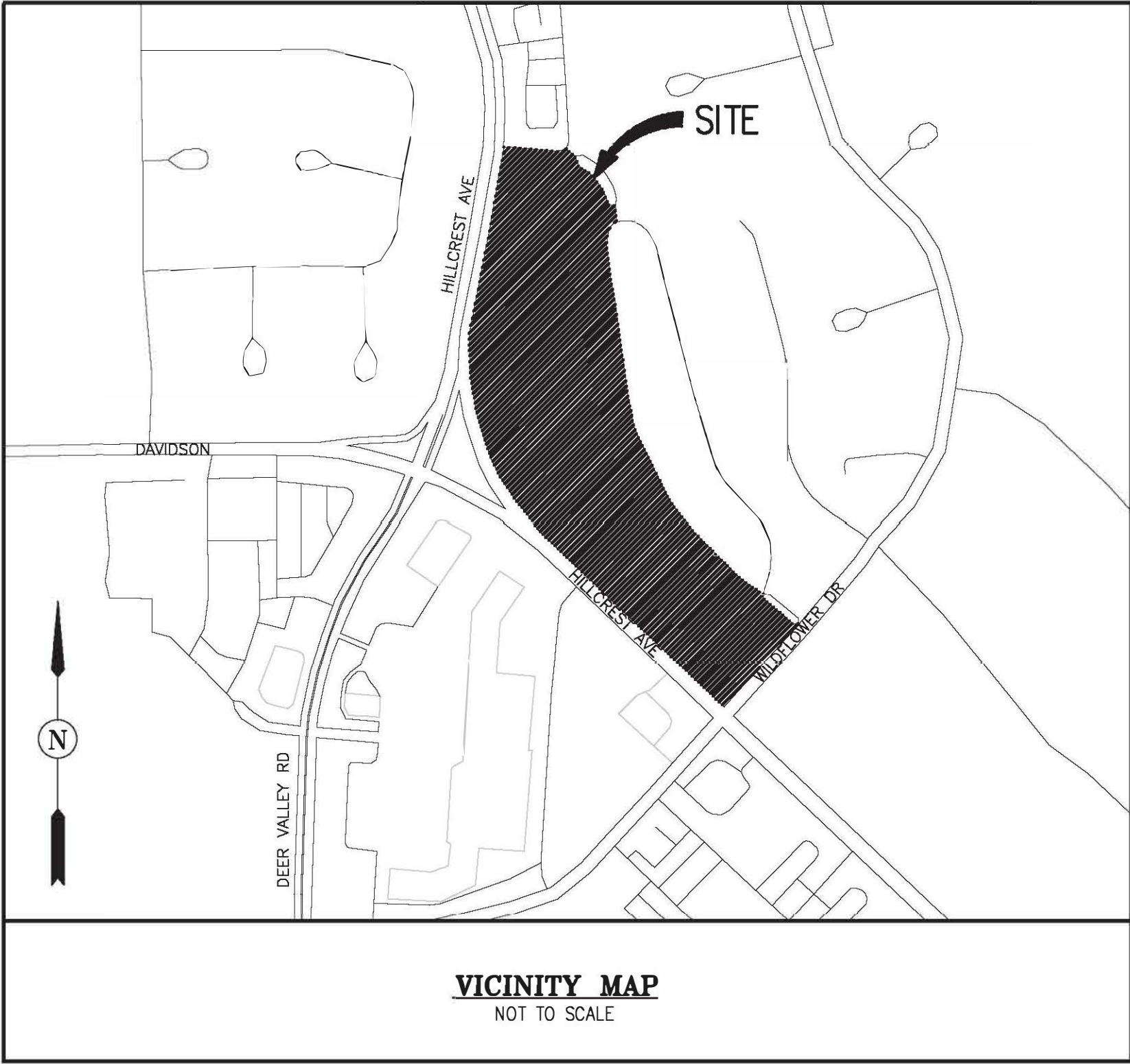


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SHEET NO.  
**17**  
OF 17 SHEETS



DENOVA HOMES  
WILDFLOWER TOWNHOMES 2  
ANTIOCH CONTRA COSTA COUNTY CALIFORNIA



- LEGEND**
- JT — PROPOSED JOINT TRENCH
  - JTX — PROPOSED JOINT TRENCH CROSSING
  - SVC — PROPOSED JOINT TRENCH SERVICE
  - EX JT — EXISTING JOINT TRENCH
  - EX GAS — EXISTING GAS
  - EX FO — EXISTING FIBER OPTIC
  - ESL — EXISTING STREET LIGHT CONDUIT
  - ECUG — EXISTING UNDERGROUND CATV LINES
  - EEUG — EXISTING UNDERGROUND ELECTRIC LINES
  - EUUG — EXISTING UNDERGROUND UTILITY LINES
  - ETUG — EXISTING UNDERGROUND TELEPHONE LINES
  - EUOH — EXISTING OVERHEAD UTILITY LINES
  - ECOH — EXISTING OVERHEAD CATV LINES
  - ETOH — EXISTING OVERHEAD TELEPHONE LINES
  - EEOH — EXISTING OVERHEAD ELECTRIC LINES
  - CUG — PROPOSED UNDERGROUND CATV LINES
  - EUG — PROPOSED UNDERGROUND ELECTRIC LINES
  - TUG — PROPOSED UNDERGROUND TELEPHONE LINES
  - EXISTING PRIMARY SPLICE BOX
  - EXISTING SECONDARY SPLICE BOX
  - PG&E 3Ø, PAD MOUNT TRANSFORMER  
CONCRETE PAD SIZE: 90" x 106"
  - 6S PG&E SECONDARY SPLICE BOX, 3' x 5' x 3'6"
  - EXISTING ELECTROLIER, SINGLE ARM
  - EXISTING ELECTROLIER, DOUBLE ARM

**SHEET INDEX**

SHEET NO.	DESCRIPTION
JT1	JOINT TRENCH COMPOSITE TITLE SHEET
JT2	JOINT TRENCH GENERAL NOTES AND DETAILS
JT3	JOINT TRENCH DETAILS
JT4	JOINT TRENCH SECTIONS AND DETAILS
JT5 - JT6	JOINT TRENCH COMPOSITE PLAN
SL1-SL2	STREET LIGHTING GENERAL NOTES AND DETAILS
SL3 - SL4	STREET LIGHTING SITE PLAN
P1 - P6	PHOTO EXHIBIT SHEET

X L.F. OF JOINT TRENCH AND X STREET LIGHTS  
SHALL BE INSTALLED WITH THIS JOINT TRENCH PLAN SET

- 19 NEW TOWNHOME BUILDINGS (168 UNITS)
- 1 NEW SERVICE COMPLETION (ELECTRIC, TELEPHONE, AND CATV)

TARRAR UTILITY REP.:	KARA PEDERSEN	JOB NO.	223027	PHONE NO.	(925) 240-2595
DEVELOPER:	DENOVA HOMES	JOB NO.	223027	PHONE NO.	(925) 605-9304
PG&E ELECTRIC COORDINATOR:	JASON BARRETT	JOB NO.	-	PHONE NO.	(925)459-2177
PG&E GAS COORDINATOR:	-	JOB NO.	-	PHONE NO.	-
TELEPHONE REP.:	KEVIN BLUTH	JOB NO.	-	PHONE NO.	(925) 271-1510
CABLE T.V. REP.:	KURTIS FAULTNER	JOB NO.	-	PHONE NO.	(925) 337-2853

FILES STATUS			
DESCRIPTION:	BY:	DATE:	STATUS:
CIVIL PLANS (ELECTRONIC FILE)	CBG	02-08-2023	R
ARCHITECTURAL PLANS (ELECTRONIC FILE)	SDG	02-08-2023	R
LANDSCAPE PLANS (ELECTRONIC FILE)	VANDERTOOLEN ASSOCIATES	02-08-2023	R
GAS DESIGN	-	-	-
ELECTRIC DESIGN	BROWN ELECTRIC ESTIMATING	XX-XX-XXXX	XXXX
TELEPHONE INTENT REPLY	AT&T	XX-XX-XXXX	XXXX
CATV INTENT REPLY	COMCAST	XX-XX-XXXX	XXXX
STREET LIGHT PLANS – PUBLIC	-	-	-
STREET LIGHT PLANS – PRIVATE	TARRAR UTILITY CONSULTANT	-	-
SOILS REPORT	XXXX	XX-XX-XXXX	XXXX

A = APPROVED • ANS = APPROVED NOT SIGNED • NA = NOT APPROVED • F = FIRST SUBMITTAL • SS = SECOND SUBMITTAL • R = RECEIVED

**DESIGN CHANGE COMPONENT**

ANY CHANGES TO THIS DESIGN  
MUST BE APPROVED BY

PG&E GAS ADE

**SUBSTRUCTURE VERIFICATION STAMP**

**DEVELOPER NOTE AND SIGN**

ALL PG&E ENCLOSURES AND BOXES HAVE BEEN SET TO GRADE  
ACCORDING TO GRADE STAKES PROVIDED BY DEVELOPERS ENGINEER.  
ALL COSTS TO RELOCATE OR READJUST BOXES AT A LATER DATE  
WILL BE BILLED TO THE DEVELOPER. PLEASE HAVE YOUR JOB SUMP  
VERIFY THE CORRECT GRADE OF ALL ENCLOSURES AND BOXES, AND  
SIGN AND DATE DRAWING.

THANK YOU

SIGNED \_\_\_\_\_  
DATE: \_\_\_\_\_

**TARRAR UTILITY CONSULTANTS**  
APPROVED FOR SUBMITTAL  
KARA PEDERSEN  
QUALIFIED APPLICANT DESIGN ENGINEER

**COMPOSITE DRAWING BY DEVELOPER**

Estimate # \_\_\_\_\_

Approved \_\_\_\_\_ Gas ADE \_\_\_\_\_ Date \_\_\_\_\_

Approved \_\_\_\_\_ Electric ADE \_\_\_\_\_ Date \_\_\_\_\_

PG & E is not responsible for the accuracy of the specifications shown  
on this drawing.

**COMPOSITE DRAWING BY DEVELOPER**

Approved \_\_\_\_\_ Telephone representative \_\_\_\_\_ Date \_\_\_\_\_

Approved \_\_\_\_\_ CATV representative \_\_\_\_\_ Date \_\_\_\_\_

813 First Street  
Brentwood, CA 94513  
(925) 240-2595  
(925) 240-7013 fax  
www.tarrar.com

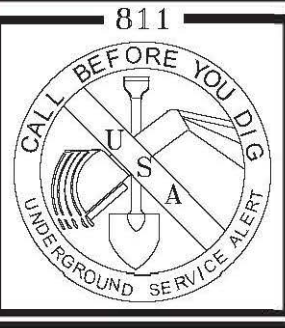
**TARRAR**  
UTILITY CONSULTANTS

- Planning
- Design
- Estimating
- Joint Trench
- Street Lighting
- Fiber Optic
- T-24
- PG&E Gas Design
- PG&E E&E Design
- M.E.P. Design
- Cost Analysis
- Due Diligence

JOINT TRENCH COMPOSITE TITLE SHEET  
DENOVA HOMES  
WILDFLOWER TOWNHOMES 2  
ANTIOCH CALIFORNIA

NO.	REVISIONS	BY	DATE

DATE: FEBRUARY 2024	DATE LAST WORKED ON: 2/5/2024
SCALE: NOT TO SCALE	DRAWN: KK
JOB NO.: 223027	CHECKED: KT
PRELIMINARY NOT FOR CONSTRUCTION	
INTENT TO CONSTRUCT	



SHEET  
JT1  
OF  
JT6  
SHEETS



PROJECT NOTES:

1. FIELD ADJUST SERVICES TO MINIMIZE INTERFERENCE WITH EXISTING FACILITIES (TYPICAL).
2. CONTRACTOR SHALL PERFORM ALL TRENCHING, EXCAVATING, BACKFILLING AND OTHER WORK AS SHOWN OR NOTED ON PLANS, AND AS SPECIFIED ON UTILITY BID DOCUMENTS.
3. FIELD ADJUST SPLICE BOXES TO KEEP CLEAR OF SIDEWALK, DRIVEWAYS AND EXISTING FACILITIES (TYPICAL).
4. A 3 FOOT LEVEL WORKING AREA MUST BE MAINTAINED AROUND ALL ELECTRIC ENCLOSURES. PRIOR TO ENERGIZING THE SYSTEM, THE ELECTRIC UTILITY COMPANY INSPECTOR WILL DETERMINE IF RETAINING WALLS ARE REQUIRED TO MEET MINIMUM CLEARANCE BETWEEN ENCLOSURES AND THE TOPS OR TOES OF SLOPES. IF RETAINING WALLS ARE REQUIRED, THE DEVELOPER AND/OR CONTRACTOR SHALL OBTAIN THE NECESSARY PERMITS FROM THE CITY/COUNTY BUILDING DEPARTMENT PRIOR TO WALL CONSTRUCTION.
5. TRANSITION TO VAULTS FROM TRENCH NOT SHOWN, SEE TRANSITION DETAIL SHEET **JT3** (TYPICAL).
6. CONTRACTOR SHALL PLACE ALL UTILITY SPLICE BOXES, ENCLOSURES & CONDUIT IN PROPER RELATIONSHIP TO FINAL GRADE (SHOWN SCHEMATICALLY).
7. ALL PG&E, TELEPHONE, CABLE T.V. AND FIBER OPTIC BOXES AND JOINT TRENCH FACILITIES ARE TO MAINTAIN A MINIMUM OF 3' SEPARATION FROM SEWER, WATER LATERALS AND DRIVEWAYS.
8. CONTRACTOR SHALL COORDINATE ALL CONNECTIONS BETWEEN PROPOSED AND EXISTING FACILITIES AS DIRECTED BY THE RESPECTIVE UTILITY COMPANY INSPECTOR. UTILITY COMPANY PERSONNEL SHALL MAKE ALL "HOT TIE-INS"; THE CONTRACTOR IS PROHIBITED FROM WORKING IN ANY ENERGIZED FACILITIES.
9. THE CONTRACTOR SHALL OBTAIN THE APPROPRIATE STREET EXCAVATION AND ENCROACHMENT PERMIT(S) FROM THE CITY/COUNTY PRIOR TO STARTING WORK IN THE PUBLIC STREET AREA.
10. FIELD LOCATE JOINT TRENCH FACILITIES TO KEEP CLEAR OF SERVICE LATERALS. SERVICE LATERALS TO BE ROUTED TO AVOID SPLICE BOX (ADDITIONAL P.U.E MAY BE REQUIRED).
11. RESPECTIVE UTILITY COMPANY TO OBTAIN CITY APPROVAL OF ALL ABOVE GROUND EQUIPMENT.
12. UNLESS OTHERWISE SHOWN ON THE PLANS, NATURAL BENDS SHALL BE USED FOR ALL CONDUIT EXCEPT STREET LIGHT CONDUIT.
13. INCIDENTAL TRENCHING TO SPLICE BOXES NOT SHOWN (TYPICAL). CONTRACTOR TO PROVIDE ADDITIONAL TRENCHING AS REQUIRED FOR CONDUIT ROUTING TO SPLICE BOXES AND CABINETS (TYPICAL).
14. ALL CONDUITS SHALL ENTER OR EXIT PERPENDICULAR TO BOX WALLS.
15. ALL CONDUITS MUST BE MANDREL TESTED AND APPROVED.
16. OFFSET SPLICE BOXES TO ROUTE TELEPHONE/FIBER OPTIC CONDUIT AS NEEDED (TYPICAL).
17. PULL ROPES SHALL BE PLACED IN ALL EMPTY CONDUITS AS REQUIRED BY EACH UTILITY COMPANY.
18. ALL PG&E SPLICE BOXES ADJACENT TO TRANSFORMER SHALL BE 26" IN DEPTH (TYPICAL).
19. ALL CONDUITS NOT ENTERING SPLICE BOXES OR ENCLOSURES SHALL BE CAPPED.
20. COORDINATE TIE-IN WITH UTILITY COMPANY AS REQUIRED.
21. THE STREET LIGHT SYSTEM SHALL BE INSTALLED IN ACCORDANCE WITH THE "MATERIAL AND LABOR RECAP" AND LIGHT SCHEDULE AS SHOWN ON THESE PLANS.
22. ALL EXISTING DUCTS TO BE USED IN THESE PLANS SHALL BE "VERIFIED" BY PULLING A MANDREL THROUGH THE ENTIRE EXISTING LENGTH PRIOR TO CONNECTION.
23. EDGE OF SPLICE BOXES & PEDESTALS SHALL BE 5' FROM EDGE OF FIRE HYDRANT AND 3' FROM STREET LIGHT (TYPICAL). CONTRACTOR TO AVOID DISTURBING FIRE HYDRANT THRUST BLOCK.
24. ALL UTILITY SUBSTRUCTURES SHALL BE INSTALLED IN ACCORDANCE WITH THE "MATERIAL AND LABOR RECAP" SHOWN ON THESE PLANS
25. MAINTAIN 3' CLEARANCE AND LEVEL AREA AROUND PRIMARY SPLICE BOXES & XFMRs.
26. DUE TO UNCERTAINTIES OF THE EXACT LOCATION OF EXISTING FACILITIES, FIELD LOCATION OF PROPOSED FACILITIES MAY BE REQUIRED. CONFIRM WITH VARIOUS UTILITIES FOR EXACT PLACEMENT.
27. FOR CLARITY – BOXES/PEDESTALS ARE SHOWN AT LARGER SIZE THAN ACTUAL. FIELD ADJUST TO KEEP CLEAR OF DRIVEWAYS (TYPICAL).
28. ALL SERVICE FACILITIES SHALL BE EXTENDED TO EITHER THE PROPERTY LINE OR TO POSITION SHOWN ON THE PLANS, AND THEN CAPPED, BURIED AND LOCATION STAKED.
29. THESE PLANS WERE PREPARED UTILIZING PLANS RECEIVED FROM R-PN1 (R-PN2).

ABBREVIATION LIST

B/C	BACK OF CURB	H.P.S.	HIGH PRESSURE SODIUM	RT	RETAINING WALL
B/W	BACK OF WALK	IRR.	IRRIGATION CONTROLLER	R/W	RIGHT OF WAY
BTU	BRITISH TERM UNITS	J.T.	JOINT TRENCH	SCH.	SCHEDULE
CB	CATCH BASIN	KV	KILO-VOLTS	SD	STORM DRAIN
CL	CENTERLINE	LE	LANDSCAPE EASEMENT	SHT.	SHEET
CAT.	CATALOG	LF	LINEAR FOOT/FEET	S/W	SIDE WALK
C OR CATV	CABLE TELEVISION	MH	MANHOLE	SS	SANITARY SEWER
CFH	CUBIC FEET PER HOUR	MIN.	MINIMUM	SSE	SANITARY SEWER EASEMENT
C.I.P.	CAPITOL IMPROVEMENT PROJECT	MPOE	MINIMUM POINT OF ENTRY	ST. LT.-S/L	STREET LIGHT
CL	CENTER LINE	N.T.S.	NOT TO SCALE	SUBD'V	SUBDIVISION
CU	COPPER	O.D.	OUTER DIAMETER	Sqft.	SQUARE FOOTAGE
E	ELECTRIC	O.H.	OVER HEAD	T	TELEPHONE
EP	EDGE OF PAVEMENT	PIEUE	PRIVATE INGRESS, EGRESS, AND UTILITY EASEMENT	TUC	TARRAR UTILITY CONSULTANTS
EVAE	EMERGENCY VEHICLE ACCESS EASEMENT	PL	PROPERTY LINE	TYP.	TYPICAL
EX.	EXISTING	P.S.	POWER SUPPLY	T/S	TRAFFIC SIGNAL
F/C	FACE OF CURB	PROJ.	PROJECT	U.G.	UNDERGROUND
FH	FIRE HYDRANT	PSDE	PRIVATE STORM DRAIN EASEMENT	U.O.N.	UNLESS OTHERWISE NOTED
FUT.	FUTURE	PSE	PUBLIC SERVICE EASEMENT	V	VOLT
F.O.	FIBER OPTIC	PVAW	PRIVATE VEHICLE ACCESS WAY	W	WATT
G	GAS	P.V.C.	POLY VINYL CHLORIDE	WT	WATER
GALV.	GALVANIZE	PwE	PUBLIC WATER LINE EASEMENT	W/	WITH
G.E.	GENERAL ELECTRIC	PWR	POWER	W/O	WITHOUT
GRD.	GROUND	PUE	PUBLIC UTILITY EASEMENT	W.E	WATER LINE EASEMENT
H.O.A.	HOME OWNERS ASSOCIATION			XFMR	TRANSFORMER

JOINT TRENCH AND UTILITY BOX LOCATION

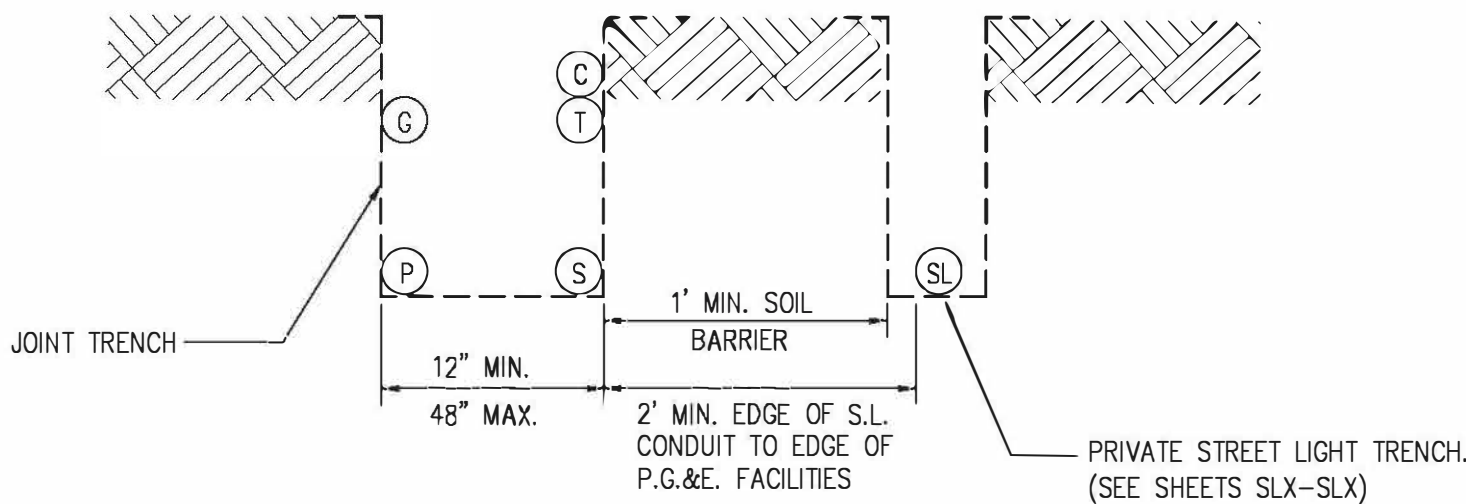
THIS AREA RESERVED FOR STREET SECTION TO BE PLACED AT A LATER TIME

JOINT TRENCH STREET SECTIONS

NOT TO SCALE  
NOTE: SEE PLANS FOR CONFIGURATIONS AND ARRANGEMENTS.  
\* UNLESS OTHERWISE SHOWN

WITHIN ROADWAY SECTION:  
(95% RELATIVE COMPACTION FOR THE TOP 6"  
BELOW ROAD SUBGRADE AND 90% BELOW THAT)

PRIVATE STREET LIGHT TRENCH LOCATION ADJACENT TO JOINT TRENCH



DETAIL **X** PRIVATE STREET LIGHT TRENCH  
N.T.S. **JTX** LOCATION ADJACENT TO JOINT TRENCH  
NOT TO SCALE

GENERAL NOTES:

1. ALL JOINT TRENCH CONSTRUCTION WORK SHALL BE IN ACCORDANCE WITH PG&E UTILITY OPERATIONS UO STANDARD S5453.
2. ALL WORK SHALL BE SUBJECT TO THE INSPECTION AND SATISFACTION OF ALL PARTICIPATING UTILITIES AND CITY INSPECTORS.
3. BACKFILL SELECTION SHALL BE SUBJECT TO THE APPROVAL OF THE RESPECTIVE UTILITY COMPANIES. THE SOILS ENGINEER AND THE CITY AND/OR COUNTY WHERE THE PROJECT IS LOCATED. CONSULT PARTICIPATING UTILITIES, SOILS ENGINEER, AND THE CITY FOR APPROVED BACKFILL MATERIAL. COMPACTION TO MEET LOCAL AGENCIES REQUIREMENTS.
4. THE BOTTOM OF THE TRENCH SHALL BE CLEARED OF ROCKS AND OTHER HARD SURFACES. DISTRIBUTION TRENCHES WITHOUT TELEPHONE CONDUIT DO NOT REQUIRE BEDDING MATERIAL. SERVICE TRENCHES WITHOUT TELEPHONE CONDUIT REQUIRE 2" SAND BEDDING AS A PAD ON WHICH UTILITY FACILITIES CAN REST. SERVICE TRENCHES CONTAINING TELEPHONE CONDUIT ONLY REQUIRE A 1" SAND BEDDING. ALL OTHER TRENCHES CONTAINING TELEPHONE CONDUIT REQUIRE A 3" SAND BEDDING. REFER TO PG&E GREEN BOOK PUBLICATION S5453, EXHIBIT B AND AT&T SPEC095 "AT&T SPECIFICATIONS" TRENCHING AND CONDUIT GUIDE FOR FURTHER INFORMATION.
5. THE CONTRACTOR SHALL BE RESPONSIBLE TO REPLACE PAVEMENT AND/OR SIDEWALK WHERE REMOVED OR DAMAGED AS A RESULT OF ITS OPERATION (UNLESS OTHERWISE NOTED). REPLACEMENT OF PAVEMENT AND/OR SIDEWALK TO BE PER CITY SPECIFICATIONS.
6. THE CONTRACTOR SHALL BE RESPONSIBLE TO COORDINATE AND NOTIFY ALL PARTICIPATING UTILITY INSTALLATIONS.
7. THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY FIELD CHANGES MADE WITHOUT FIRST NOTIFYING TARRAR UTILITY CONSULTANTS.
8. **IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE EXISTENCE AND/OR PRECISE LOCATION OF ALL UNDERGROUND FACILITIES PRIOR TO THE START OF CONSTRUCTION. TARRAR UTILITY CONSULTANTS MAKES NO WARRANTY WHATSOEVER THAT THE EXISTING UNDERGROUND UTILITIES AND/OR STRUCTURES DEPICTED ON THE PLANS HAVE BEEN ACCURATELY LOCATED OR THAT THERE ARE NO OTHER UNDERGROUND UTILITIES AND STRUCTURES IN ADDITION TO WHAT HAS BEEN SHOWN. CALL U.S.A. A MINIMUM OF 48 HOURS PRIOR TO STARTING CONSTRUCTION. FOR CALIFORNIA NORTH, (KERN COUNTY AND NORTHERLY, AND NEVADA) CALL (800)227-2600. FOR CALIFORNIA SOUTH, (SAN BERNARDINO COUNTY AND SOUTHERLY) CALL (800)422-4133.**
9. CONTRACTOR SHALL COMPLY WITH ALL STATE, COUNTY AND CITY LAWS AND ORDINANCES AND WITH THE REGULATIONS OF THE DEPARTMENT OF INDUSTRIAL RELATIONS, O.S.H.A. AND ANY OTHER GOVERNMENTAL AGENCY RELATING TO THE SAFETY AND CHARACTER OF WORK, EQUIPMENT AND LABOR PERSONNEL.
10. THE DRAWINGS AND SPECIFICATIONS SHALL BE CONSIDERED TO BE COMPLEMENTARY TO EACH OTHER. ANYTHING SHOWN ON THE DRAWINGS AND NOT MENTIONED IN THE SPECIFICATIONS, OR MENTIONED IN THE SPECIFICATIONS AND NOT SHOWN ON THE DRAWINGS, SHALL BE OF LIKE EFFECT AS IF SHOWN ON OR MENTIONED IN BOTH. IF DISCREPANCY IS FOUND, NOTIFY TARRAR UTILITY CONSULTANTS PRIOR TO STARTING WORK.
11. TRENCH AND CONDUIT LAYOUTS ARE SHOWN SCHEMATICALLY.
12. TRENCHING OR SUBSTRUCTURE EXCAVATION MAY NECESSITATE OPERATION OVER, UNDER, OR ADJACENT TO OTHER UNDERGROUND UTILITIES (STORM, SEWER, WATER, ETC...). THE CONTRACTOR IS RESPONSIBLE TO LOCATE, PROSPECT, EXPOSE AND PROTECT ALL ADJACENT OR CROSSING UNDERGROUND UTILITIES. THIS WORK TO PROTECT THOSE UTILITIES IS NOT CONSIDERED AS EXTRA WORK. IT IS THE CONTRACTOR'S RESPONSIBILITY TO REVIEW IMPROVEMENT PLANS, IN CONJUNCTION WITH THIS PLAN, AND BID THE WORK ACCORDINGLY.
13. THE QUANTITIES SHOWN ON THESE PLANS ARE ONLY ESTIMATES OF WHAT WILL ACTUALLY BE REQUIRED FOR THE CONSTRUCTION OF THE OVERALL PROJECT. FINAL QUANTITIES MAY VARY ACCORDING TO CHANGES, ADDITIONS, DELETIONS OR OMISSIONS ON THE ORIGINAL PLAN.
14. VERIFY ALL SUBSTRUCTURE EXCAVATION DIMENSIONS WITH SUPPLIER(S) BEFORE BIDDING.
15. TARRAR UTILITY CONSULTANTS ASSUMES NO RESPONSIBILITY FOR ANY VARIANCE BETWEEN THESE PLANS AND THE ACTUAL FIELD CONDITIONS. THE CONTRACTOR SHOULD REVIEW THE PROJECT SITE PRIOR TO SUBMITTING ITS BID.
16. THE CONTRACTOR IS REQUIRED TO EXCAVATE BELL HOLE(S) AT TIE-IN LOCATIONS AS DIRECTED BY PARTICIPATING UTILITY.
17. CONTRACTOR WILL COMPLY WITH ALL LAWS, ORDINANCES AND REGULATIONS. CONTRACTOR SHALL BE FAMILIAR WITH O.S.H.A. INDUSTRIAL ORDERS AND SHALL CONDUCT HIS WORK ACCORDINGLY. WHEN WORKING ENERGIZED EQUIPMENT, THE UTILITY OWNER SHALL BE NOTIFIED TO SUPPLY THE APPROPRIATE MAN POWER AND SAFETY PRECAUTIONS AS NEEDED. THE CONTRACTOR IS RESPONSIBLE FOR PUBLIC SAFETY AND TRAFFIC CONTROL MEASURES.
18. THE CONTRACTOR IS RESPONSIBLE TO PROVIDE AS-BUILT DRAWINGS AFTER INSTALLATION OF PG&E'S GAS AND ELECTRIC SYSTEMS (PRIOR TO "HOT TIE-INS").
19. THE CITY INSPECTOR SHALL BE NOTIFIED TWO WORKING DAYS PRIOR TO COMMENCEMENT OF WORK. COORDINATE WITH THE INSPECTOR ANY SERVICES TO BE ABANDONED.
20. THE CONTRACTOR IS TO VERIFY THE RIGHT OF WAY, PUBLIC UTILITY EASEMENT AND/OR PUBLIC SERVICE EASEMENT ACQUISITION WITH THE APPLICANT PRIOR TO CONSTRUCTION WITHIN AREAS OF QUESTION.
21. PG&E'S GENERAL TERM AND CONDITIONS FOR GAS AND ELECTRIC EXTENSION AND SERVICE CONSTRUCTION BY "APPLICANT" (EFFECTIVE 07/1/95) TO BE UTILIZED FOR ALL TRENCHING, BACKFILLING, AND INSTALLATION WORK.
22. IN THE EVENT OF DISPUTES OR DISAGREEMENT OVER ANY INSTALLATIONS, DESIGNS, PLANS OR DRAWINGS, THE SPECIFICATIONS AND REQUIREMENTS OF THE INDIVIDUAL UTILITY COMPANIES AND THEIR INSPECTORS SHALL TAKE PRECEDENCE. IN CASE OF DISCREPANCIES WITHIN THE DRAWINGS AND SPECIFICATIONS HEREIN, THE CONTRACTOR SHALL CONSULT TARRAR UTILITY CONSULTANTS FOR INTERPRETATION BEFORE WORK IS STARTED.
23. TARRAR UTILITY CONSULTANTS HEREIN, ASSUMES NO RESPONSIBILITY WHATSOEVER FOR THE QUALITY, QUANTITY OR TIMING OF WORK TO BE PERFORMED BY THE CONTRACTOR, UTILITY COMPANY CONSTRUCTION CREWS, OR OTHER SUB-CONTRACTOR OF DEVELOPER.
24. ALL TRENCHING, BACKFILLING AND INSTALLATION WORK IS TO BE IN ACCORDANCE WITH THE STANDARD PRACTICES AND SPECIFICATIONS OF EACH UTILITY COMPANY PARTICIPATING IN THE UTILITY TRENCHES WITHIN THE PROJECT.
25. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING POINTS OF ACCESS THAT ARE AGREEABLE TO ADJACENT LAND USES AND TENANTS AT ALL TIMES.
26. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ASCERTAINING WHAT INSPECTIONS WILL BE REQUIRED FOR APPROVAL OF THE WORK AND FOR COORDINATING ALL SUCH INSPECTIONS. THE CONTRACTOR SHALL GIVE AT LEAST 48 HOURS PRIOR NOTICE TO THE CITY, SOILS ENGINEER, UTILITY COMPANIES OR ANY OTHER INDIVIDUALS OR PUBLIC AGENCIES, THAT THE WORK IS READY FOR INSPECTION.
27. THE CONTRACTOR SHALL NOTIFY DEVELOPER 48 HOURS PRIOR TO THE NEED FOR SURVEY STAKING. THE CONTRACTOR IS RESPONSIBLE FOR THE PRESERVATION OF ALL CONSTRUCTION STAKING SET BY THE DEVELOPER'S SURVEYORS AND WILL BE BACK CHARGED FOR ANY RE-STAKING THAT IS REQUIRED. ANY EXTRA CONSTRUCTION STAKING NECESSITATED SOLELY BY THE CONTRACTOR'S NEGLIGENCE WILL BE CHARGED TO AND PAID FOR BY THE CONTRACTOR.
28. ALL TRANSFORMERS AND TRANSFORMER PADS ARE TO BE INSTALLED PER PG&E SPECIFICATIONS. PROTECTIVE BOLLARDS ARE TO BE PLACED WHERE NEEDED.
29. THE CONTRACTOR SHALL MAKE HIMSELF FAMILIAR WITH THE PROJECT IMPROVEMENT PLANS AND CONDUCT HIS WORK ACCORDINGLY.
30. KEEP ALL BOXES AND PEDESTALS WITHIN PUBLIC UTILITY EASEMENTS OR RIGHT OF WAY, AS SHOWN.
31. ALL SAND BACKFILL MUST HAVE TESTING OF PH LEVEL AS WELL AS SAND EQUIVALENT. SEE **CITY OF ANTIOCH** REQUIREMENTS.
32. THE PROPOSED CONSTRUCTION OPERATION MAY TAKE PLACE AT OR NEAR FENCE LINES, PROPERTY LINES AND PROPERTY IMPROVEMENTS PRIOR TO CONSTRUCTION, CONTRACTOR SHALL BE RESPONSIBLE FOR IDENTIFYING THESE AREAS AND FOR MAINTAINING THESE AREAS AND FACILITIES AT ALL TIMES DURING THE CONSTRUCTION OPERATION.
33. THE CONTRACTOR ASSUMES SOLE AND COMPLETE RESPONSIBILITY FOR THE SITE CONDITION AND SHALL DEFEND AND HOLD THE DEVELOPER AND TARRAR UTILITY CONSULTANTS HARMLESS FROM ANY ALLEGED CLAIMS OR LIABILITIES, EXCEPT THOSE ARISING FROM SOLE NEGLIGENCE OF THE DEVELOPER OR TARRAR UTILITY CONSULTANTS.
34. THE APPROXIMATE LOCATIONS OF ALL EXISTING UTILITY COMPANY UNDERGROUND LINES, POLES BOXES, ETC., WERE OBTAINED FROM A REVIEW OF AVAILABLE UTILITY COMPANY RECORDS, REPRESENTATIONS OF UTILITY COMPANY PERSONAL, OR FIELD OBSERVATIONS. NEITHER THE DEVELOPER NOR TARRAR UTILITY CONSULTANTS ASSUME ANY RESPONSIBILITY FOR VARIANCES BETWEEN THESE PLANS AND THE ACTUAL FIELD CONDITIONS. NO EXTRA PAYMENT WILL BE MADE TO THE CONTRACTOR FOR ANY ADDITIONAL TRENCHING, BOX EXCAVATIONS, MATERIALS, ETC., THAT MAY BE REQUIRED TO COMPLETE THIS PROJECT IN THE EVENT AN EXISTING TIE-IN POINT SUBSTRUCTURE IS EITHER NON-EXISTING OR IS NOT SHOWN ON THE PLANS IN ITS ACTUAL FIELD POSITION. IT IS THE CONTRACTOR'S OBLIGATION AND RESPONSIBILITY TO SAFELY LOCATE ALL EXISTING UNDERGROUND FACILITIES BY SURFACE MARKING AND/OR HAND EXCAVATION PRIOR TO STARTING CONSTRUCTION.
35. "DEVELOPER AND/OR CONTRACTOR IS RESPONSIBLE TO OBTAIN A **CITY OF ANTIOCH** ENCROACHMENT PERMIT FOR ALL WORK DONE IN THE PUBLIC RIGHT OF WAY. DEVELOPER AND/OR CONTRACTOR IS ALSO RESPONSIBLE TO PROVIDE JOINT TRENCH PLANS TO THE **CITY OF ANTIOCH** AT THE TIME OF APPLICATION FOR THE ENCROACHMENT PERMIT."

813 First Street  
Brentwood, CA 94513  
(925) 240-2595  
(925) 240-7013 fax  
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- PG&E Elec Design
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- Cost Analysis
- Due Diligence

JOINT TRENCH GENERAL NOTES AND DETAILS  
DENOVA HOMES  
WILDFLOWER TOWNHOMES 2  
ANTIOCH CALIFORNIA

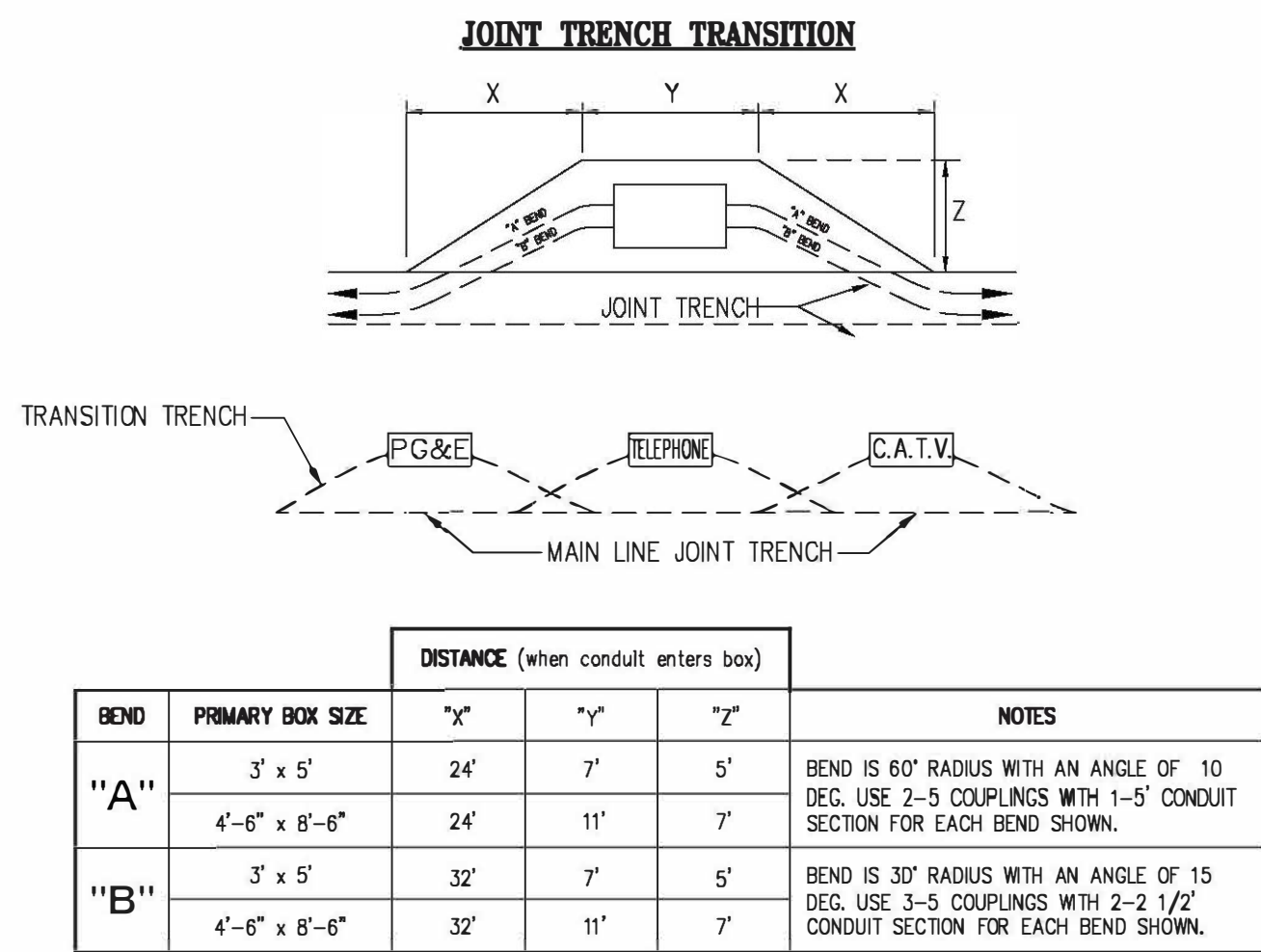
NO.	REVISIONS	BY	DATE

DATE: FEBRUARY 2024	DATE LAST WORKED ON: 2/5/2024
SCALE: NOT TO SCALE	DRAWN: KK
JOB NO.: 223027	CHECKED: KT
PRELIMINARY NOT FOR CONSTRUCTION	
INTENT TO CONSTRUCT	



SHEET  
**JT2**  
**JT6**  
OF  
SHEETS

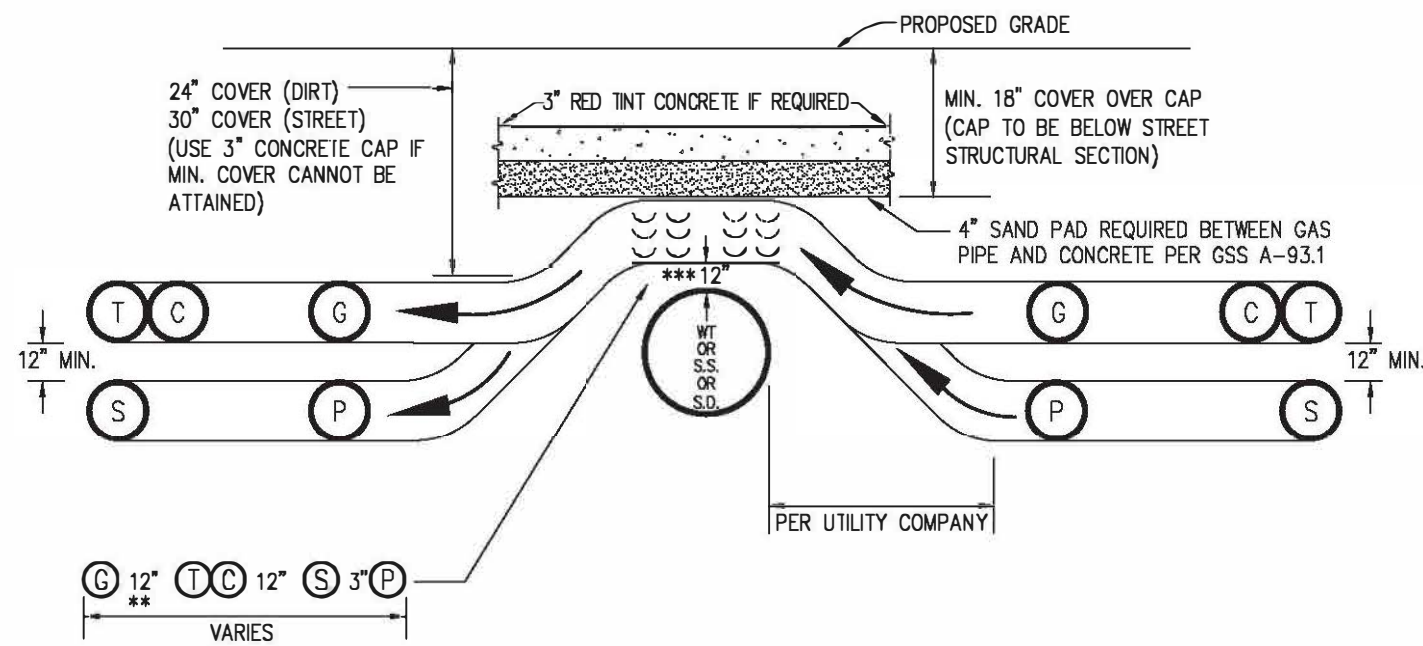




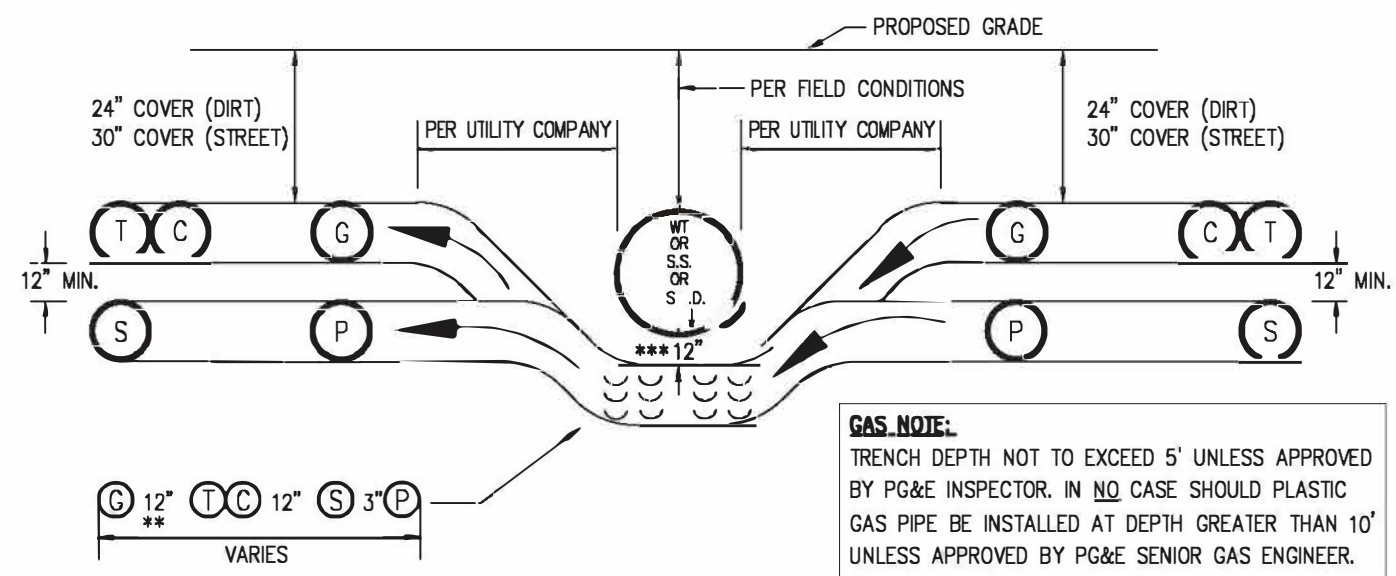
**NOTE:**

- CONTRACTOR TO EXCAVATE TRANSITIONS FROM MAIN-LINE TRENCH TO VAULTS AS REQUIRED BY EACH UTILITY.
- TRANSITIONS NOT SHOWN ON COMPOSITE DRAWING FOR CLARITY.
- CONTRACTOR TO INCLUDE COST OF TRANSITIONS IN VAULT EXCAVATION COST.

DETAIL 1 TYPICAL PRIMARY BOX EXCAVATION USING CONDUIT  
N.T.S. JT3



**JOINT TRENCH OVER WATER, SANITARY SEWER OR STORM DRAIN CHOICE 1 (PREFERRED METHOD)**



**JOINT TRENCH UNDER WATER, SANITARY SEWER OR STORM DRAIN CHOICE 2 (OPTIONAL METHOD)**

- \* SEE MINIMUM COVER & CLEARANCE CHART
- \*\* WITH MUTUAL AGREEMENT, WHEN 4" O.D. OR SMALLER GAS PIPE IS INSTALLED SEPARATION MAY BE REDUCED TO NOT LESS THAN 6" BETWEEN GAS AND COMMUNICATION DUCTS (TELEPHONE & CATV).
- \*\*\* 6" MINIMUM REQUIRED BY PG&E-ADDITIONAL CLEARANCE MAYBE REQUIRED BY CITY OR COUNTY

DETAIL 2 OVER UNDER DETAILS  
N.T.S. JT3

**CONSTRUCTION LABOR AND MATERIAL RESPONSIBILITY**

**PUBLIC UTILITY SYSTEM (JOINT TRENCH)**

TRENCHING:	PG&E	ELECTRIC	CONTRACTOR
EXCAVATE, BACKFILL AND COMPACT:	PG&E	GAS	CONTRACTOR
GAS MATERIAL:	PG&E	TELEPHONE	CONTRACTOR
FURNISH:	PG&E	C.A.T.V.	CONTRACTOR
INSTALL:	PG&E		
ELECTRIC CABLE:	PG&E		
FURNISH:	PG&E		
INSTALL:	PG&E		
ELECTRIC CONDUIT:	PG&E		
FURNISH:	PG&E		
INSTALL:	PG&E		
ELECTRIC SPICE BOXES:	PG&E		
EXCAVATE:	PG&E		
FURNISH:	PG&E		
INSTALL:	PG&E		
ELECTRIC TMWR ENCLS:	PG&E		
EXCAVATE:	PG&E		
FURNISH:	PG&E		
INSTALL:	PG&E		
ELECTRIC EQUIP. ENCLS:	PG&E		
EXCAVATE:	PG&E		
FURNISH:	PG&E		
INSTALL:	PG&E		
ELECTRIC TMWR PADS:	PG&E		
EXCAVATE:	PG&E		
FURNISH:	PG&E		
INSTALL:	PG&E		
ELECTRIC SWITCH PADS:	PG&E		
EXCAVATE:	PG&E		
FURNISH:	PG&E		
INSTALL:	PG&E		
TELEPHONE CONDUIT:	PG&E		
FURNISH:	PG&E		
INSTALL:	PG&E		
TELEPHONE CABLE:	PG&E		
FURNISH:	PG&E		
INSTALL:	PG&E		
TELEPHONE SPICE BOX:	PG&E		
EXCAVATE:	PG&E		
FURNISH:	PG&E		
INSTALL:	PG&E		
TELEPHONE INTER. PADS:	PG&E		
EXCAVATE:	PG&E		
FURNISH:	PG&E		
INSTALL:	PG&E		
C.A.T.V. CONDUITS:	PG&E		
FURNISH:	PG&E		
INSTALL:	PG&E		
C.A.T.V. SPICE BOXES:	PG&E		
EXCAVATE:	PG&E		
FURNISH:	PG&E		
INSTALL:	PG&E		

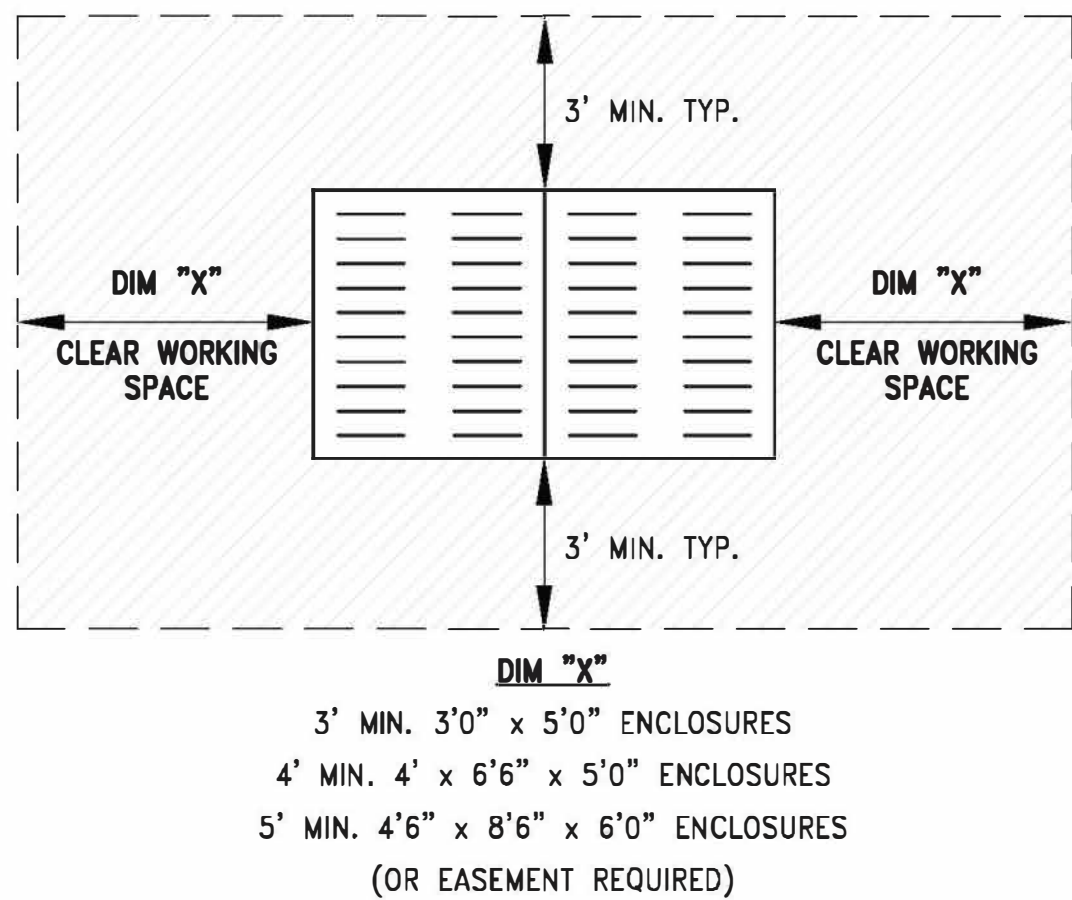
**STREET LIGHTING SYSTEM**

WIRE:	PG&E	CONTRACTOR
FURNISH:	PG&E	CONTRACTOR
INSTALL:	PG&E	CONTRACTOR
CONDUIT:	PG&E	CONTRACTOR
FURNISH:	PG&E	CONTRACTOR
INSTALL:	PG&E	CONTRACTOR
BASES:	PG&E	CONTRACTOR
FURNISH:	PG&E	CONTRACTOR
INSTALL:	PG&E	CONTRACTOR
LUMINAIRES:	PG&E	CONTRACTOR
FURNISH:	PG&E	CONTRACTOR
INSTALL:	PG&E	CONTRACTOR
SPICE BOXES:	PG&E	CONTRACTOR
EXCAVATE:	PG&E	CONTRACTOR
FURNISH:	PG&E	CONTRACTOR
INSTALL:	PG&E	CONTRACTOR
POLES & ARMS:	PG&E	CONTRACTOR
EXCAVATE:	PG&E	CONTRACTOR
FURNISH:	PG&E	CONTRACTOR
INSTALL:	PG&E	CONTRACTOR

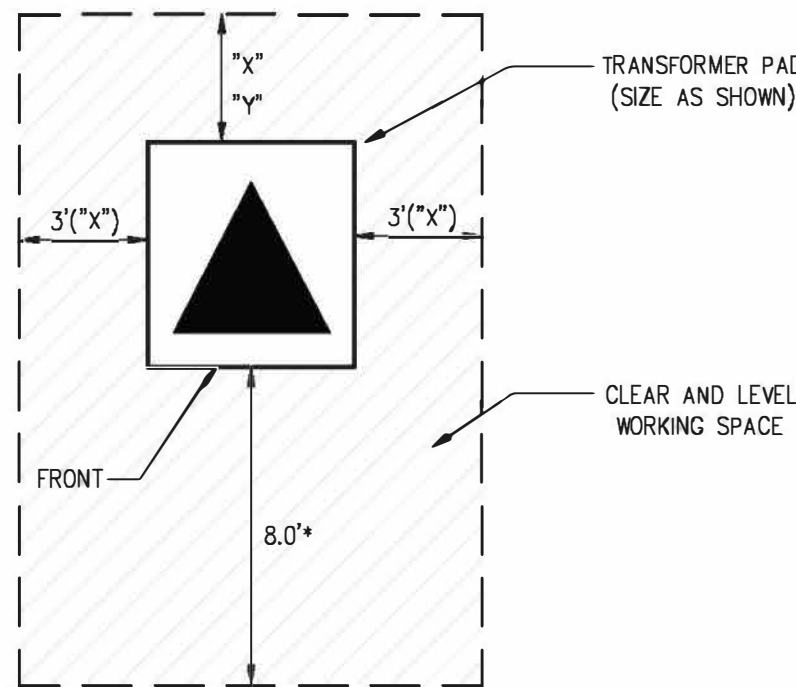
SCHEDULE:	152A
INSTALL IN JOINT TRENCH:	NO/YES
INSTALL IN SEPARATE TRENCH:	YES/NO
CONDUIT SIZE:	1 1/2"
CONDUIT TYPE:	SCH. 40
WIRE SIZE:	#8
TYPE:	CU

ADDITIONAL NOTES:  
DEVELOPER TO SUPPLY AND INSTALL GAS & ELECTRIC FACILITIES UNDER THE COMPETITIVE BIDDING PROVISIONS OF PG&E GREEN BOOK RULES 15, 16 AND 20.

● DESIGNATES THE WORK TO BE PERFORMED BY THE CONTRACTOR AND EACH UTILITY COMPANY.



DETAIL 3 PG&E ENCLOSURE CLEARANCES  
N.T.S. JT3



"x" = 3' MIN. CLEARANCE FROM COMBUSTIBLE WALL

"y" = 2' MIN. CLEARANCE FROM NON-COMBUSTIBLE WALL

\* MINIMUM 9' (COMMERCIAL/HIGH DENSITY) FROM FACE OF CURB

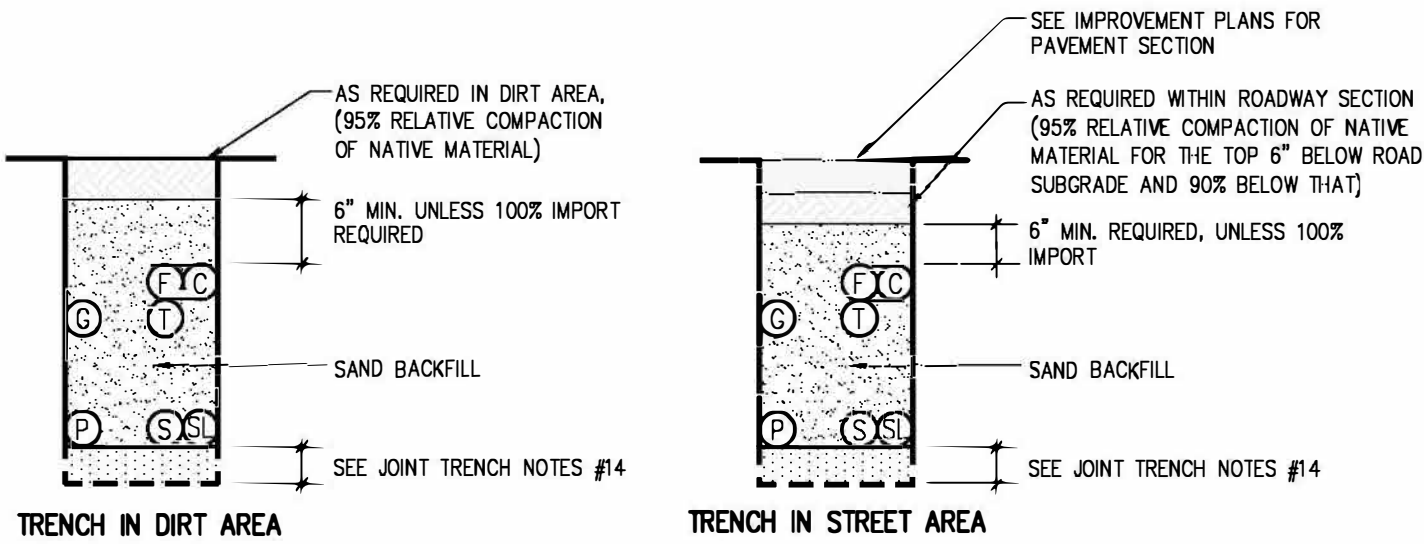
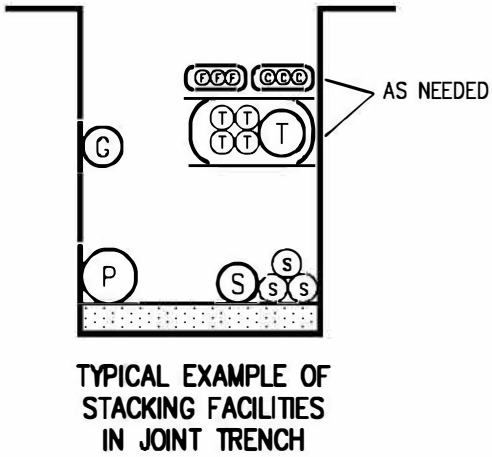
\* MINIMUM 3' (RESIDENTIAL/LOW DENSITY) FROM FACE OF CURB

\*\* MINIMUM 4' FROM ANY DOORWAY

DETAIL 4 PG&E PAD MOUNT TRANSFORMER DETAIL  
N.T.S. JT3



JOINT TRENCH MINIMUM COVER AND CLEARANCES												
MINIMUM SEPARATION FROM												
	UTILITY	G	T	TD	C	S	P	SL	SLP	F	FE	MINIMUM COVER
G	(GAS)*	—	12"	12"	6"	12"	12"	6"	12"	12"	24"; 30" IN STREET	
T	(TELEPHONE) DUCT	12"	—	1"	12"	12"	12"	12"	1"	12"	24"; 30" IN STREET	
TD	(TELEPHONE) DIRECT BURY	12"	1"	—	1"	12"	12"	12"	1"	12"	24"; 30" IN STREET	
C	(CABLE T.V.)	12"	1"	1"	—	12"	12"	12"	1"	12"	24"; 30" IN STREET	
S	(ELECT. SECONDARY)	6"	12"	12"	12"	1.5"	3"	12"	1.5"	12"	24"; 30" IN STREET	
P	(ELECT. PRIMARY)	12"	12"	12"	12"	3"	3"	3"	3"	12"	36"; 36" IN STREET	
SL	(PUBLIC AND PRIVATE-STREET LIGHT)**	12"	12"	12"	12"	12"	—	12"	12"	12"	24"; 30" IN STREET	
SLP	(P.G.E.-STREET LIGHT)	6"	12"	12"	12"	1.5"	3"	12"	1.5"	12"	24"; 30" IN STREET	
FE	(FOREIGN ELECTRIC SOURCES, NON PG&E)	12"	12"	12"	12"	12"	12"	12"	12"	—	24"; 30" IN STREET	
F	(FIBER OPTIC)	12"	1"	1"	1"	12"	12"	12"	—	12"	24"; 30" IN STREET	



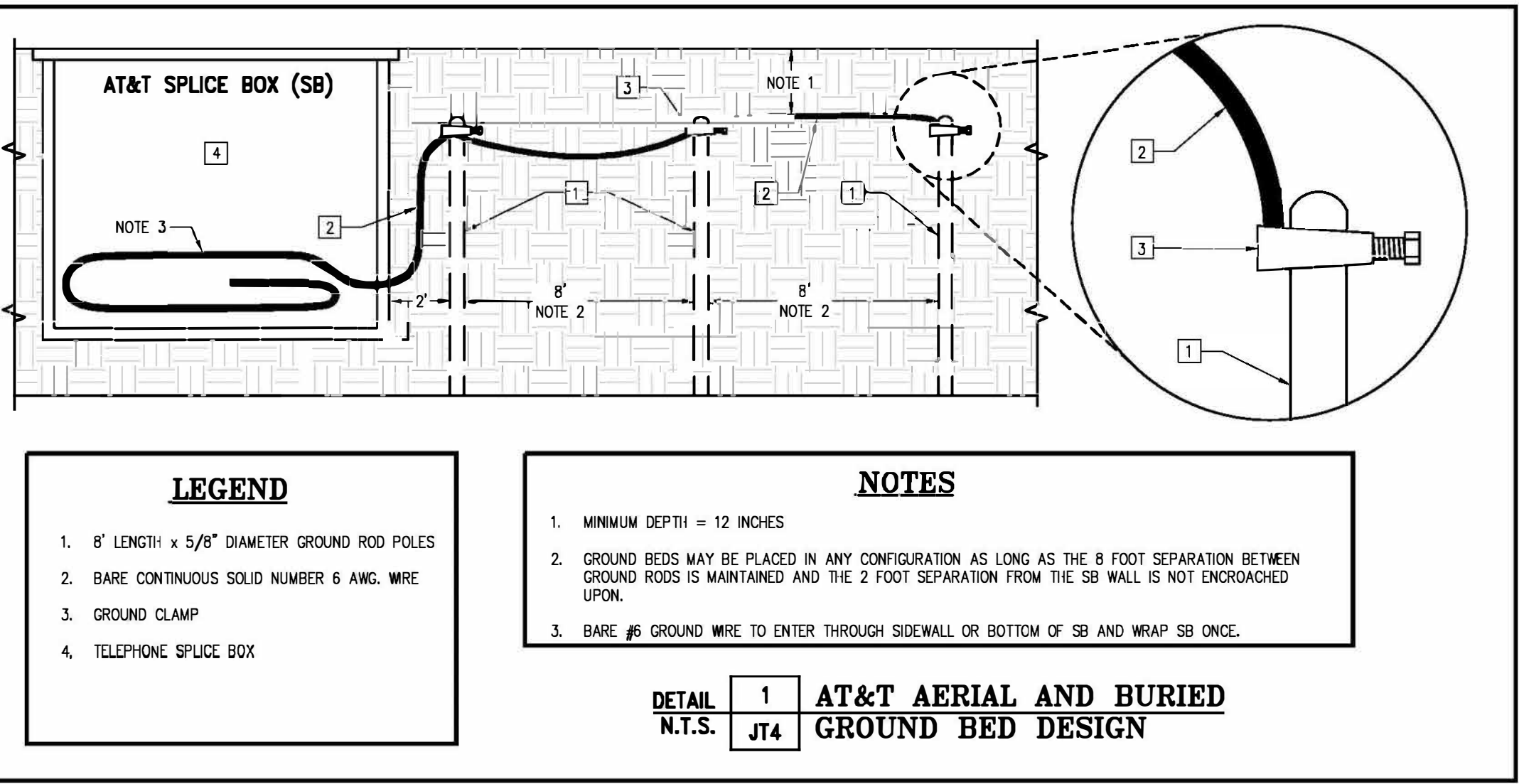
- LEGEND**
- MEETS UTILITY TRENCH ALLOTMENT
  - EXCEEDS UTILITY TRENCH ALLOTMENT
  - GAS
  - ELECTRIC PRIMARY
  - ELECTRIC SECONDARY
  - TELEPHONE (DUCT OR DIRECT BURY)
  - CATV
  - STREET LIGHT (PUBLIC OR PRIVATE)
  - STREET LIGHT (PG&E)
  - FOREIGN ELECTRIC
  - FIBER OPTIC

**JOINT TRENCH NOTES:**

- TRENCH COVER & CLEARANCES SHOWN ARE MINIMUMS ONLY AND MAY REQUIRE ALTERATIONS TO SUIT FIELD CONDITIONS.
- IT IS RECOMMENDED THAT ALL FACILITIES ARE TO BE A MINIMUM OF 12" BELOW SUB-BASE DISTURBANCE.
- \* WITH MUTUAL AGREEMENT FROM PARTICIPATING UTILITIES, WHEN 4" O.D. OR SMALLER GAS PIPE IS INSTALLED, SEPARATION MAY BE REDUCED TO NOT LESS THAN 6" BETWEEN GAS AND COMMUNICATION DUCTS (TELEPHONE, C.A.T.V. & FIBER OPTIC).
- \* WHERE 6" GAS MAIN IS LOCATED IN THE JOINT TRENCH A 18" MINIMUM SEPARATION FROM GAS MAIN TO ALL UTILITIES WILL BE REQUIRED.
- \*\* WITH MUTUAL AGREEMENT FROM PARTICIPATING UTILITIES, STREET LIGHT SEPARATION MAY BE REDUCED TO 0" BETWEEN STREET LIGHT AND COMMUNICATION DUCTS (TELEPHONE, C.A.T.V. & FIBER OPTIC).
- TRENCH CONFIGURATIONS SHOWN ARE FOR INSTALLATION WHERE EACH OCCUPANT IS UTILIZING HIS ENTIRE SPACE ALLOCATION. OTHER CONFIGURATIONS OR REDUCED DIMENSIONS MAY BE USED, PROVIDED THAT MINIMUM COVER AND CLEARANCES ARE MAINTAINED.
- THE CONTRACTOR IS TO ADJUST TRENCH DEPTHS AT ALL JOINT TRENCH LATERAL CROSSINGS TO MAINTAIN REQUIRED CLEARANCES BETWEEN ALL PARTICIPATING UTILITIES.
- TRENCH SECTIONS ARE SHOWN SCHEMATICALLY AND INDICATE AREAS OF OCCUPANCY ONLY; THEY DO NOT REFLECT SIZE OR QUANTITY OF FACILITIES TO BE INSTALLED.
- TRENCH FOOTAGES PER SECTION ARE APPROXIMATE. SECTIONS ARE DESIGNED TO ACCOMMODATE ALL REQUIRED FACILITIES AS INDICATED ON EACH TRENCH PARTICIPANT'S CONSTRUCTION DRAWINGS.
- THE CONTRACTOR SHALL VERIFY TRENCH FOOTAGES FOR ACCURACY PRIOR TO EXCAVATION AND TAKE NECESSARY PRECAUTION CROSSING WATER AND SEWER FACILITIES.
- THE CONTRACTOR SHALL REFER TO THE COMPOSITE, CONDUIT, AND/OR EACH RESPECTIVE UTILITY INSTALLATION PLAN FOR THE NECESSARY CONDUIT CABLE AND/OR PIPE TO BE INSTALLED IN THIS PROJECT.
- TYPE "W2" TRENCH SHALL BE INSTALLED AFTER CURB AND GUTTER INSTALLATION. CONTRACTOR SHALL COORDINATE ADDITIONAL MOVE-INS NECESSARY TO COMPLETE THE SERVICES TO THE DWELLING UNITS WITH THE DEVELOPER, ALL AGENCIES AND THE UTILITY COMPANIES. THE COST OF THESE MOVE-INS SHALL BE INCLUDED IN THE CONTRACTOR'S UNIT PRICE FOR TRENCHING.
- THE AVERAGE TRENCH DEPTHS SHOWN ARE BASED ON THE MINIMUM UTILITY COMPANY REQUIREMENTS FOR DEPTH AND SEPARATION. CONTRACTOR SHALL ADJUST TRENCH WIDTH & DEPTH AS REQUIRED TO ADEQUATELY CLEAR EXISTING UNDERGROUND FACILITIES AND MAINTAIN MINIMUM UTILITY CLEARANCES. ALL TRENCHES OVER 60" DEEP MUST COMPLY WITH OSHA REQUIREMENTS. (SEE THE JOINT TRENCH MINIMUM COVER AND CLEARANCE TABLE)
- CONTRACTOR SHALL USE SAND BEDDING AND SHADING AS REQUIRED BY THE UTILITY COMPANIES. THE BOTTOM OF THE TRENCH SHALL BE CLEARED OF ROCKS AND OTHER HARD SURFACES. DISTRIBUTION TRENCHES WITHOUT TELEPHONE CONDUIT DO NOT REQUIRE BEDDING MATERIAL. SERVICE TRENCHES WITHOUT TELEPHONE CONDUIT REQUIRE 2" SAND BEDDING AS A PAD ON WHICH UTILITY FACILITIES CAN REST. SERVICE TRENCHES CONTAINING TELEPHONE CONDUIT ONLY REQUIRE A 1" SAND BEDDING. ALL OTHER TRENCHES CONTAINING TELEPHONE CONDUIT REQUIRE A 3" SAND BEDDING. REFER TO PG&E GREEN BOOK PUBLICATION SS453, EXHIBIT B AND AT&T SPEC95 "AT&T SPECIFICATIONS" TRENCHING AND CONDUIT GUIDE FOR FURTHER INFORMATION.
- ALL TRENCHING AND BACKFILLING TO BE DONE IN ACCORDANCE WITH THE CITY OF ANTIOCH ENGINEERING STANDARDS AND SPECIFICATIONS.
- ALL PG&E, TELEPHONE, CABLE, AND FIBER OPTIC BOXES AND JOINT TRENCH FACILITIES ARE TO MAINTAIN A MINIMUM OF 3' SEPARATION FROM SEWER AND WATER LATERALS AND DRIVEWAYS. ALL UTILITY VAULTS, BOXES, PEDESTALS, ETC. MUST MAINTAIN A 5' MINIMUM CLEARANCE FROM FIRE HYDRANTS, AND 3' MINIMUM FROM STREETLIGHTS.

JOINT TRENCH OCCUPANCY GUIDE																		
TRENCH SECTION	A*	B*	C*	D*	E*	F*	G*	H*	I	J	K	L	M	N	O	P	Q	R
GAS	X	X	X	X					X	X	X	X	X	X	X		X	
TELEPHONE	X	X		X	X				X	X	X	X	X	X	X	X	X	
CABLE T.V.	X	X	X		X	X			X	X	X	X	X	X	X	X	X	
ELECTRIC SEC.	X	X	X	X	X	X	X			X	X	X	X	X	X	X	X	
ELECTRIC PRI.	X	X	X	X	X	X	X	X										
FIBER OPTICS	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X

\*THESE SECTIONS MAY OR MAY NOT CONTAIN SECONDARY



THIS AREA RESERVED FOR JOINT TRENCH SECTIONS  
TO BE PLACED AT A LATER TIME

813 First Street  
Brentwood, CA 94513  
(925) 240-2595  
(925) 240-7013 fax  
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- T-24
- PG&E Gas Design
- PG&E Elec Design
- M.E.P. Design
- Cost Analysis
- Due Diligence

JOINT TRENCH SECTIONS AND DETAILS  
DENOVA HOMES  
WILDFLOWER TOWNHOMES 2  
ANTIOCH CALIFORNIA

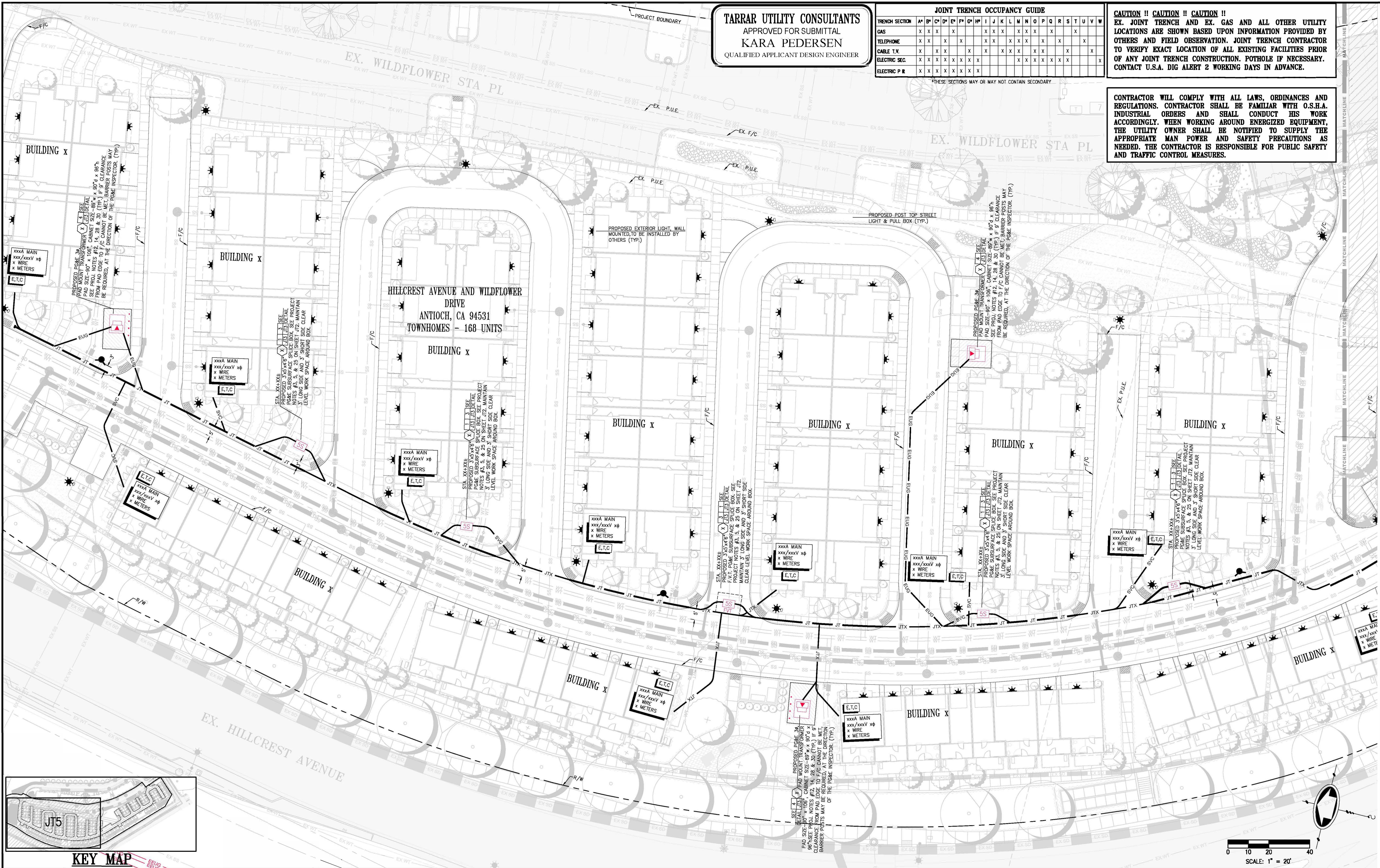
NO.	REVISIONS	BY	DATE

DATE: FEBRUARY 2024	DATE LAST WORKED ON: 2/6/2024
SCALE: NOT TO SCALE	DRAWN: KK
JOB NO: 223027	CHECKED: KT
PRELIMINARY NOT FOR CONSTRUCTION	
INTENT TO CONSTRUCT	



SHEET  
JT4  
JT6  
OF  
SHEETS





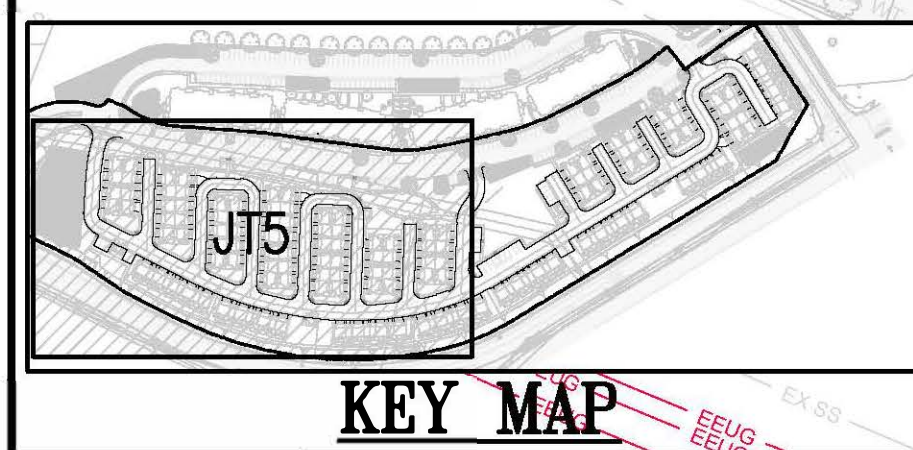
**TARRAR UTILITY CONSULTANTS**  
APPROVED FOR SUBMITTAL  
**KARA PEDERSEN**  
QUALIFIED APPLICANT DESIGN ENGINEER

JOINT TRENCH OCCUPANCY GUIDE																			
TRENCH SECTION	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S
GAS	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
TELEPHONE	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
CABLE TV	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
ELECTRIC SEC.	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
ELECTRIC P.R.	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X

\*THESE SECTIONS MAY OR MAY NOT CONTAIN SECONDARY

**CAUTION !! CAUTION !! CAUTION !!**  
EX. JOINT TRENCH AND EX. GAS AND ALL OTHER UTILITY LOCATIONS ARE SHOWN BASED UPON INFORMATION PROVIDED BY OTHERS AND FIELD OBSERVATION. JOINT TRENCH CONTRACTOR TO VERIFY EXACT LOCATION OF ALL EXISTING FACILITIES PRIOR OF ANY JOINT TRENCH CONSTRUCTION. POT HOLE IF NECESSARY. CONTACT U.S.A. DIG ALERT 2 WORKING DAYS IN ADVANCE.

CONTRACTOR WILL COMPLY WITH ALL LAWS, ORDINANCES AND REGULATIONS. CONTRACTOR SHALL BE FAMILIAR WITH O.S.H.A. INDUSTRIAL ORDERS AND SHALL CONDUCT HIS WORK ACCORDINGLY. WHEN WORKING AROUND ENERGIZED EQUIPMENT, THE UTILITY OWNER SHALL BE NOTIFIED TO SUPPLY THE APPROPRIATE MAN POWER AND SAFETY PRECAUTIONS AS NEEDED. THE CONTRACTOR IS RESPONSIBLE FOR PUBLIC SAFETY AND TRAFFIC CONTROL MEASURES.



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JOINT TRENCH COMPOSITE PLAN  
DENOVA HOMES  
WILDFLOWER TOWNHOMES 2  
ANTIOCH CALIFORNIA

NO.	REVISIONS	BY	DATE

DATE: FEBRUARY 2024  
SCALE: 1" = 20'  
JOB NO.: 223027

DATE LAST WORKED ON: 2/5/2024  
DRAWN: KK  
CHECKED: KP  
PRELIMINARY  
NOT FOR CONSTRUCTION

INTENT TO CONSTRUCT

811  
CALL BEFORE YOU DIG  
UNDERGROUND SERVICE ALERT

SHEET  
**JT5**  
OF  
**JT6**  
SHEETS

SEE JT6

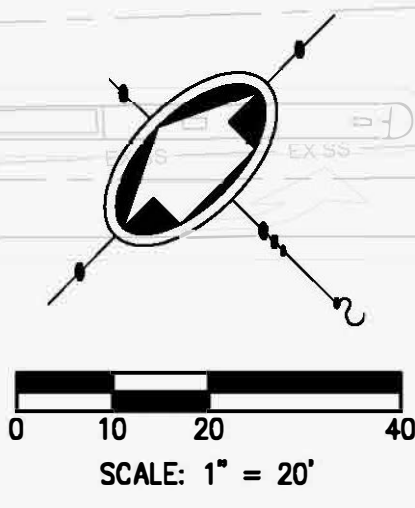
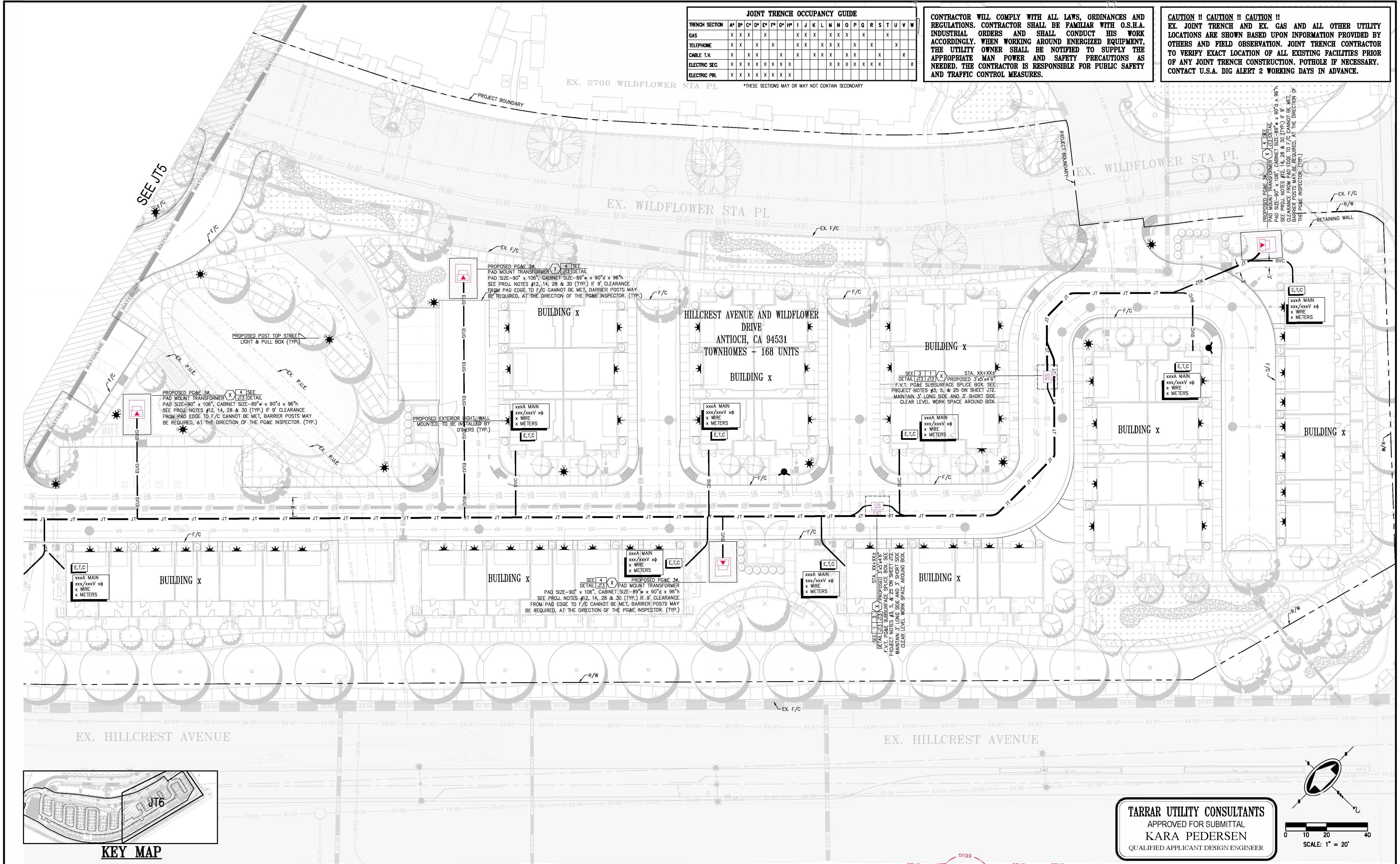


JOINT TRENCH OCCUPANCY GUIDE																
TRENCH SECTION	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P
GAS	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
TELEPHONE	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
CABLE T.V.	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
ELECTRIC SEC.	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
ELECTRIC PRI.	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X

\*THESE SECTIONS MAY OR MAY NOT CONTAIN SECONDARY

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• Cost Analysis

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JOINT TRENCH COMPOSITE PLAN

DENOVA HOMES

WILDFLOWER TOWNHOMES 2

ANTIOCH CALIFORNIA

NO.	REVISIONS	BY	DATE

DATE: FEBRUARY 2024

SCALE: 1" = 20'

JOB NO.: 223027

DATE LAST WORKED ON: 2/5/2024

DRAWN: KK

CHECKED: KP

PRELIMINARY

NOT FOR CONSTRUCTION

INTENT TO CONSTRUCT

811

CALL BEFORE YOU DIG

UTAH

UNDERGROUND SERVICE UNIT

SHEET

JT6

OF


JT6

SHEETS

A42



DENOVA HOMES  
WILDFLOWER TOWNHOMES 2  
ANTIOCH CONTRA COSTA COUNTY CALIFORNIA

**Sternberg Lighting**  
 Established 1923 • EMPLOYEE OWNED  
**GENESIS**  **Product Configurator**

Catalog Number: \_\_\_\_\_

Type:  
 16' pole  
 ground  
 mount

Customer Approval: \_\_\_\_\_

Signature \_\_\_\_\_

Date \_\_\_\_\_

Conceptual assembly drawing, subject to Engineering verification by factory.

**1.1**  
**SL1**

**STERNBERG 1750LED-1L40T4-MDL06-CA-HSS (REDUCED 40%) 36.3W**  
**QUANTITY: 15**

**1.2**  
**SL1**

**STERNBERG 1750LED-1L40T4-MDL06-CA-HSS (REDUCED 40%) 36.3W**  
**QUANTITY: 4**

**DRIVER: MDL06**

Driver:

Midwatt Omnitronic Low Voltage Driver - 150-077V

200 mA @ 0.5 A

**OPTIONS: CA/**

Lens:

Clear Acrylic (CA)

**FINISH: BKT**

Finish:

Black. BKT is used to powder coated to Black. Textured finish. Prior to painting, the assembly must be thoroughly cleaned and prepared to a finish and then painted with the appropriate priming, mixing, and painting system. A finish is used to finish, and non-flammable paint is used to finish the assembly.

**CENTER POST TOP FIXTURE: 1750-ED**

The 1750-ED fixture is a large pole, omnidirectional, 1750-watt, a tall, cylindrical, omnidirectional fixture mounted on a 16-foot tall pole. The fixture is used for outdoor lighting.

It is a tall fixture with a height of 16 feet.

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1	PRIVATE POST TOP ELECTROLIER DETAIL
SL1	NOT TO SCALE

**SternbergLighting**  
ESTABLISHED 1923 / EMPLOYEE OWNED  
**GENESIS** Product Configurator

Catalog Number:

Type:  
13" Single  
(raised  
footing mt.)

Customer Approval:

Conceptual assembly drawing, subject to Engineering verification by factory.

Signature

Date

**CENTER TOP FIXTURE: 1750-LED**

The 1750LED fixture consists of a single wide beam design. It houses a lens, cylindrical heat sink and mounting arm. It is designed to be mounted to a pole or wall and is not for use as a street light.

It is not intended for use as a street light.

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














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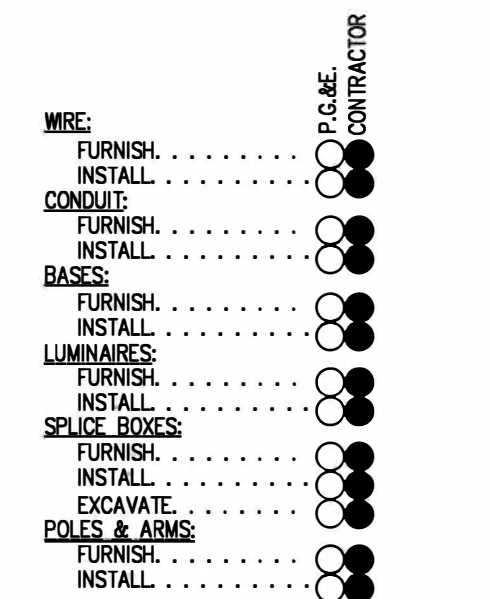
2	PRIVATE POST TOP ELECTROLIER DETAIL
SL1	NOT TO SCALE

**LEGEND**

- |  |                 |  |
|--|-----------------|--|
|   | ESL             | EXISTING STREET LIGHT CONDUIT  |
|   | SL              | PROPOSED STREET LIGHT CONDUIT  |
|   | X(X'S'L. COND.) | STREET LIGHTING CONDUIT LABEL, X' INDICATES CONDUIT LENGTH, (XX'S'L. COND.) INDICATES CONDUIT SIZE |
|   | X'(X'X)         | STREET LIGHTING WIRE LABEL, X' INDICATES WIRE LENGTH, (X'X) INDICATES WIRE TYPE.                   |
|   | (2W)            | 2-#8 CU. WIRE (SEE NOTE 15)  |
|   | (3W)            | 3-#8 CU. WIRE (SEE NOTE 15)  |
|   | (3WG)           | 2-#8 CU. WIRE & 1-#8 BARE CU. GROUND (UNLESS OTHERWISE NOTED); SEE NOTE 15 & GROUNDING DETAIL      |
|   | (4WG)           | 3-#8 CU. WIRE & 1-#8 BARE CU. GROUND (UNLESS OTHERWISE NOTED); SEE NOTE 15 & GROUNDING DETAIL      |
|   |                 | EXISTING TRAFFIC SIGNAL  |
|   |                 | STREET LIGHTING PULL BOX STATE TYPE #3 1/2, 10" x 17" x 12"  |
|   |                 | STREET LIGHTING GROUND BOX STATE TYPE #5, 13" x 24" x 12"  |
|   |                 | PROPOSED PRIVATE STREET LIGHT, SINGLE POST TOP   |
|   |                 | PROPOSED EXTERIOR LIGHT, WALL MOUNT (BY OTHERS)  |
|   |                 | EXISTING PRIVATE STREET LIGHT, SINGLE POST TOP   |
|   |                 | EXISTING PRIVATE STREET LIGHT, TWIN POST TOP   |
|   |                 | EXISTING STREET LIGHT, SINGLE ARM  |
|   |                 | CIRCUIT NUMBER/PHASE (SEE ELECTRIC SOURCE DETAILS)   |
|   |                 | POLE NUMBER  |
|  |                 | STREET LIGHT POINT OF SERVICE  |
|  |                 | FACE OF CURB   |

## CONSTRUCTION LABOR AND MATERIAL RESPONSIBILITY

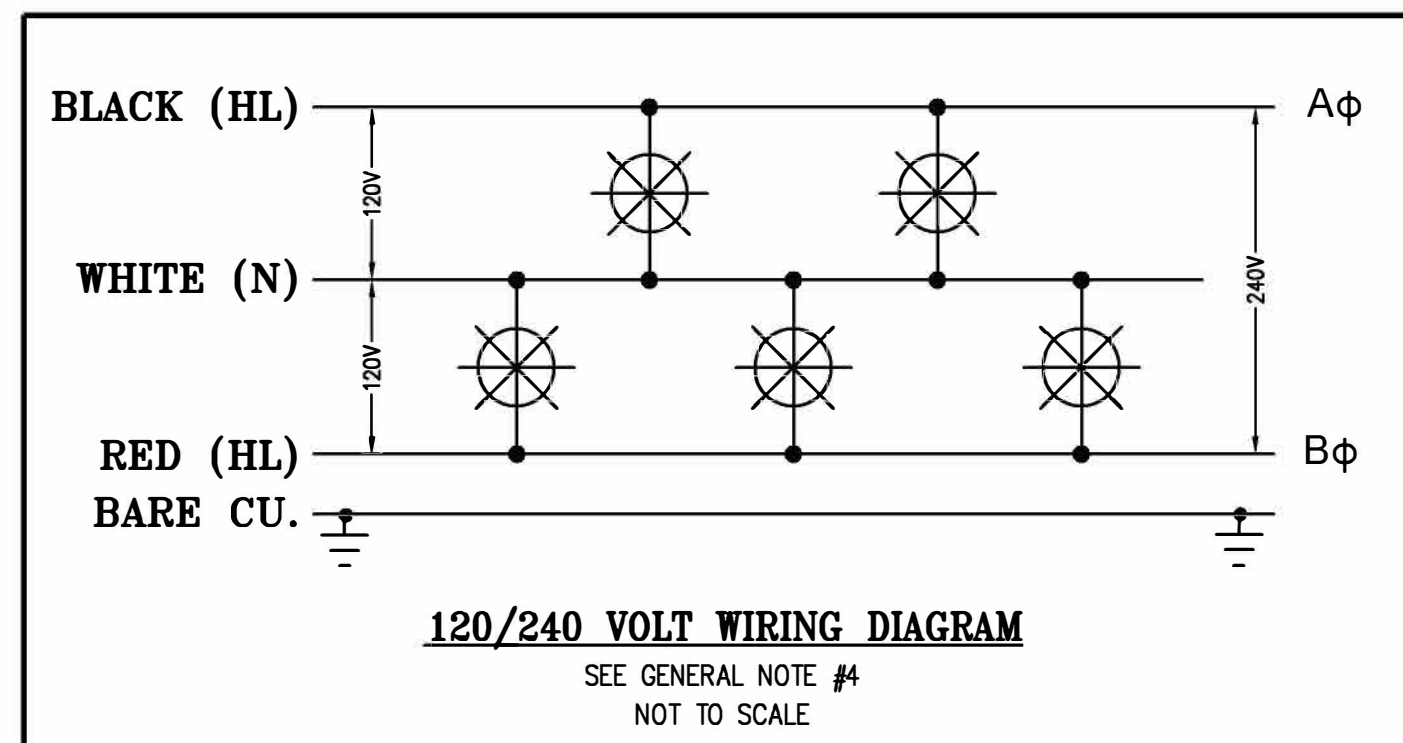
**STREET LIGHTING SYSTEM**  
**(PRIVATE)**



SCHED ULE:	LS3
INSTALL IN JOINT TRENCH:	NO
INSTALL IN SEPARATE TRENCH:	YES
COND UITSIZE:	2"
COND UITYTYPE:	SCH. 40
WIRE SIZE: VARIES	TYPE: CU

ADDITIONAL NOTES:  
-PRIVATE LIGHTS TO BE MAINTAINED BY H.O.A.  
(HOMEOWNER ASSOCIATION) UPON TRACT  
ACCEPTANCE.

## VOLTAGE DROP CALCULATIONS

[illegible]

FOR REVIEW ONLY

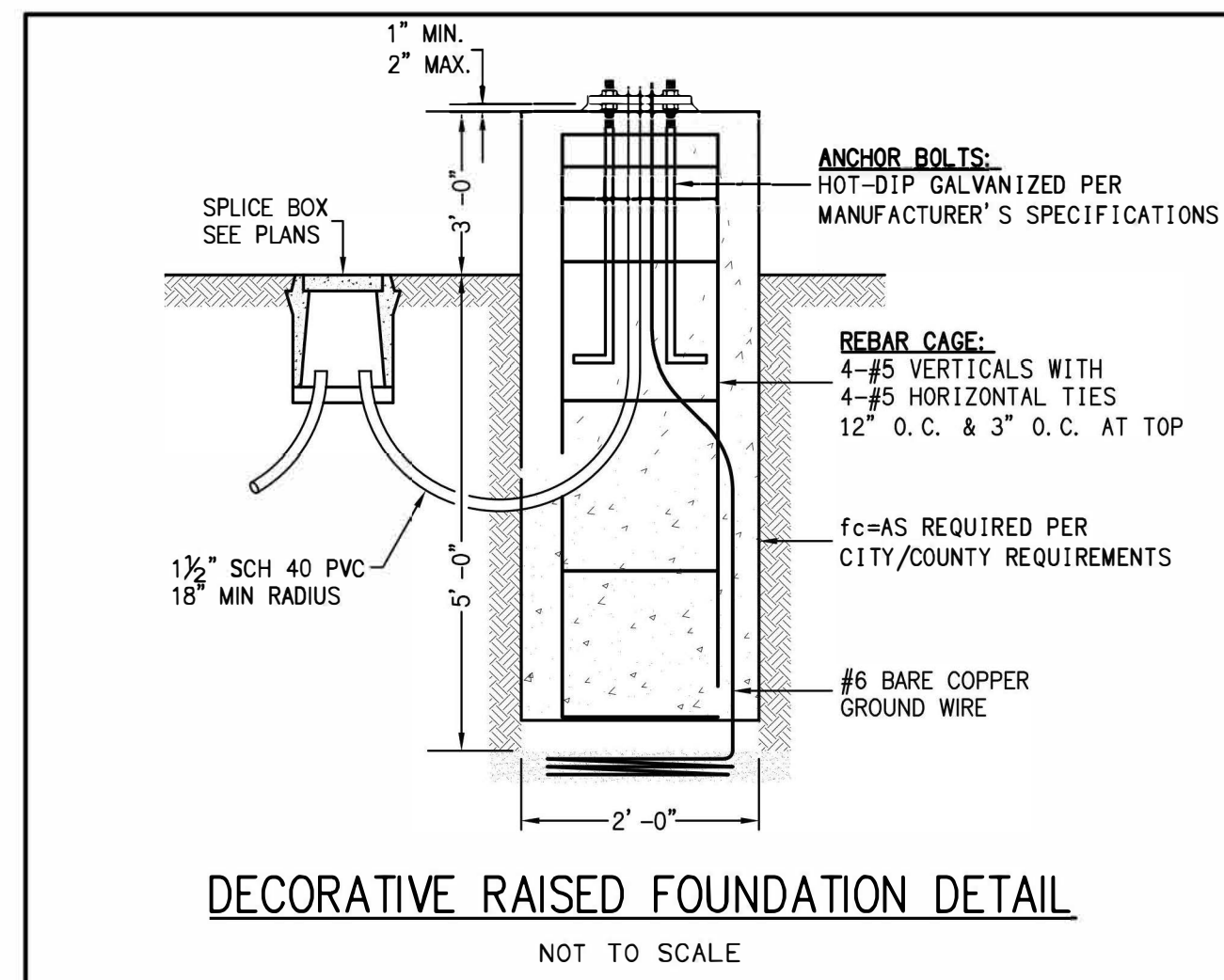
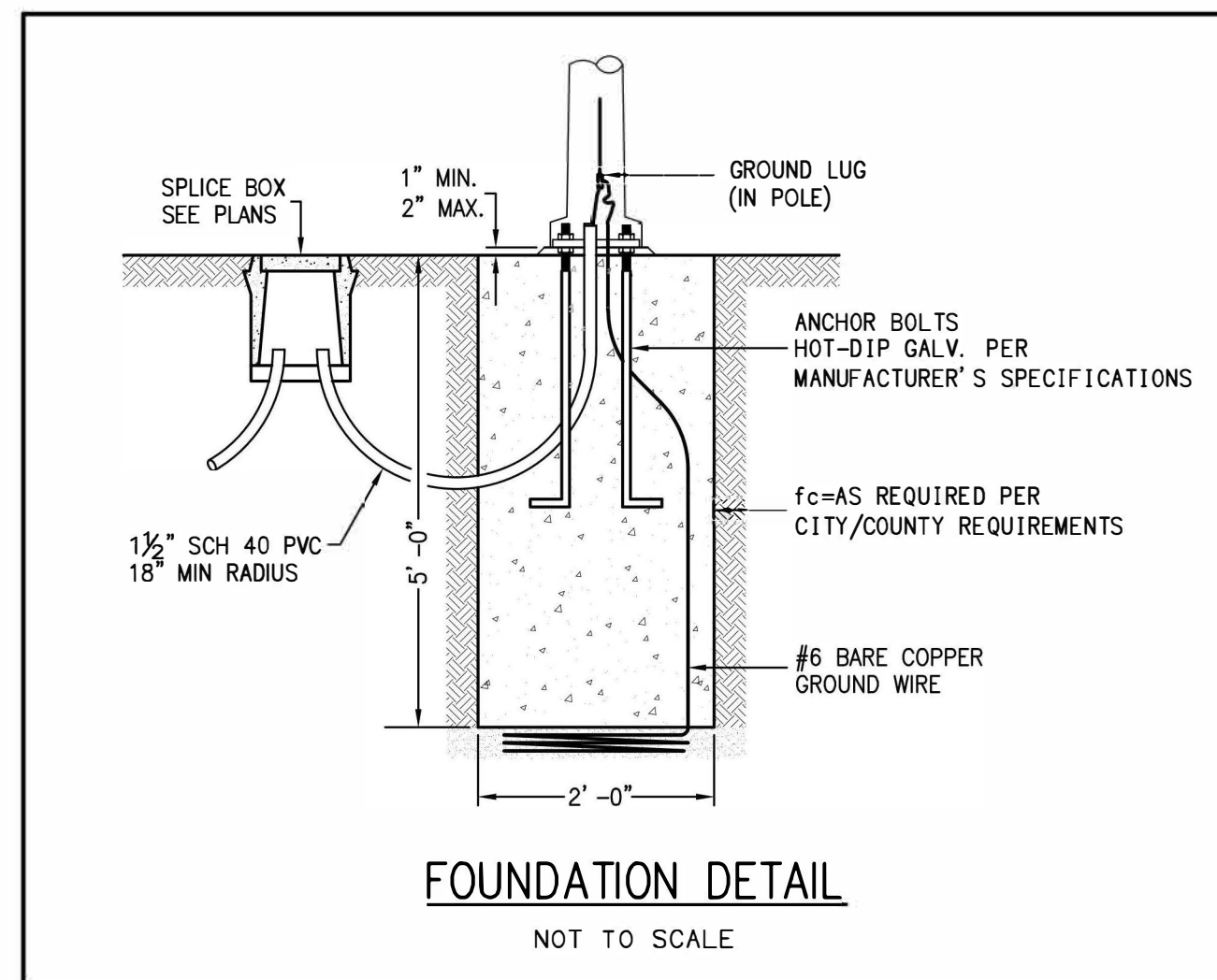
<p>813 First Street Brentwood, CA 94513 (925) 240-2595 (925) 240-7013 fax www.tarrar.com</p>	<h1>TARRAR</h1> <div style="display: flex; justify-content: space-around; align-items: center;"> <div style="border: 1px solid red; padding: 2px 10px;">UTILITY</div> <div style="border: 1px solid red; padding: 2px 10px; text-align: center;">  </div> <div style="border: 1px solid red; padding: 2px 10px;">CONSULTANTS</div> </div>	<ul style="list-style-type: none"> <li>• Planning</li> <li>• Design</li> <li>• Estimating</li> <li>• Joint Trench</li> <li>• Street Lighting</li> <li>• Fiber Optic</li> <li>• I-24</li> <li>• PG&amp;E Gas Design</li> <li>• PG&amp;E Elec Design</li> <li>• M.E.P. Design</li> <li>• Cost Analysts</li> <li>• Due Diligence</li> </ul>
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## STREET LIGHTING GENERAL NOTES AND DETAILS

DENOVA HOMES  
WILDFLOWER TOWNHOMES 2  
ANTIOCH CALIFORNIA

NO.	REVISONS	BY	DATE	DATE: FEBRUARY 2024	DATE LAST WORKED ON: 4/11/2023		SHEET
				SCALE: NOT TO SCALE	DRAWN: SM      CHECKED: KT		SL1
				JOB NO.: 223027	PRELIMINARY NOT FOR CONSTRUCTION		SL4
				INTENT TO CONSTRUCT			OF
							SHEETS





# VOLTAIRE MINI ARCHITECTURAL WALL PACK

CATALOG #					TYPE	
PROJECT					NOTES	
<b>EXAMPLE</b> <b>VWM H - L20/40V - T3 - DBZ - SDGL - OPTIONS - DIM - UNV</b>						
SERIES	TYPE	LEN/PAKAGE	CR/LA CCT	DISTRIBUTION	FINISH	SHIELDING
						OPTIONAL ACCESSORIES
						ORDER
						OUTLINE

## CROSS SECTIONS

### VWMH (Weight: 6.5 Lbs)

### VWMV (Weight: 6.5 Lbs)

## ORDERING INFORMATION

SERIES		Voltaire Mini Architectural Wall Pack
VWM		
TYPE		
H	Horizontal	
V	Vertical	

### LED PACKAGE

See back for fixture performance data.

LUMEN PACKAGE	CR/MINIAL LUMENS	MINIMUM CRI & CCT	AVERAGE SYSTEM WATTAGE
L10	1,000	830 - 80 CRI, 3000K	13
L17	1,700	840 - 80 CRI, 4000K 750 - 70 CRI, 5000K	16
L20	2,000	730 - 70 CRI, 3000K 740 - 70 CRI, 4000K 750 - 70 CRI, 5000K	25

### DISTRIBUTION

T3	Luminaire distribution (L10 & L17 only)
TL	Type III (L20 only)

### FINISH OPTIONS

BLK	For commercial, visit the VWM at <a href="http://www.hew.com">www.hew.com</a>
DBZ	Black (RAL #9004)
DBR	Dark bronze
GRAY	Medium bronze
SILV	Standard gray
WHT	Satin aluminum (RAL #9008) White (RAL #9003)

### SHIELDING

SDGL	Solar® diffused textured tempered glass lens
CGL	Clear tempered glass lens

### OPTIONS

PC	See back for option details.
SP10	Factory-installed button-style photocell (120V, 108V, or 277V only; must specify voltage)
UNV	10kA/10kV surge protection

### DRIVER

DIM	Dimming driver provided for 0-10V controls
-----	--

### VOLTAGE

120	120V
208	208V
277	277V
UNV	120-277V

## FEATURES

### GENERAL

- Engineered with the highest quality materials to ensure reliability, performance, and quality.
- Provides security and accent lighting for walkways, entries, perimeter, and facades.
- Intended for use in both daylight and nighttime applications.
- Aesthetically designed horizontal and vertical housings blend seamlessly with a variety of architectural styles.
- Proportionally modified to allow runoff of dirt and water for an always clean appearance.
- Optional energy-saving photocell available.
- Rated >50,000 hours at 70% lumen maintenance (L70).
- ANSI 9000K, 4000K, or 5000K CCT.
- Minimum 70 CRI.
- Available in six standard finish options.
- This fixture is proudly made in the USA.

### THERMAL

- Integral die-cast aluminum heatsink provides optimal passive thermal management.
- Concealed heatsink design preserves architectural appearance.
- Rated ambient operating temperature 30°C to 50°C (L10, L17), -30°C to 45°C (L20).

### OPTICAL

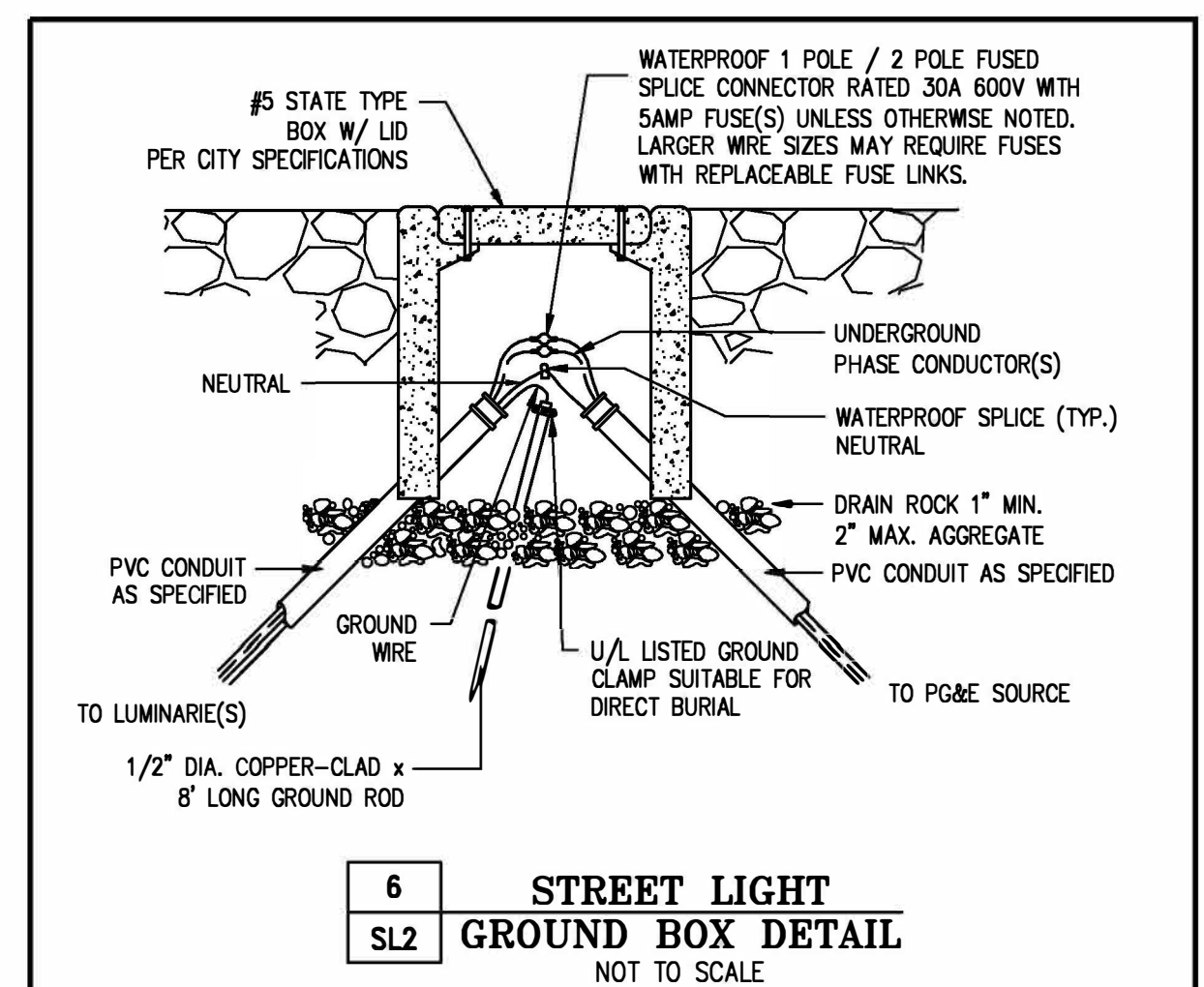
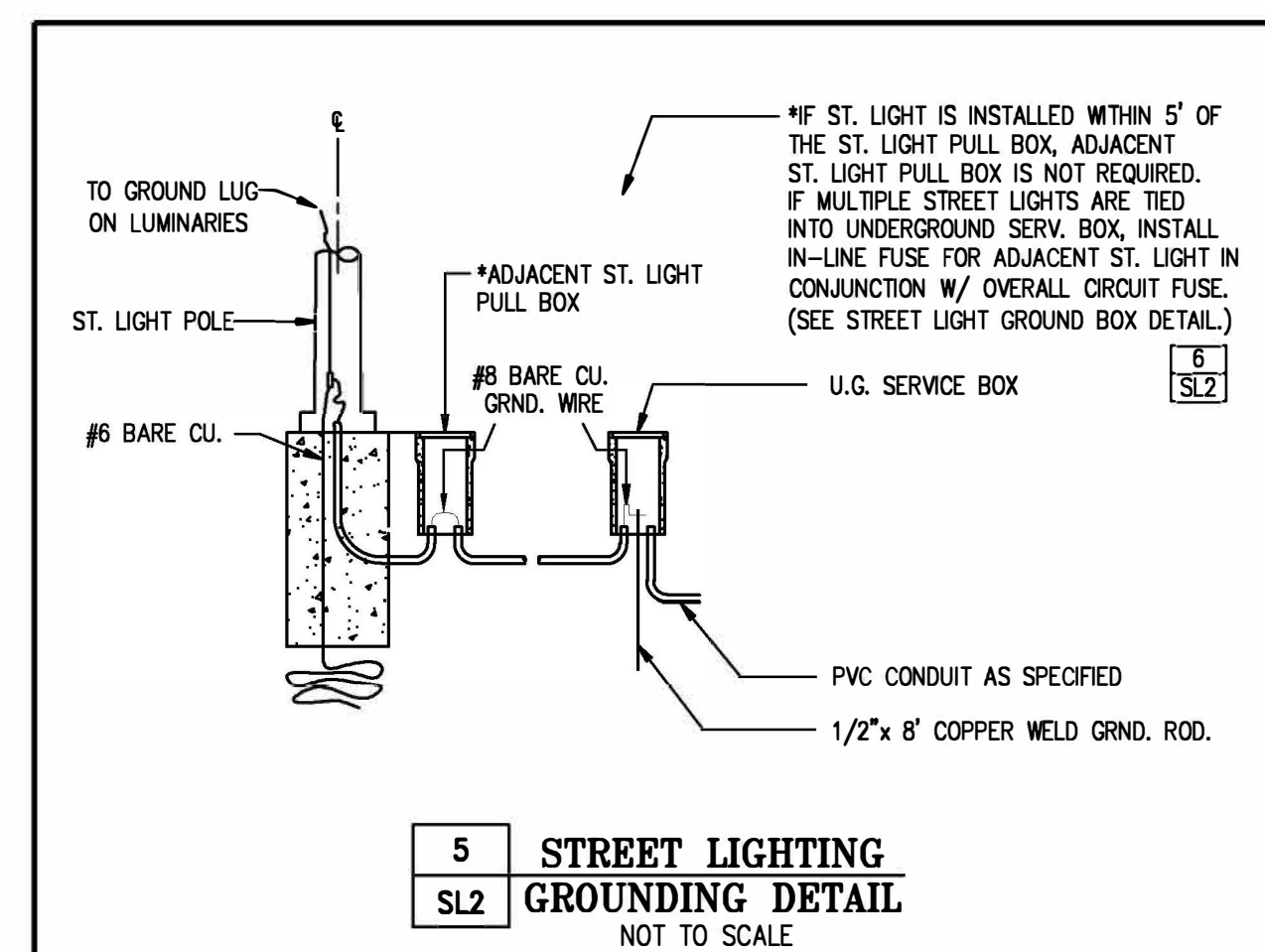
- Fat cutoff, dark sky compliant optic.
- Mounting applications only place light where it's needed with minimal glare.

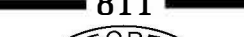
### ELECTRICAL

- 0-10V dimming standard
- 10kA/10kV surge protection optional.

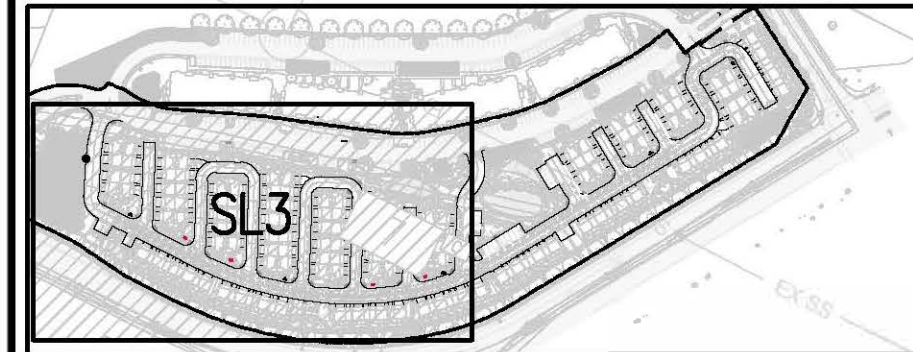
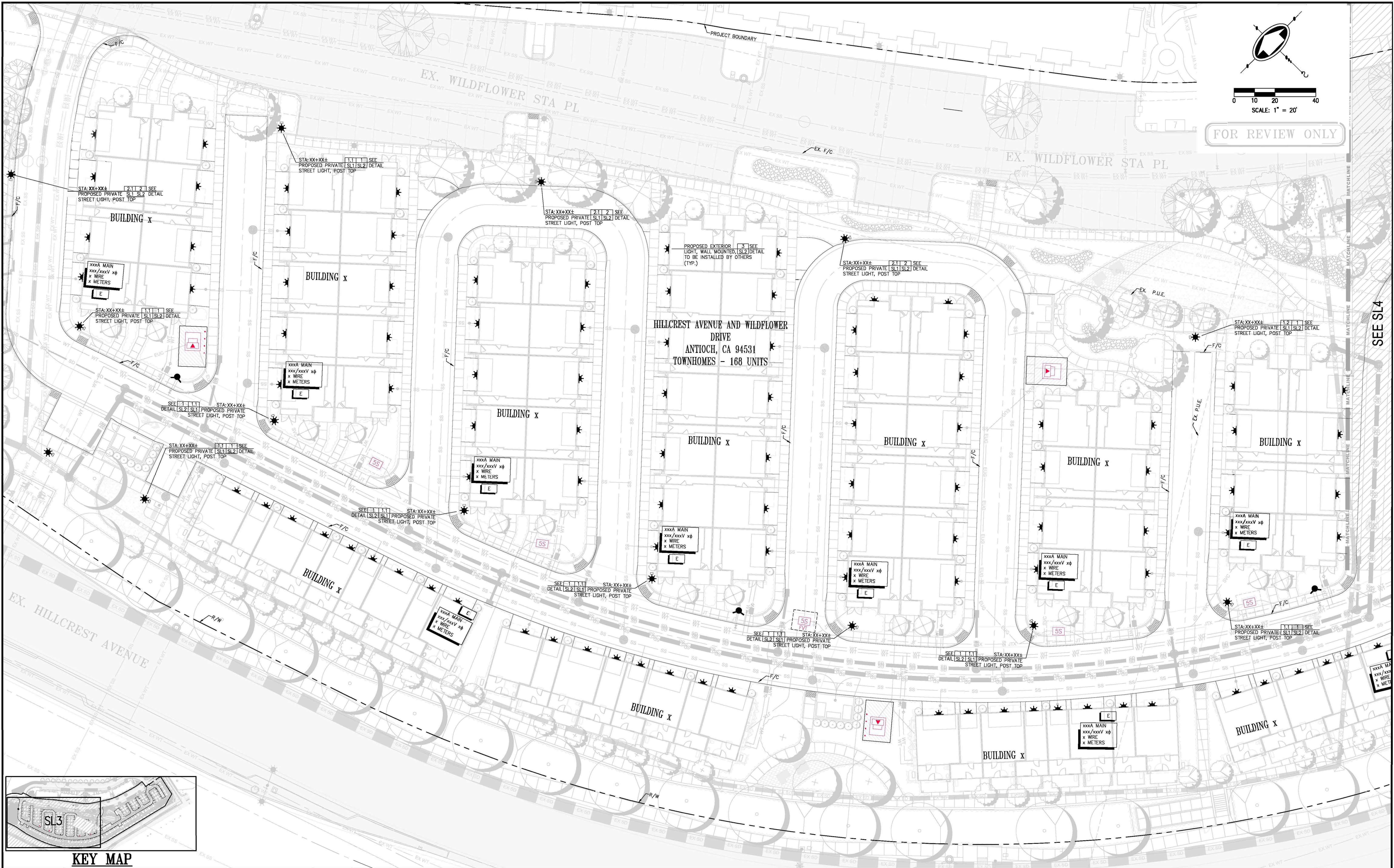
H.E. Williams, Inc. • Carthage, Missouri • [www.hew.com](http://www.hew.com) • 417-358-4065

Wall Mount  
Page 10



NO	REVISIONS	BY	DATE	DATE: FEBRUARY 2024	DATE LAST WORKED ON: 4/11/2023		SHEET
				SCALE: NOT TO SCALE	DRAWN: SM      CHECKED: KT		SL2
				JOB NO.: 223027	PRELIMINARY NOT FOR CONSTRUCTION		SL4
				INTENT TO CONSTRUCT			OF
							SHEETS





KEY MAP

813 First Street  
Brentwood, CA 94513  
(925) 240-2595  
(925) 240-7013 fax  
www.tarrar.com

TARRAR

UTILITYCONSULTANTS

• Planning

• Design

• Estimating

• Joint Trench

• Street Lighting

• Fiber Optic

• T-24

• PG&E Gas Design

• PG&E Elec Design

• H.E.P. Design

• Cost Analysis

• Due Diligence

STREET LIGHTING SITE PLAN

DENOVA HOMES

WILDFLOWER TOWNHOMES 2

ANTIOCH CALIFORNIA

NO.	REVISIONS	BY	DATE

DATE: FEBRUARY 2024

SCALE: 1" = 20'

JOB NO.: 223027

DATE LAST WORKED ON: 1/29/2024

DRAWN: SM

CHECKED: KT

PRELIMINARY

NOT FOR CONSTRUCTION

INTENT TO CONSTRUCT

811

CALL BEFORE YOU DIG

UTAH

UNDERGROUND SERVICE UTILITIES

SHEET

SL3

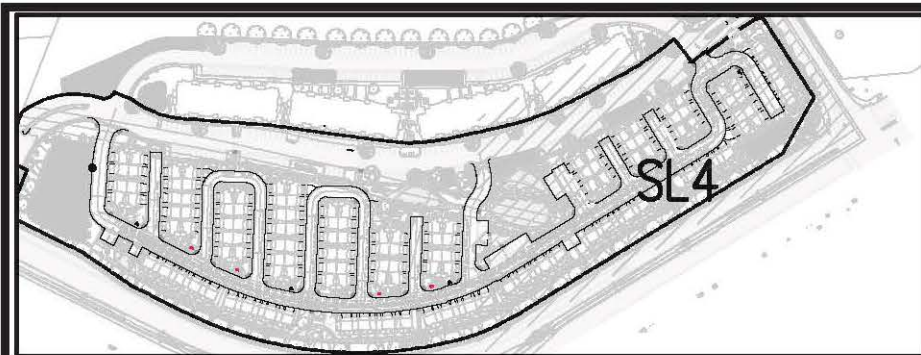
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OF

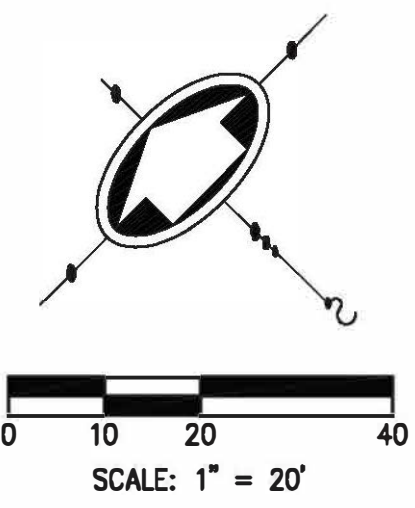
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SHEETS

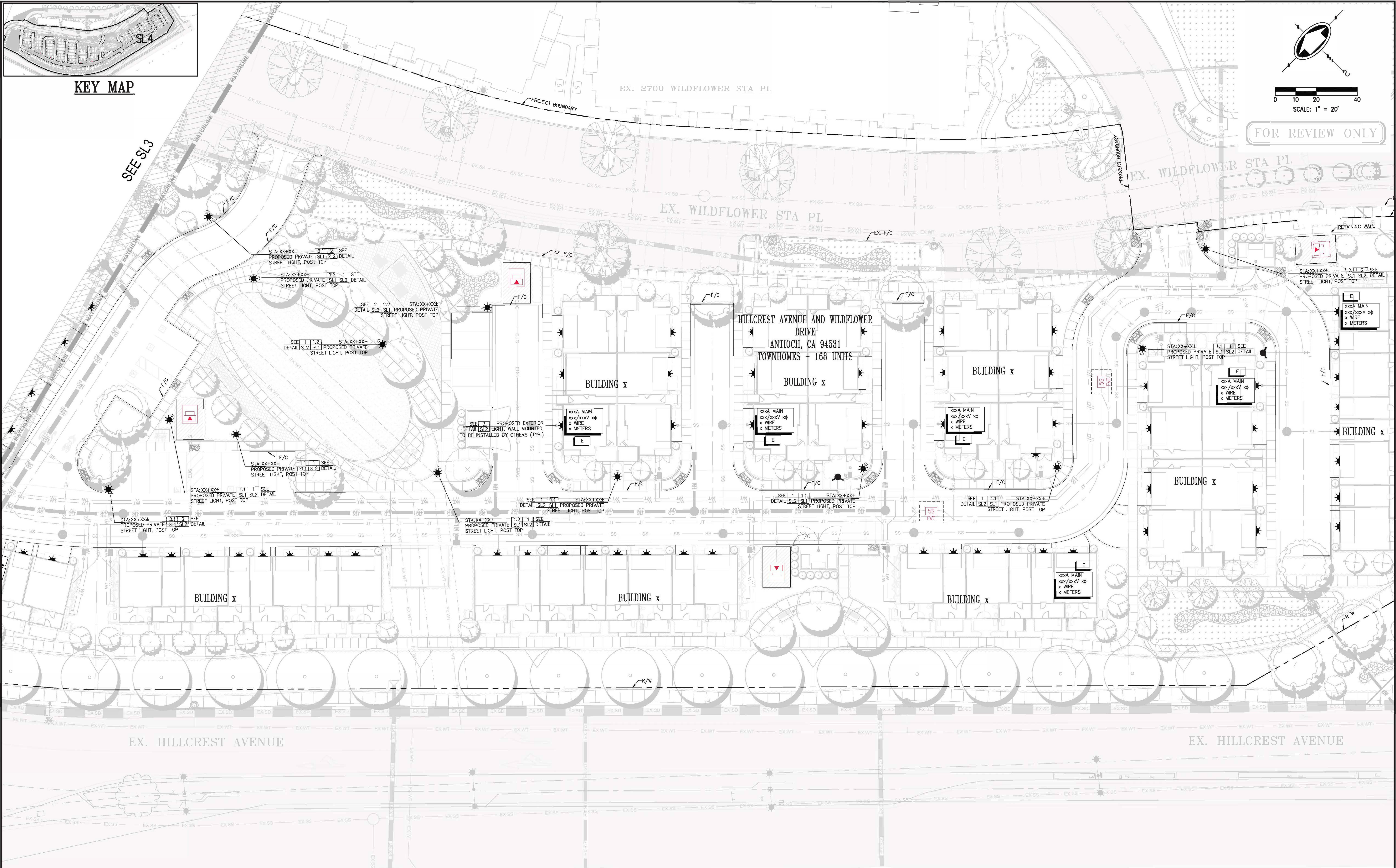




KEY MAP



FOR REVIEW ONLY



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811

CALL BEFORE YOU DIG

UNDERGROUND SERVICE ALERT

SHEET

SL4

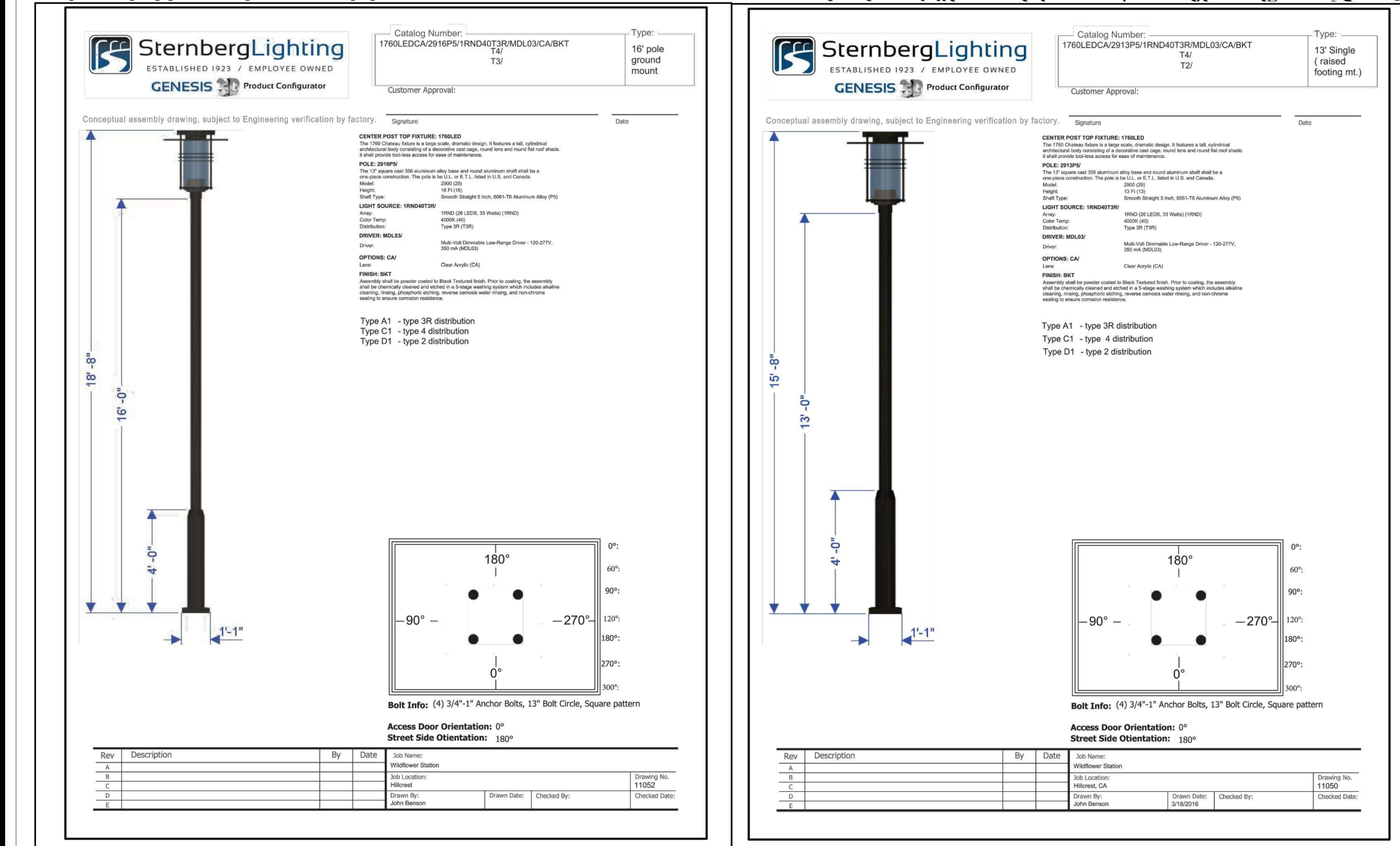
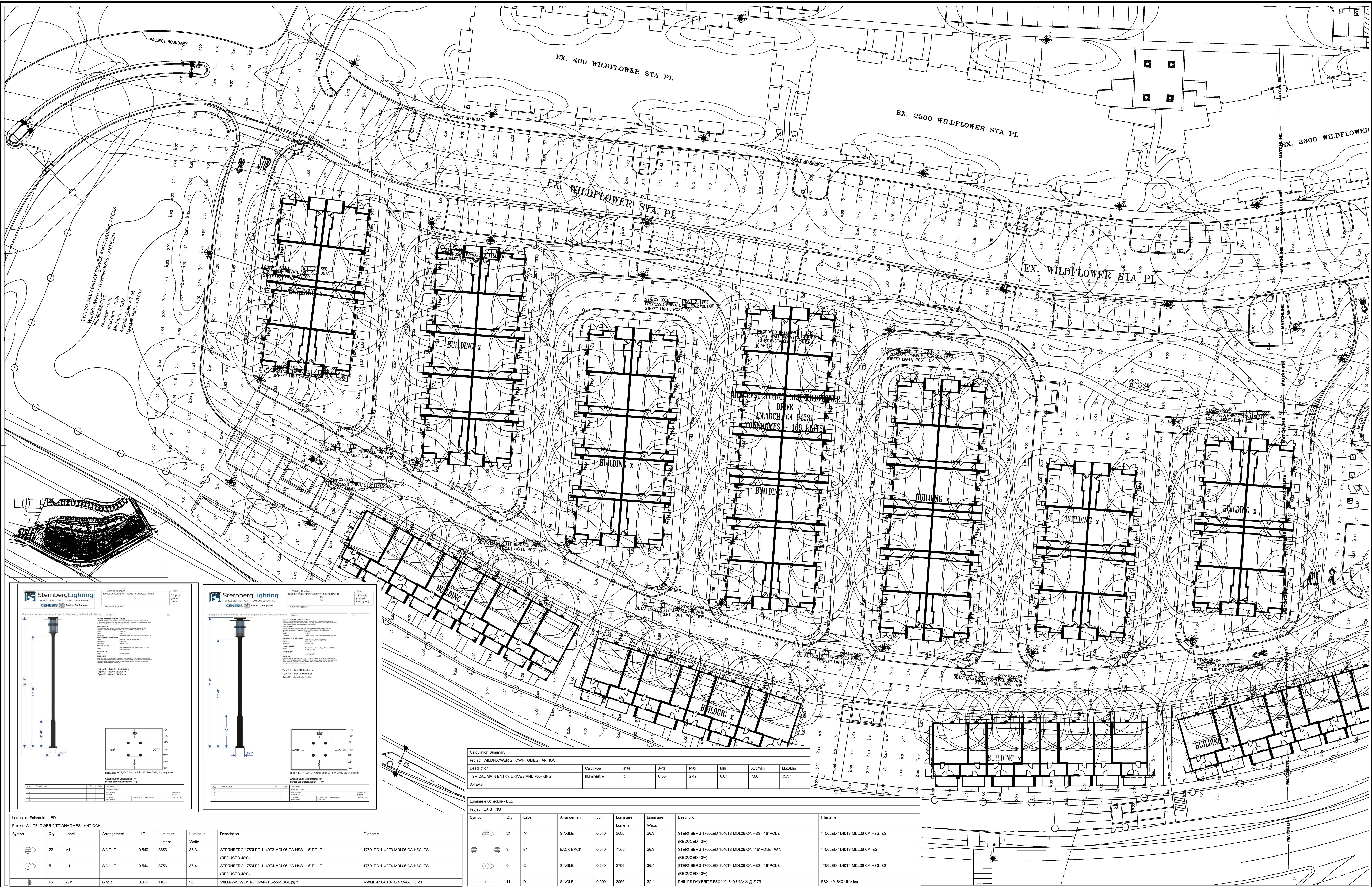
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OF

A46

SHEETS





Luminaire Schedule - LED						
Project: WILDFLOWER 2 TOWNHOMES - ANTIOCH						
Symbol	Qty	Label	Arrangement	LLF	Luminaire Lumens	Description
	22	A1	SINGLE	0.540	3656	STERNBERG 1750LED-1L40T3-MDL06-CA-HSS - 16' POLE (REDUCED 40%)
	5	C1	SINGLE	0.540	3756	STERNBERG 1750LED-1L40T4-MDL06-CA-HSS - 16' POLE (REDUCED 40%)
	161	WM	Single	0.900	1163	WILLIAMS WMMH-L10-840-TL-XXX-SDGL @ 8'

Calculation Summary						
Project: WILDFLOWER 2 TOWNHOMES - ANTIOCH						
Description: TYPICAL MAIN ENTRY DRIVES AND PARKING AREAS						
CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
Illuminance	Fc	0.55	2.49	0.07	7.86	35.57

Luminaire Schedule - LED						
Project: EXISTING						
Symbol	Qty	Label	Arrangement	LLF	Luminaire Lumens	Description
	21	A1	SINGLE	0.540	3656	STERNBERG 1750LED-1L40T3-MDL06-CA-HSS - 16' POLE (REDUCED 40%)
	3	B1	BACK-BACK	0.540	4260	STERNBERG 1750LED-1L40T3-MDL06-CA - 16' POLE TWIN (REDUCED 40%)
	5	C1	SINGLE	0.540	3756	STERNBERG 1750LED-1L40T4-MDL06-CA-HSS - 16' POLE (REDUCED 40%)
	11	D1	SINGLE	0.900	3965	PHILIPS DAYBRITE FSX440L840-UNV-X @ 7'7"

ALL VALUES SHOWN ARE MAINTAINED HORIZONTAL FOOTCANDLES AT GRADE

PHOTOMETRIC DATA USED AS INPUT FOR THESE CALCULATIONS IS BASED ON ESTABLISHED IES PROCEDURES AND PUBLISHED LAMP RATINGS. FIELD PERFORMANCE WILL DEPEND ON ACTUAL LAMP, BALLAST, ELECTRICAL, AND SITE CHARACTERISTICS.

Calculations have been performed according to IES standards and good practice. Some differences between measured values and calculated results may occur due to tolerances in calculation methods, testing procedures, component performance, measurement techniques and field conditions such as voltage and temperature variations. Input data used to generate the attached calculations such as room dimensions, reflectances, furniture and architectural elements significantly affect the lighting calculations. If the real environment conditions do not match the input data, differences will occur between measured values and calculated values.

Associated Lighting Representatives, Inc.

ASSOCIATED LIGHTING REPRESENTATIVES, INC.  
7777 PARDEE LANE  
P.O. BOX 2265  
OAKLAND, CA 94621  
PHONE: (510) 638-0158 - FAX (510) 638-2908

REPORT FOR: TARRAR UTILITY CONSULTANTS  
BY: APPLICATIONS ENGINEERING, RAMON ZAPATA  
SALES REPRESENTATIVE: ALR; TIM HALEY

AGI32 VERSION 20.1  
AGI (C) 2021 LIGHTING ANALYSTS, INC.  
10288 W. CENTENNIAL ROAD, SUITE 202  
LITTLETON, CO 80127

PROJECT DESCRIPTION  
WILDFLOWER 2 TOWNHOMES

DRAWING NO. / INPUT FILE  
21801HAL-R2.DWG / 21801HAL-R2.A32

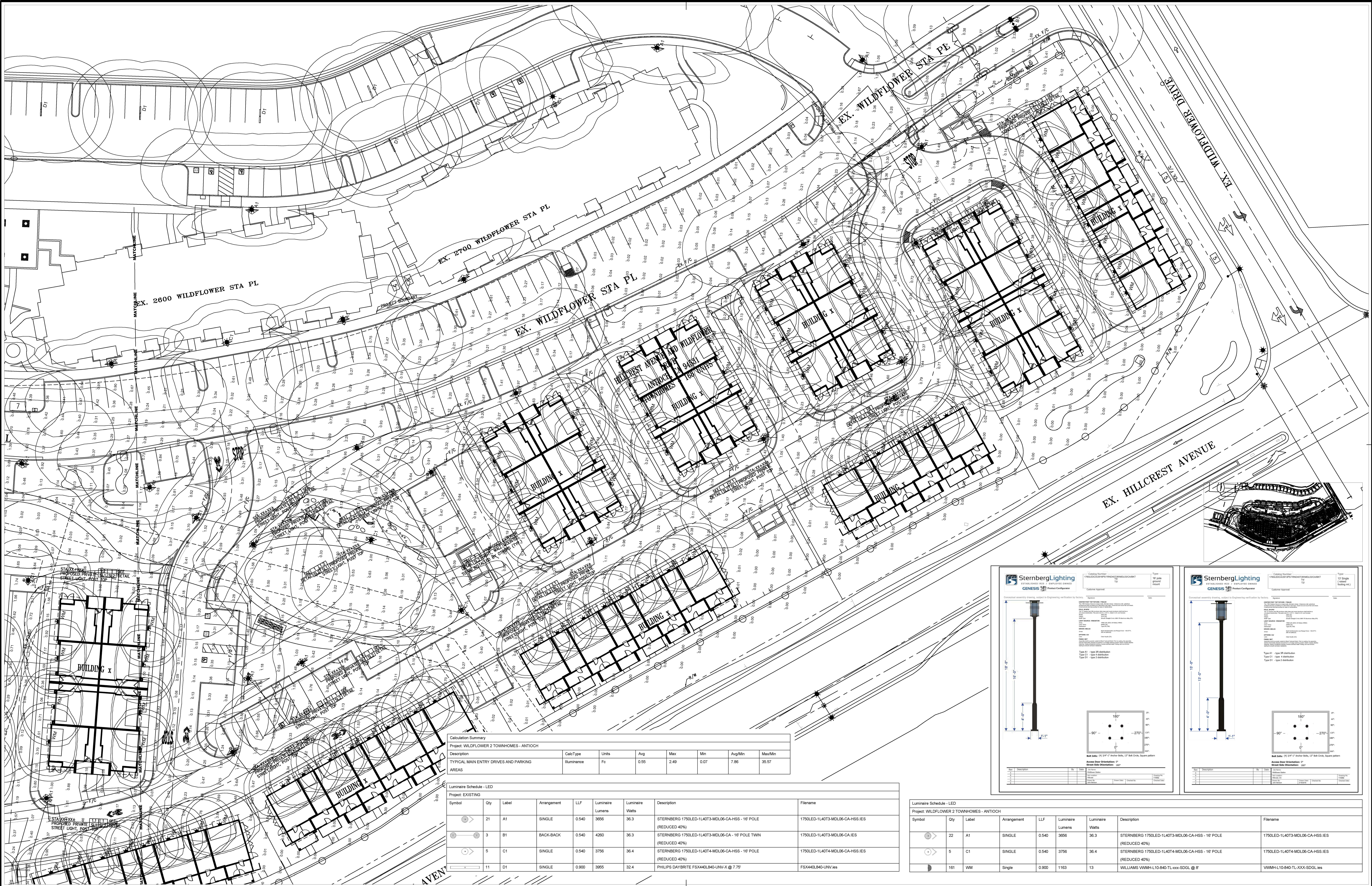
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DATE  
02.01.2024

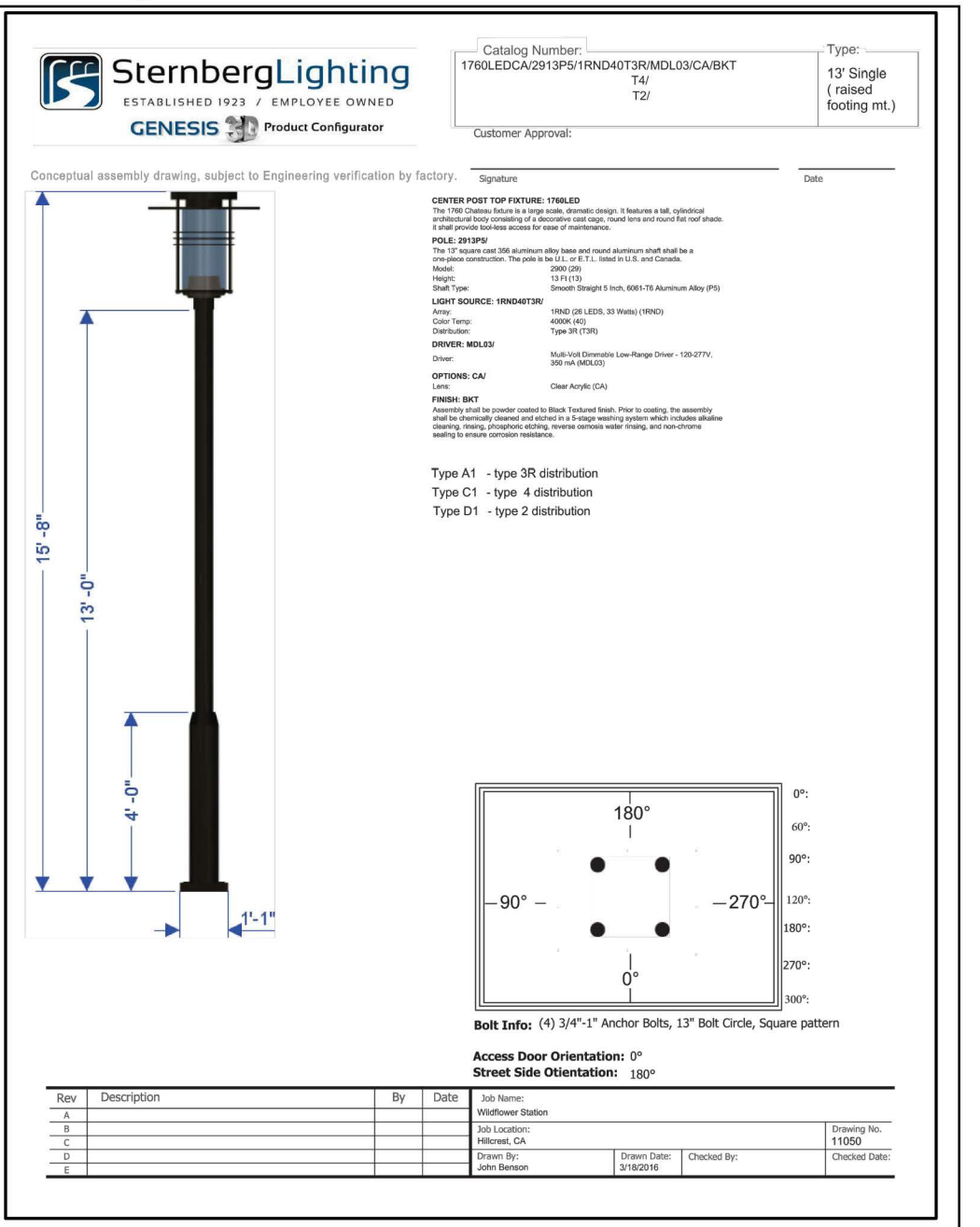
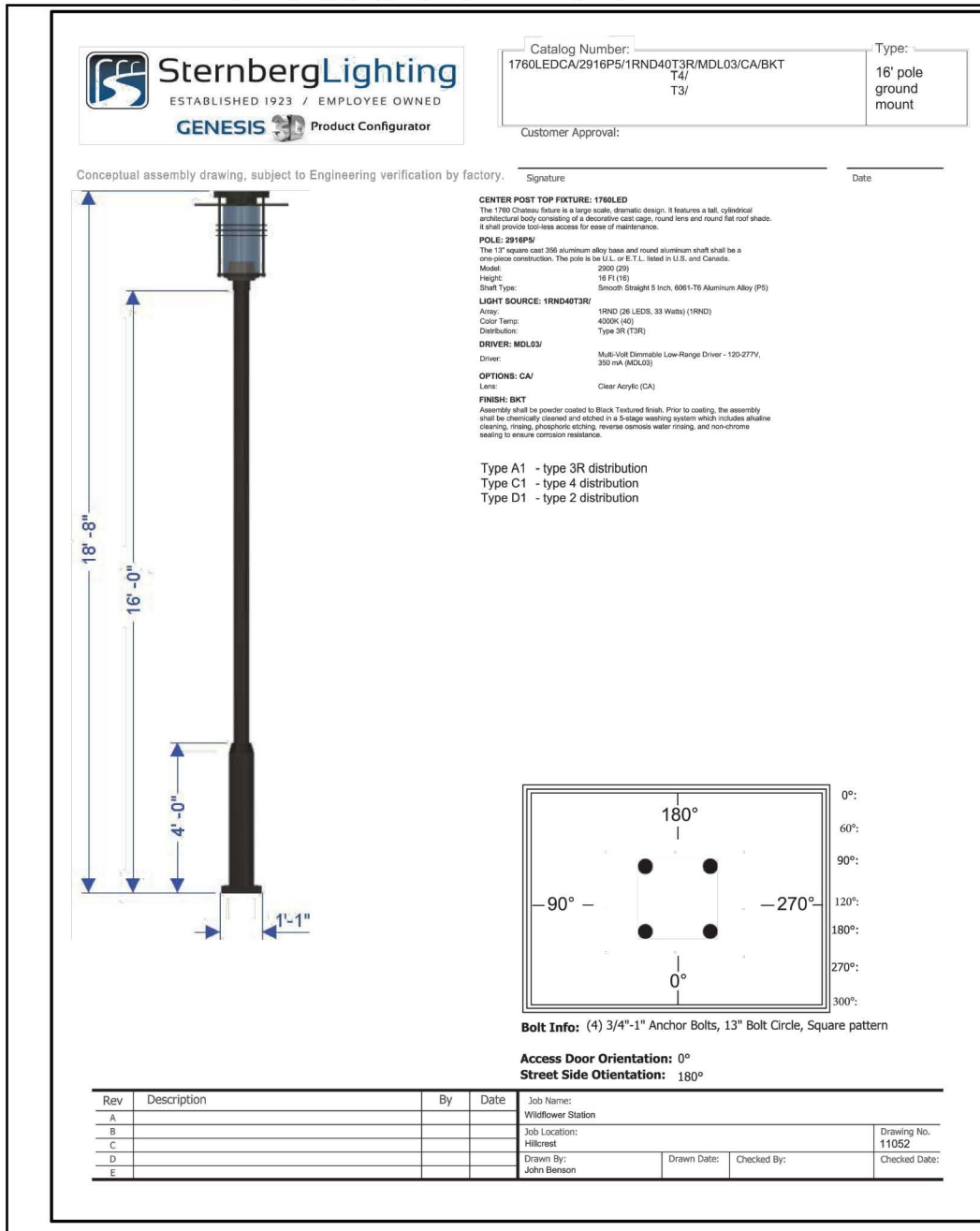
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Calculation Summary							
Project: WILDFLOWER 2 TOWNHOMES - ANTIOCH							
Description							
TYPICAL MAIN ENTRY DRIVES AND PARKING AREAS			Calc Type	Units	Avg	Max	Min
			Illuminance	Fc	0.55	2.49	0.07
							Avg/Min
							Max/Min
							7.86
							35.57

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Project: EXISTING							
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	5	C1	SINGLE	0.540	3756	36.4	STERNBERG 1750LED-1L40T4-MDL06-CA-HSS - 16" POLE (REDUCED 40%)
	11	D1	SINGLE	0.900	3955	32.4	PHILIPS DAYBRITE FSX440L840-UNV-X @ 7.75'
							Filename
							1750LED-1L40T3-MDL06-CA-HSS-IES
							1750LED-1L40T3-MDL06-CA-IES
							1750LED-1L40T4-MDL06-CA-HSS-IES
							FSX440L840-UNV-ies



Luminaire Schedule - LED							
Project: WILDFLOWER 2 TOWNHOMES - ANTIOCH							
Symbol	Qty	Label	Arrangement	LLF	Luminaire Lumens	Luminaire Watts	Description
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WILDFLOWER 2 TOWNHOMES

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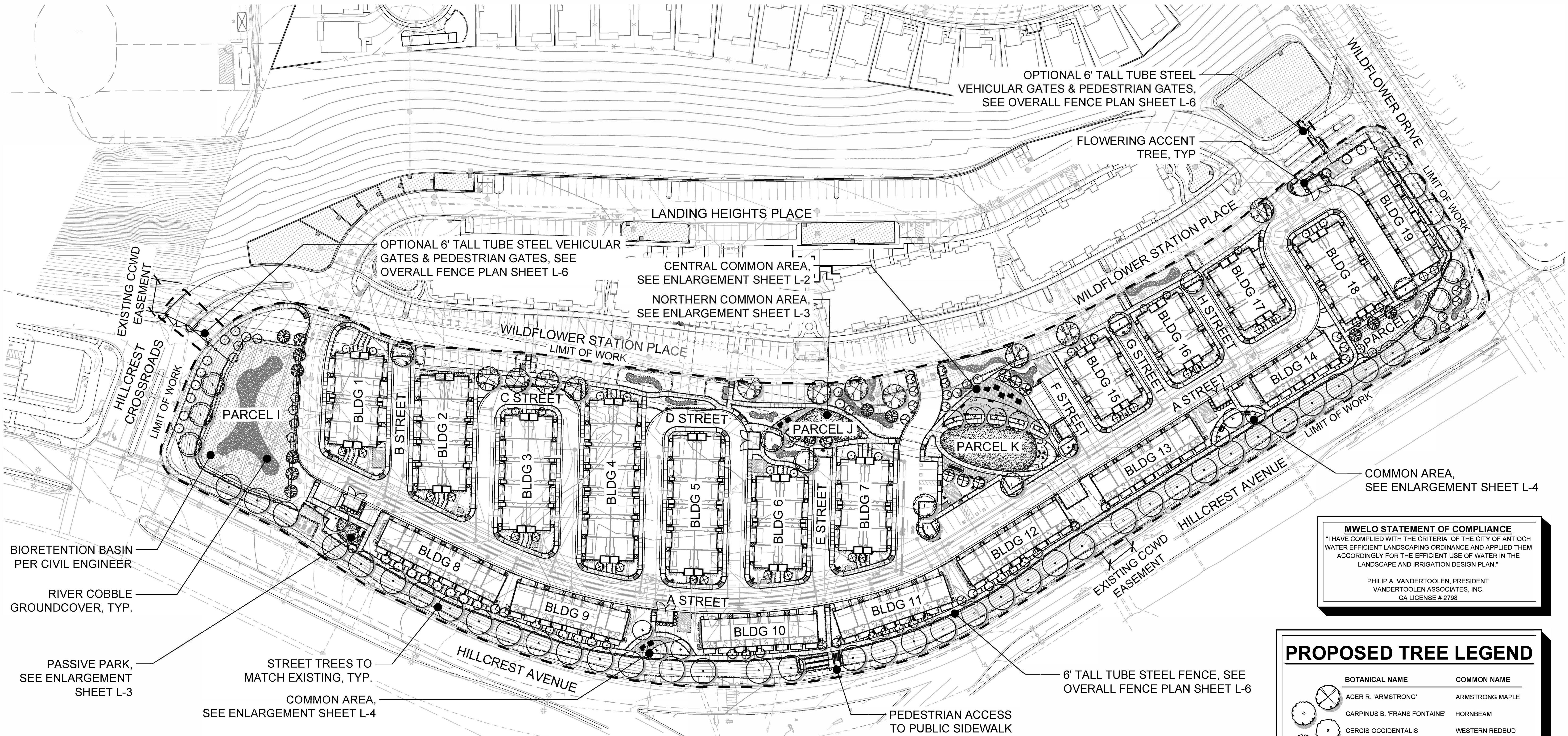
SCALE  
1" = 30'

SHEET  
2 OF 2

DATE  
02.01.2024

REV  
2





BIORETENTION BASIN  
PER CIVIL ENGINEER

RIVER COBBLE  
GROUND COVER, TYP.

PASSIVE PARK,  
SEE ENLARGEMENT  
SHEET L-3

STREET TREES TO  
MATCH EXISTING, TYP.  
  
COMMON AREA,  
SEE ENLARGEMENT SHEET L-4

OPTIONAL 6' TALL TUBE STEEL  
VEHICULAR GATES & PEDESTRIAN GATES, SEE  
OVERALL FENCE PLAN SHEET L-6

LANDING HEIGHTS PLACE  
  
CENTRAL COMMON AREA,  
SEE ENLARGEMENT SHEET L-2  
  
NORTHERN COMMON AREA,  
SEE ENLARGEMENT SHEET L-3

OPTIONAL 6' TALL TUBE STEEL  
VEHICULAR GATES & PEDESTRIAN GATES,  
SEE OVERALL FENCE PLAN SHEET L-6

FLOWERING ACCENT  
TREE, TYP

WILDFLOWER STATION PLACE  
LIMIT OF WORK

COMMON AREA,  
SEE ENLARGEMENT SHEET L-4

**MWEO STATEMENT OF COMPLIANCE**

"I HAVE COMPLIED WITH THE CRITERIA OF THE CITY OF ANTIOCH WATER EFFICIENT LANDSCAPING ORDINANCE AND APPLIED THEM ACCORDINGLY FOR THE EFFICIENT USE OF WATER IN THE LANDSCAPE AND IRRIGATION DESIGN PLAN."

PHILIP A. VANDERTOOLEN, PRESIDENT  
VANDERTOOLEN ASSOCIATES, INC.  
CA LICENSE # 2798

**PLANTING NOTES**

- THE PLANTING DESIGN FOR THE SITE IS DROUGHT TOLERANT AND CONSISTS OF A BALANCE OF EVERGREEN AND DECIDUOUS PLANTING AS WELL AS NATIVE AND ORNAMENTAL PLANTING.
- ALL SHRUBS AND GROUND COVER MATERIAL SELECTED HAVE A WUCOLS WATER USE RATING OF VERY LOW TO MODERATE.
- PROPOSED TREE CANOPIES ARE SHOWN AT 85% MATURE GROWTH DIAMETER IN ORDER TO ENSURE THERE IS SUFFICIENT ROOM FOR TREE GROWTH AND HEALTH.
- TREES, SHRUBS AND GROUNDCOVERS SHALL BE PROTECTED FROM VEHICULAR ENCROACHMENT BY CURBS PER CITY ODS 3.3.1.G.
- PROPOSED TREES SHALL BE INSTALLED SECURELY WITH DOUBLE STAKING PER CITY ODS 3.3.1.I.
- TREES, SHRUBS AND GROUNDCOVERS SHALL NOT INTERFERE WITH SITE LIGHTING OR RESTRICT EMERGENCY ACCESS TO FIRE HYDRANTS OR FIRE ALARM BOXES PER CITY ODS 3.3.1.H.
- UTILITY METERS, TRANSFORMERS AND OTHER SERVICES ELEMENTS SHALL BE SCREENED WITH PLANT MATERIAL WHERE POSSIBLE PER EAST LONE OAK SPECIFIC PLAN.
- PROPOSED TREE LOCATIONS ARE BASED ON STANDARD JOINT TRENCH LOCATIONS. FINAL TREE LOCATIONS TO BE DETERMINED WHEN FINAL UTILITY LOCATIONS ARE PROVIDED. TREE PLACEMENT MAY BE ADJUSTED AND FINAL TREE COUNT MAY CHANGE.
- TREES AND LARGE SHRUBS SHALL NOT BE LOCATED UNDER OVERHEAD LINES OR ON UNDERGROUND UTILITIES. TREES AND LARGE SHRUBS SHALL BE LOCATED:
  - 6' MINIMUM FROM EDGE OF DRIVEWAY, WATER METER, GAS METER AND SEWER LATERALS.
  - 20' MINIMUM FROM BEGINNING OF CURB RETURNS AT INTERSECTIONS.
  - 15' MINIMUM FROM UTILITY POLES AND STREETLIGHTS.
  - 8' MINIMUM FROM FIRE HYDRANTS, SPRINKLER AND STANDPIPE CONNECTIONS. PER CITY ODS 3.3.1.H
- TREES PLANTED WITHIN 10' OF PAVEMENT SHALL HAVE ROOT BARRIER INSTALLED PER CITY ODS 3.3.1.I.

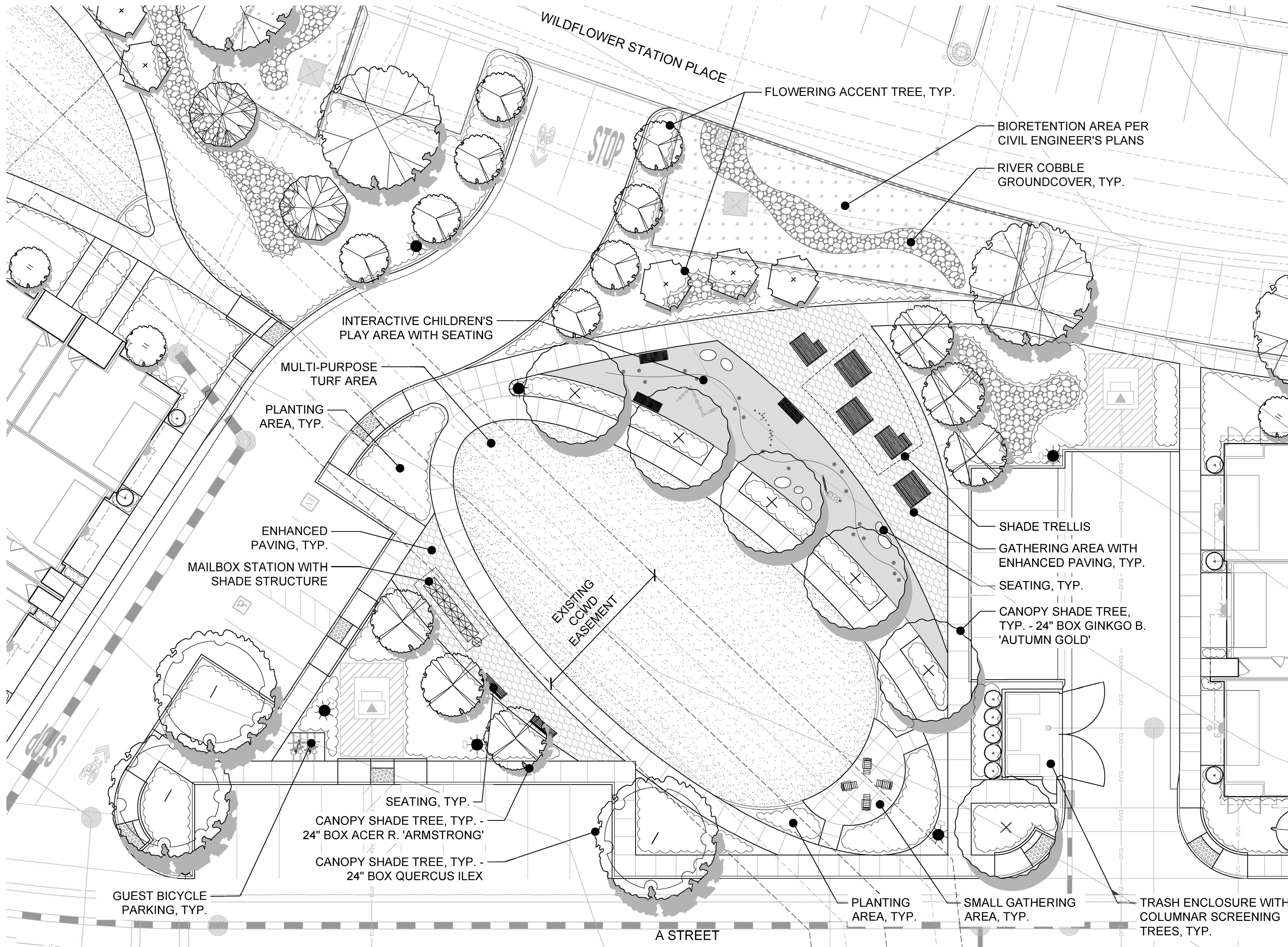
OPEN SPACE CHART		
	REQUIRED	PROVIDED
OPEN SPACE (AMC 9-5.706):	200 SF/UNIT	385 SF/UNIT
*TOTAL SF	31,800 SF	61,276 SF
PRIVATE (60 SF/UNIT MIN.)	9,540 SF	16,022 SF
COMMON:		
CENTRAL OPEN AREA - PARCEL K		18,239 SF
PASSIVE PARK		3,312 SF
OPEN PLAY AREA - PARCEL J		15,694 SF
COMMON BETWEEN BLDG 9 & 10		4,297 SF
COMMON BETWEEN BLDG 13 & 14		3,712 SF
TOTAL SF		45,254 SF
**LANDSCAPING (AMC 9-5.708):	25% OF SITE (1.6 AC)	32% OF SITE (2.8 AC)

\*OPEN SPACE TOTAL DOES NOT INCLUDE BIORETENTION AREA.  
\*\*LANDSCAPING TOTAL INCLUDES BIORETENTION AREA.

- NOTES:
- SEE SHEET L-9 FOR COMPLETE PLANT PALETTE INCLUDING PROPOSED TREES, SHRUBS, GROUNDCOVERS, GRASSES, BIORETENTION PLANTING, AND TURF. COMPLETE PLANT PALETTE IDENTIFIES BOTANICAL NAME, COMMON NAME, WATER USE, SPECIES NATIVE OR ADAPTIVE, CONTAINER SIZE, AND HEIGHT AND WIDTH OF PROPOSED PLANT MATERIAL.
  - LANDSCAPED AREAS SHALL INCORPORATE PLANTINGS UTILIZING THREE-TIER SYSTEM: (1) GRASSES AND GROUNDCOVERS, (2) SHRUBS AND VINES, AND (3) TREES PER CITY OF ANTIOCH ODS 3.3.1.C.

PROPOSED TREE LEGEND		
	BOTANICAL NAME	COMMON NAME
	ACER R. 'ARMSTRONG'	ARMSTRONG MAPLE
	CARPINUS B. 'FRANS FONTAINE'	HORNBEAM
	CERCIS OCCIDENTALIS	WESTERN REDBUD
	CORNUS KOUSA 'SATOMI'	DOGWOOD
	GINKGO B. 'AUTUMN GOLD'	MAIDENHAIR TREE
	LAGERSTROEMIA I. 'MUSKOGEE'	GRAPE MYRTLE
	LAURUS 'SARATOGA'	SARATOGA SWEET BAY
	PISTACIA C. 'KEITH DAVEY'	CHINESE PISTACHE
	PLATANUS A. 'COLUMBIA'	LONDON PLANE TREE
	PODOCARPUS M. 'MAKI'	SHRUBBY YEW PINE
	PRUNUS C. 'KRAUTER VESUVIUS'	PURPLE LEAF PLUM
	QUERCUS ILEX	HOLLY OAK
	ZELKOVA S. 'MUSASHINO'	SAWLEAF ZELKOVA





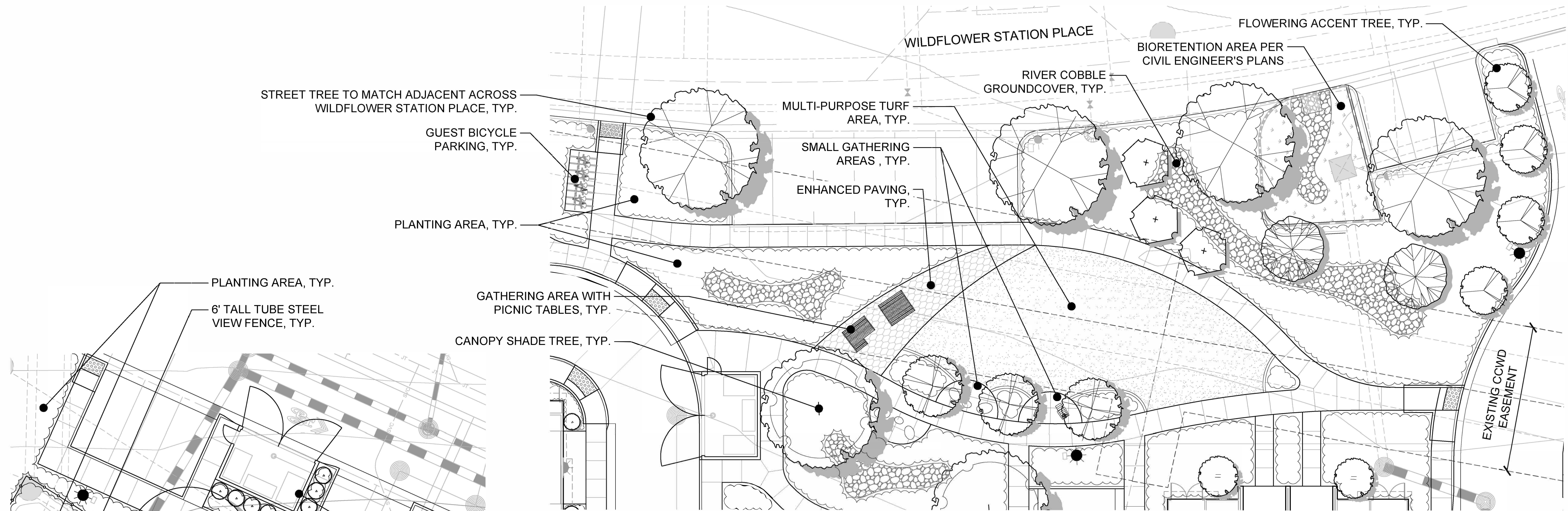
## PROPOSED TREE LEGEND

BOTANICAL NAME	COMMON NAME
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LAGERSTROEMIA I. 'MUSKOGEE'	CRAPE MYRTLE
LAURUS 'SARATOGA'	SARATOGA SWEET BAY
PISTACIA C. 'KEITH DAVEY'	CHINESE PISTACHE
PLATANUS A. 'COLUMBIA'	LONDON PLANE TREE
PODOCARPUS M. 'MAKI'	SHRUBBY YEW PINE
PRUNUS C. 'KRAUTER VESUVIUS'	PURPLE LEAF PLUM
QUERCUS ILEX	HOLLY OAK
ZELKOVA S. 'MUSASHINO'	SAWLEAF ZELKOVA

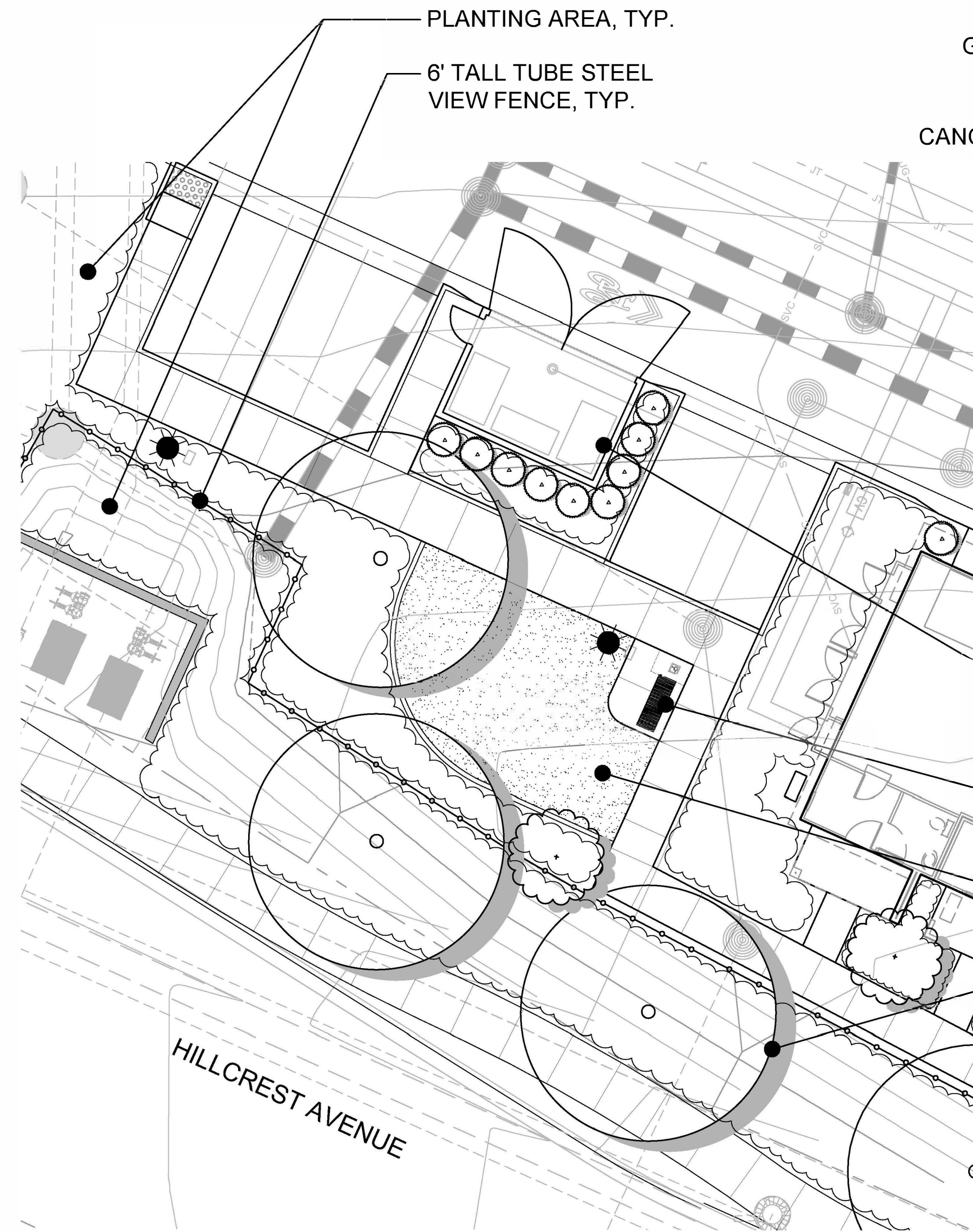
## NOTES:

- SEE SHEET L-9 FOR COMPLETE PLANT PALETTE INCLUDING PROPOSED TREES, SHRUBS, GROUNDCOVERS, GRASSES, BIORETENTION PLANTING, AND TURF. COMPLETE PLANT PALETTE IDENTIFIES BOTANICAL NAME, COMMON NAME, WATER USE, SPECIES NATIVE OR ADAPTIVE, CONTAINER SIZE, AND HEIGHT AND WIDTH OF PROPOSED PLANT MATERIAL.
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### NORTHERN COMMON AREA - PARCEL J



### PASSIVE PARK

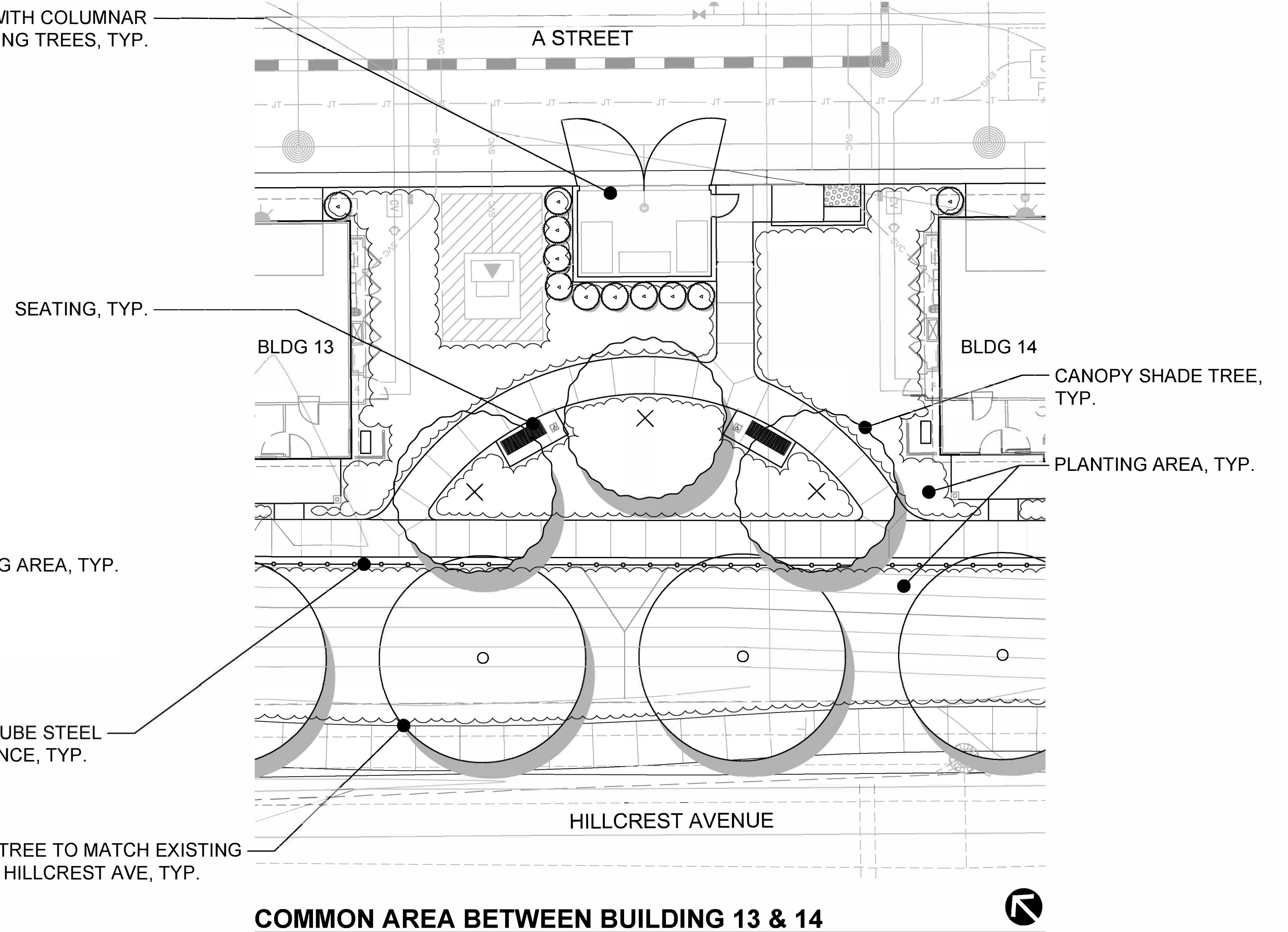
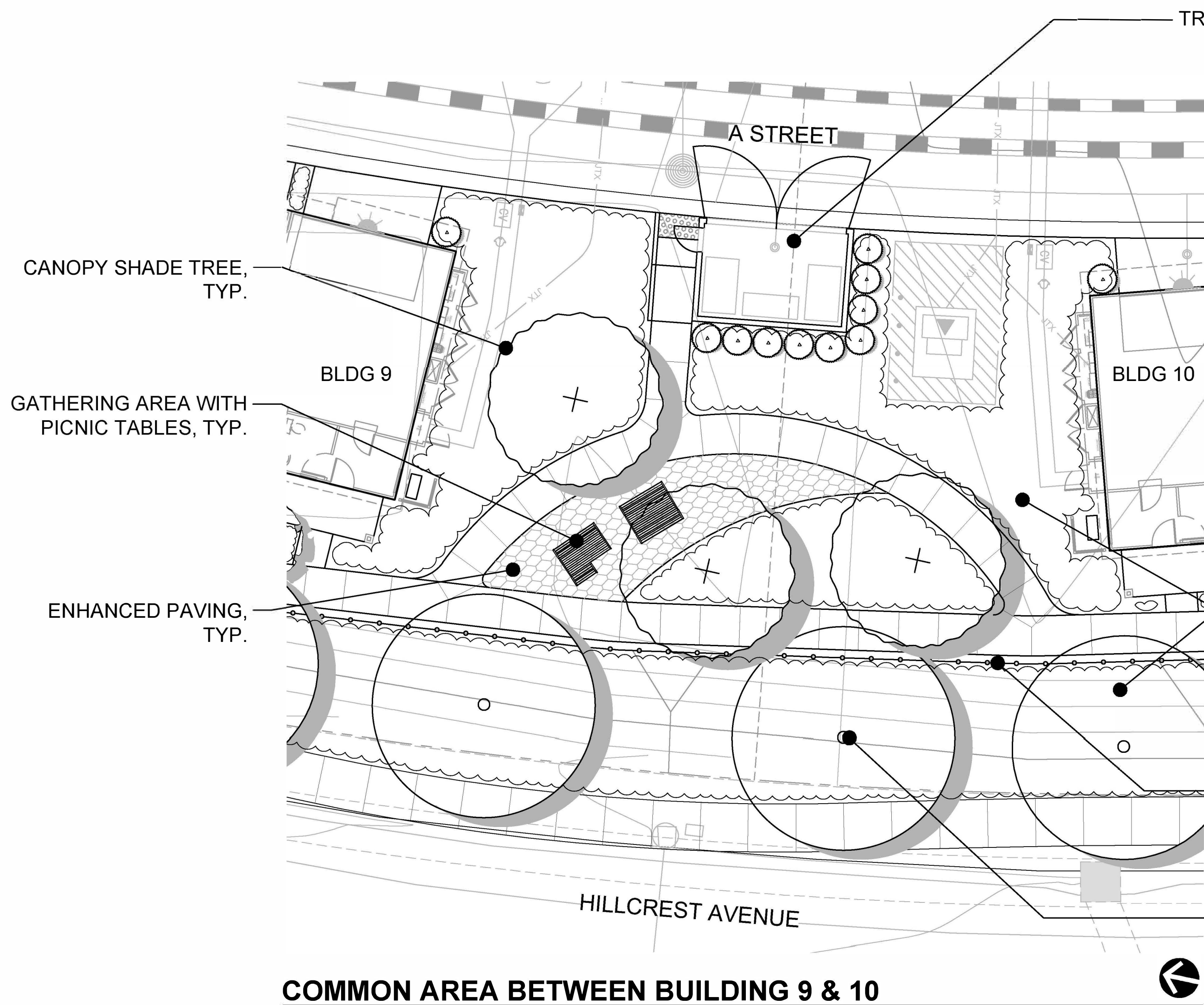
#### NOTES:

- SEE SHEET L-9 FOR COMPLETE PLANT PALETTE INCLUDING PROPOSED TREES, SHRUBS, GROUNDCOVERS, GRASSES, BIORETENTION PLANTING, AND TURF. COMPLETE PLANT PALETTE IDENTIFIES BOTANICAL NAME, COMMON NAME, WATER USE, SPECIES NATIVE OR ADAPTIVE, CONTAINER SIZE, AND HEIGHT AND WIDTH OF PROPOSED PLANT MATERIAL.
- LANDSCAPED AREAS SHALL INCORPORATE PLANTINGS UTILIZING THREE-TIER SYSTEM: (1) GRASSES AND GROUNDCOVERS, (2) SHRUBS AND VINES, AND (3) TREES PER CITY OF ANTIOCH ODS 3.3.1.C.

#### PROPOSED TREE LEGEND

BOTANICAL NAME	COMMON NAME
ACER R. 'ARMSTRONG'	ARMSTRONG MAPLE
CARPINUS B. 'FRANS FONTAINE'	HORNBEAM
CERCIS OCCIDENTALIS	WESTERN REDBUD
CORNUS KOUSA 'SATOMI'	DOGWOOD
GINKGO B. 'AUTUMN GOLD'	MAIDENHAIR TREE
LAGERSTROEMIA I. 'MUSKOGEE'	CRAPE MYRTLE
LAURUS 'SARATOGA'	SARATOGA SWEET BAY
PISTACIA C. 'KEITH DAVEY'	CHINESE PISTACHE
PLATANUS A. 'COLUMBIA'	LONDON PLANE TREE
PODOCARPUS M. 'MAKI'	SHRUBBY YEW PINE
PRUNUS C. 'KRAUTER VESUVIUS'	PURPLE LEAF PLUM
QUERCUS ILEX	HOLLY OAK
ZELKOVA S. 'MUSASHINO'	SAWLEAF ZELKOVA





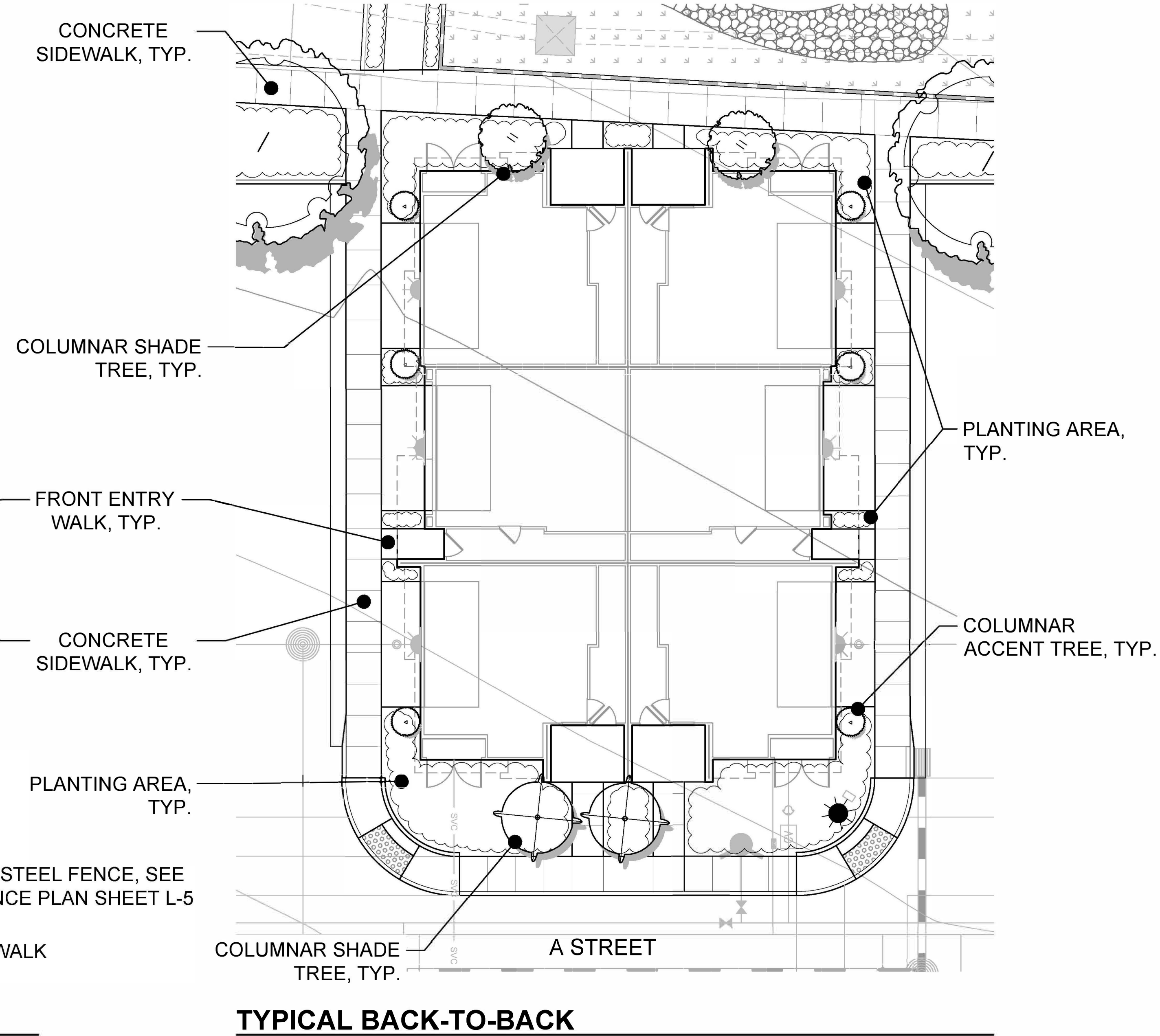
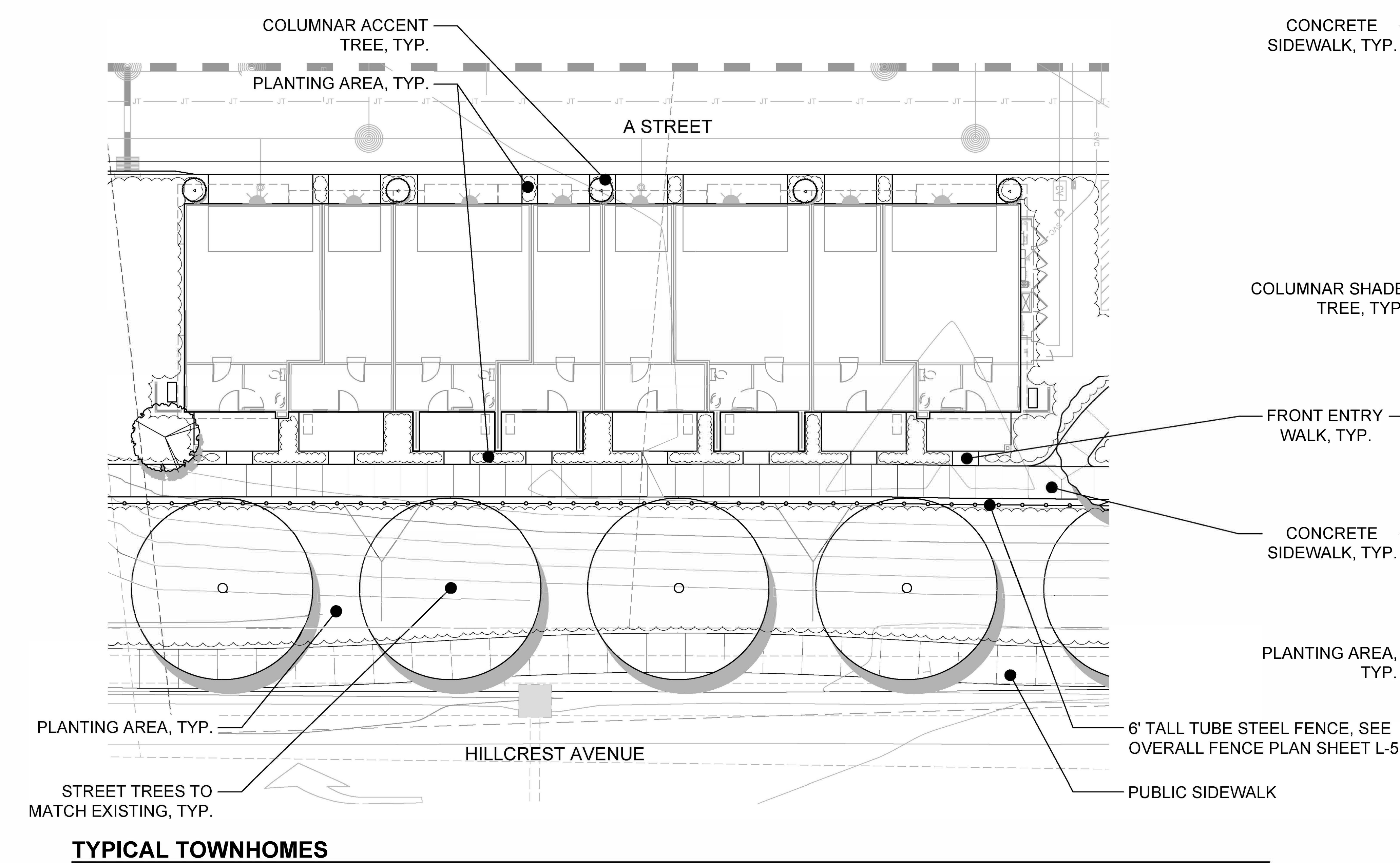
#### NOTES:

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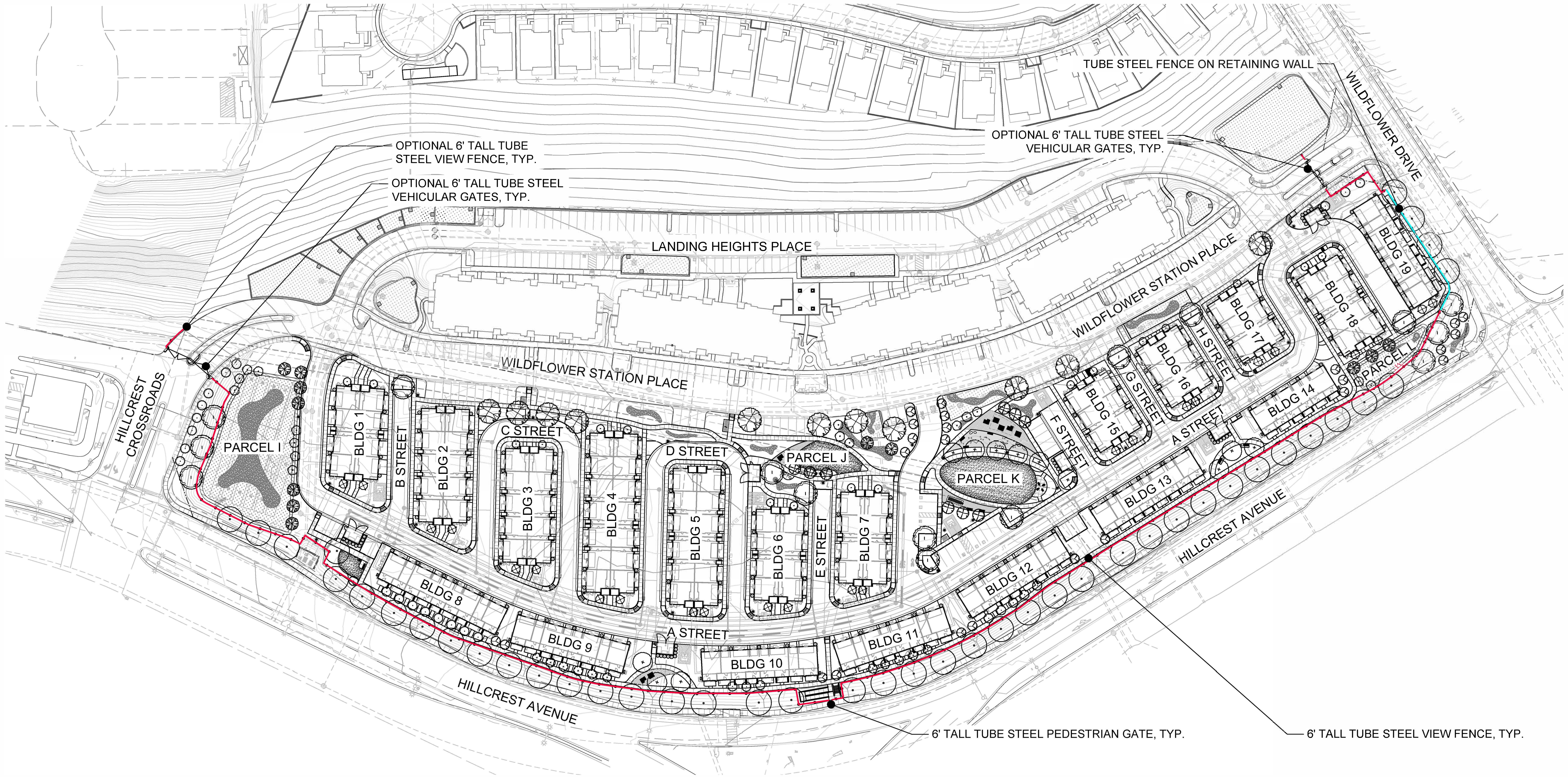
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ZELKOVA S. 'MUSASHINO'	SAWLEAF ZELKOVA





6' TUBE STEEL VIEW FENCE



6' TUBE STEEL PEDESTRIAN GATE



6' TUBE STEEL VEHICULAR GATES



TUBE STEEL FENCE ON CMU BLOCK RETAINING WALL. REFER TO CIVIL ENGINEER'S PLANS FOR WALL HEIGHT.

**FENCE LEGEND**

6' TALL TUBE STEEL VIEW FENCE

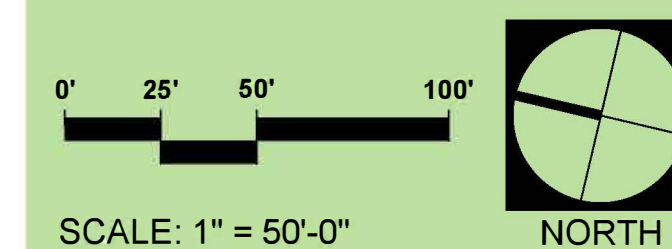
TUBE STEEL FENCE ON RETAINING WALL

**DeNova Homes**  
Building a Better Community.  
1500 WILLOW PASS COURT  
CONCORD, CALIFORNIA 94520  
(925)685-0110

**WILDFLOWER STATION**  
Antioch, California

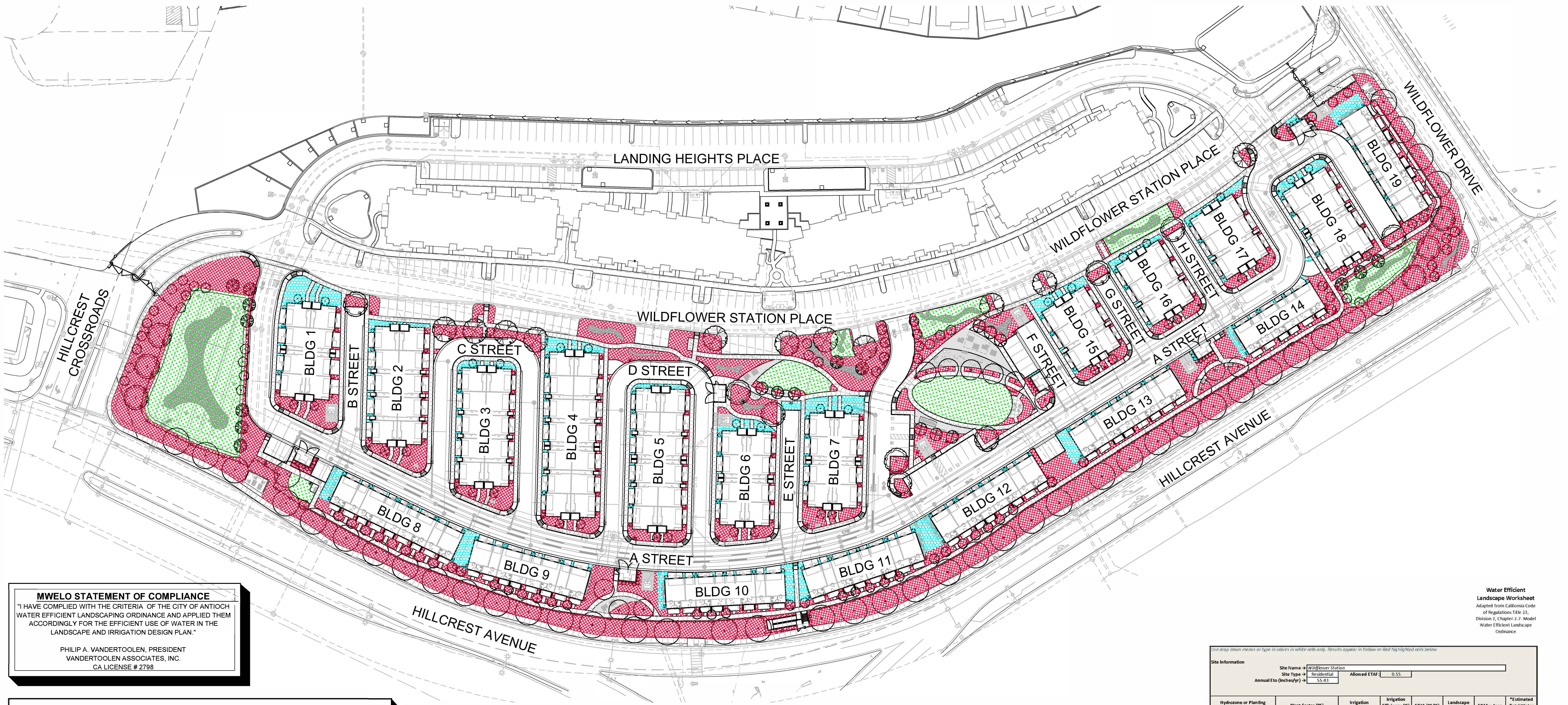
**OVERALL FENCE PLAN**  
CONCEPTUAL LANDSCAPE PLAN  
FEBRUARY 2024

vanderToolen Associates  
700 Ygnacio Valley Rd.  
Suite 100  
Walnut Creek, CA 94596  
tel: 925.274.1305  
www.vandertoolen.com



**L-6**  
Project No. 00523





**MWELO STATEMENT OF COMPLIANCE**  
"I HAVE COMPLIED WITH THE CRITERIA OF THE CITY OF ANTIOCH WATER EFFICIENT LANDSCAPING ORDINANCE AND APPLIED THEM ACCORDINGLY FOR THE EFFICIENT USE OF WATER IN THE LANDSCAPE AND IRRIGATION DESIGN PLAN."  
  
PHILIP A. VANDERTOOLEN, PRESIDENT  
VANDERTOOLEN ASSOCIATES, INC.  
CA LICENSE # 2798

**IRRIGATION NOTES**

1. **IRRIGATION ZONES:** ALL LANDSCAPED AREAS HAVE AN IRRIGATION ZONE DESIGNATION OF "SHRUBS / GROUNDCOVERS/ TREES" OR "TURF." NO IRRIGATION ZONES FOR ANNUALS AND TURFED SLOPES EXCEEDING 10% ARE PROPOSED.

2. **DEPTH OF IRRIGATION LINES:** ALL ON-GRADE LATERAL LINES SHALL BE BURIED TO A DEPTH OF 18" MIN. ALL ON-GRADE MAINLINES SHALL BE BURIED TO A DEPTH OF 24" MIN.

3. **BACKFLOW PREVENTER:** BACKFLOW PREVENTER SHALL BE A REDUCED PRESSURE PRINCIPLE BACKFLOW PREVENTER (FEBCO 825Y OR EQUAL) TYPE AS APPROVED BY WATER PURVEYOR.

4. **IRRIGATION SPRINKLER TYPES:** ALL SPRINKLERS SHALL UTILIZE MATCHED PRECIPITATION, PRESSURE COMPENSATING NOZZLES FOR MAXIMUM UNIFORMITY OF DISTRIBUTION. IRRIGATION SYSTEMS TO BE INSPECTED PERIODICALLY FOR BROKEN OR DEFICIENT EQUIPMENT.

5. **IRRIGATION CONTROLLERS:** CONTROLLER SHALL BE AN AUTOMATIC ET (EVAPOTRANSPIRATION) WITH MULTIPLE PROGRAMMING CAPABILITY. CONTROLLER TO BE REPROGRAMMED SEASONALLY TO MINIMIZE RUNOFF OR OVER WATERING. MOISTURE SENSING DEVICES SHALL BE UTILIZED TO CONTROL IRRIGATION CYCLES ACCORDING TO SPECIFIC IRRIGATION REQUIREMENTS.

6. **CLASS OF IRRIGATION PIPE:** ALL MAINLINE SHALL BE PVC 315 FOR DIAMETERS 2" OR LARGER & PVC SCHEDULE 40 FOR DIAMETERS LESS THAN 2". ALL LATERAL LINE SHALL BE CLASS 200 PVC.

7. **IRRIGATION EMITTERS:** ALL SHRUB/ GROUNDCOVER AREAS SHALL BE IRRIGATED USING DRIP IRRIGATION SYSTEM. ALL TREE AREAS SHALL BE IRRIGATED USING BUBBLER IRRIGATION SYSTEM.

**IRRIGATION CONCEPT STATEMENT**

THE IRRIGATION DESIGN FOR THE SITE SHALL COMPLY WITH THE STATE OF CALIFORNIA MODEL WATER EFFICIENT LANDSCAPE ORDINANCE (TITLE 23 - DIVISION 2-CHAPTER 2.7) AND THE CITY OF ANTIOCH WATER EFFICIENT LANDSCAPE STANDARDS.

THE IRRIGATION SYSTEMS WILL BE AUTOMATICALLY CONTROLLED BY AN ET IRRIGATION CONTROLLER CAPABLE OF MULTIPLE PROGRAMMING AND INDEPENDENT TIMING OF INDIVIDUAL IRRIGATION SYSTEMS. THE CONTROLLER WILL HAVE A 24-HOUR CLOCK TO ALLOW MULTIPLE START TIMES AND REPEAT CYCLES TO ADJUST FOR SOIL PERCOLATION RATES.

THE IRRIGATION SYSTEMS WILL CONSIST PRIMARILY OF LOW VOLUME, LOW FLOW BUBBLERS FOR TREES, POINT SOURCE DRIP IRRIGATION FOR SHRUBS AND GROUNDCOVERS.

PLANTS WILL BE GROUPED ONTO SEPARATE VALVES ACCORDING TO SUN EXPOSURE AND WATER USE TO ALLOW FOR IRRIGATION APPLICATION BY HYDROZONE. THE IRRIGATION SCHEDULING WILL REFLECT THE REGIONAL EVAPO-TRANSPARATION RATES. THE ENTIRE SITE WILL BE DESIGNED TO RUN DURING NIGHTTIME HOURS WHEN IRRIGATION IS MOST EFFICIENT.

**HYDROZONE LEGEND**

- TREES, SHRUBS & GROUNDCOVERS - DRIP IRR SOUTH/ WEST EXPOSURE
- TREES, SHRUBS & GROUNDCOVERS - DRIP IRR NORTH/ EAST EXPOSURE
- TURF & BIO DETENTION AREAS - MP ROTATOR SPRAY

**Water Efficient Landscape Worksheet**  
Adapted from California Code of Regulations Title 23, Division 2, Chapter 2.7, Model Water Efficient Landscape Ordinance

Use drop down menus or type in values in white cells only. Results appear in yellow or red highlighted cells below.

**Site Information**

Site Name → Wildflower Station  
Site Type → Residential  
Annual Eto (Inches/yr) → 55.43  
Allowed ETAF → 0.55

Hydrozone or Planting Description (a)	Plant Factor (PF)	Irrigation Method (b)	Irrigation Efficiency (IE) (c)	ETAF (PF/IE)	Landscape Area (sqft)	ETAF x Area	*Estimated Total Water Use (gal./yr.)
<b>Regular Landscape Areas</b>							
Low/Mod. Water Use Shrubs	0.4	Mod./Ave.	Drip	0.81	0.5	95,427	47,124
Low/Mod. Water Use Trees	0.4	Mod./Ave.	Bubbler	0.81	0.5	2,667	1,317
Turf	0.7	High	Overhead Spray	0.75	0.9	6,891	3,366
Bioretention	0.4	Mod./Ave.	Overhead Spray	0.75	0.5	20,376	10,867
					<b>SUBTOTAL →</b>	<b>125,291</b>	<b>65,675</b>
<b>Special Landscape Areas</b>							
1					1	0	0
2					1	0	0
3					1	0	0
					<b>SUBTOTAL →</b>	<b>3</b>	<b>0</b>
					<b>**Estimated Total Water Use (ETWU) →</b>	<b>2,257,025</b>	
					<b>Maximum Allowed Water Allowance (MAWA) →</b>	<b>2,368,284</b>	

**(a) Hydrozone #/Planting Description**  
1.) Front Lawn  
2.) low water use planting  
3.) medium water use

**(b) Irrigation Method**  
Overhead Spray  
Drip  
Bubbler

**(c) Irrigation Efficiency**  
0.75 for spray head  
0.81 for drip

**(\*) ETWU (Annual Gallons Required)**  
 $ETWU = (BLDG) \times [(ETAF \times EA) + (E \times ETAF) \times SA]$   
where 0.62 is a conversion factor that converts acre-inches per acre per year to gallons per square foot per year, EA is the total landscape area in square feet, and ETAF is 0.55 for residential areas and 0.45 for non-residential areas.

**ETAF Calculations**

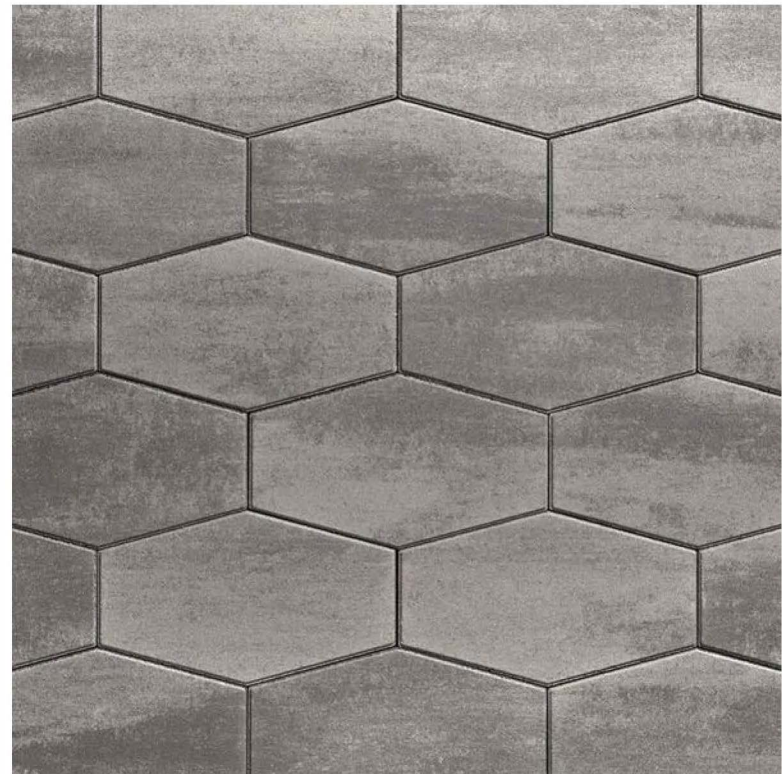
Regular Landscape Areas	Total ETAF x Area	Total Area	Average ETAF
	65,675	125,291	0.52

**All Landscape Areas**

Total ETAF x Area	Total Area	Sitewide ETAF
65,675	125,291	0.52

**Notes:**  
Average ETAF (ET adjustment factor) for Regular Landscape Areas must be 0.55 or below for residential areas, and 0.45 or below for non-residential areas.  
  
Calculator developed to meet code effective Dec. 1, 2015  
This calculator is for estimating purposes only.





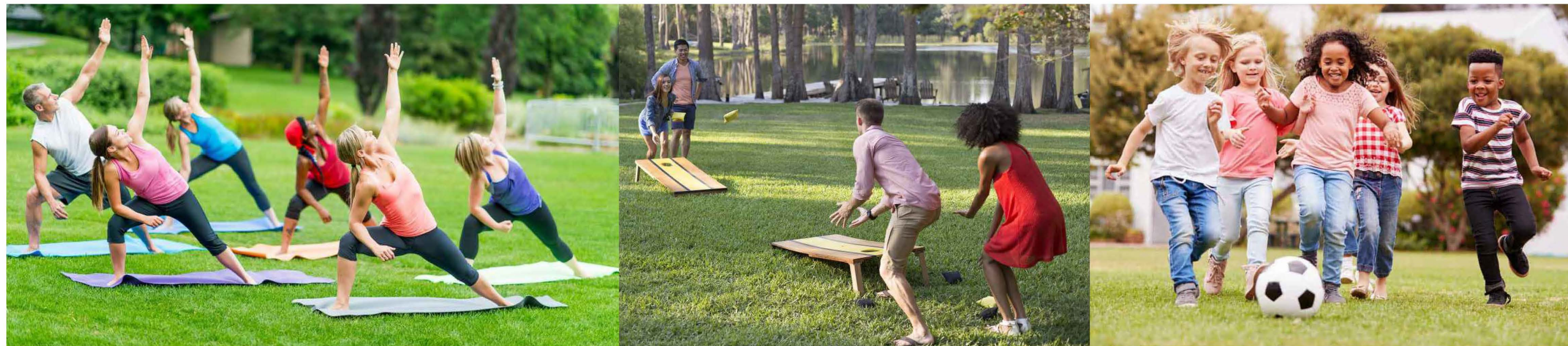
**A** DECORATIVE PAVING



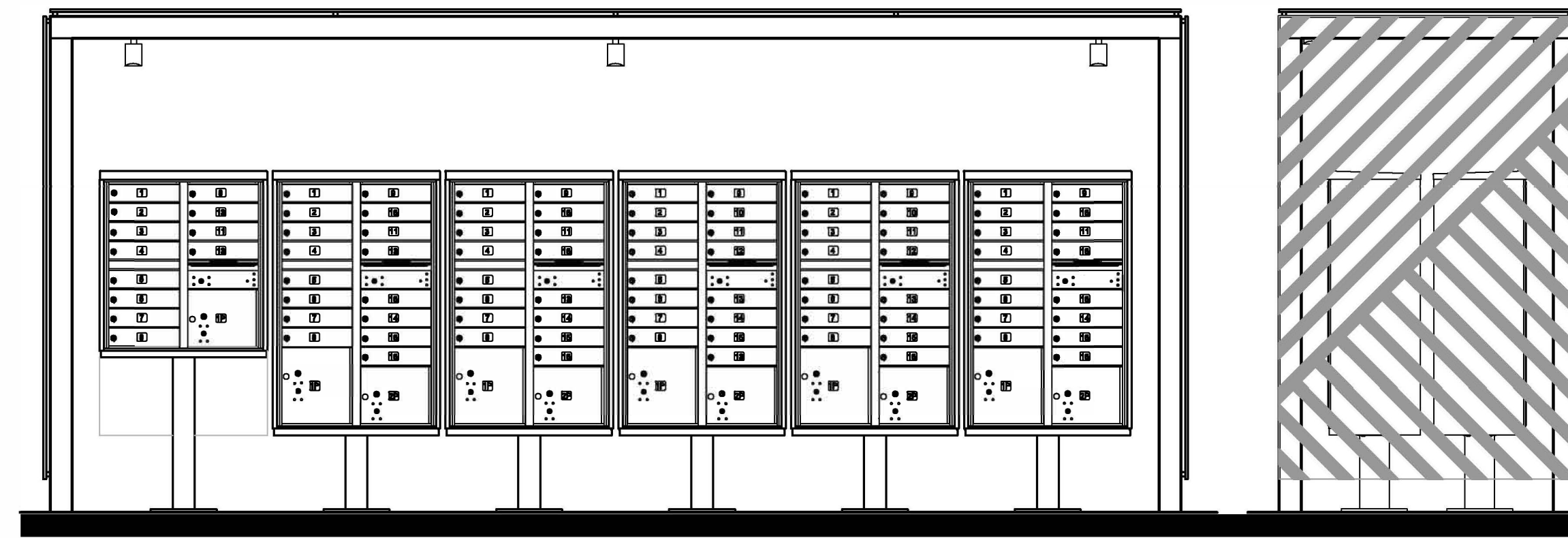
**B** SHADE TRELLIS



**C** CHILDREN'S PLAY AREA  
NOTE: NO CLIMBING STRUCTURES



**D** MULTIPURPOSE TURF AREA



**E** CLUSTER MAILBOX



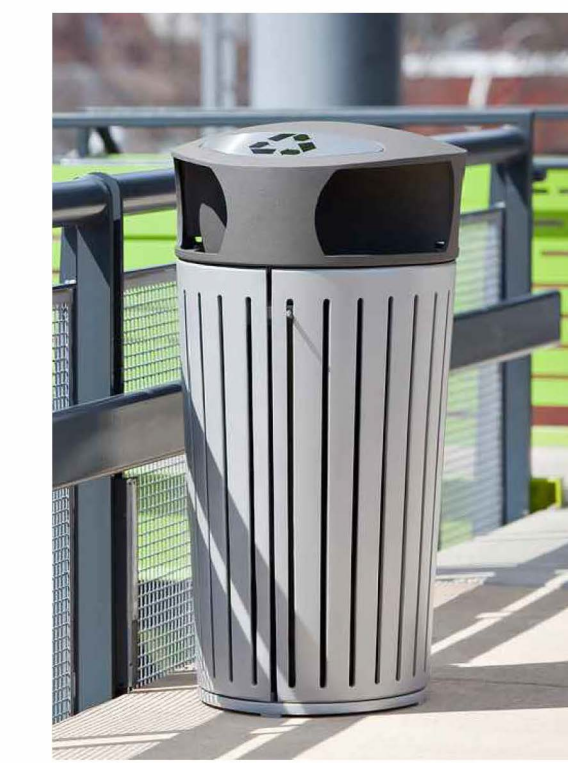
**F** BIKE RACK



**G** SEATING



**H** PICNIC TABLE

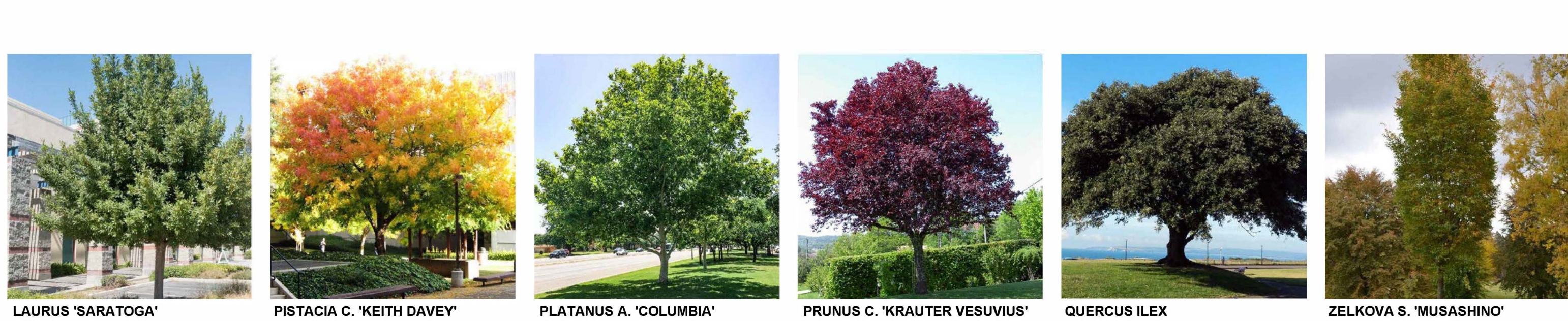
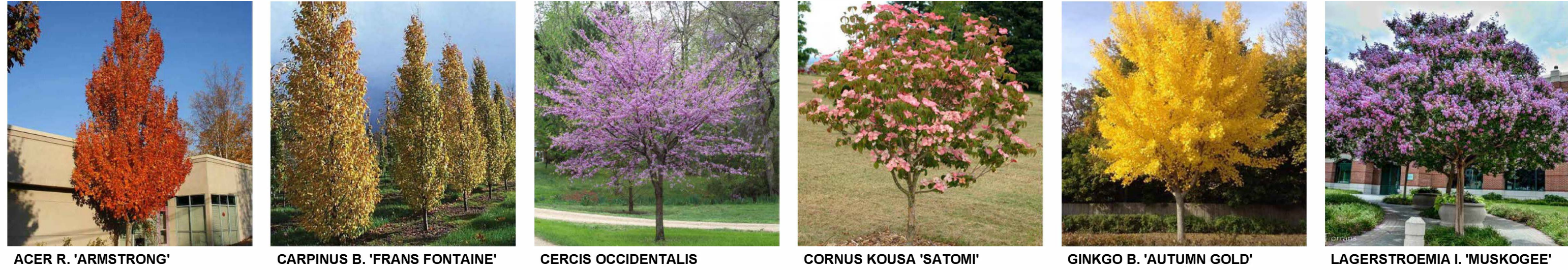


**I** TRASH & RECYCLING RECEPTACLE

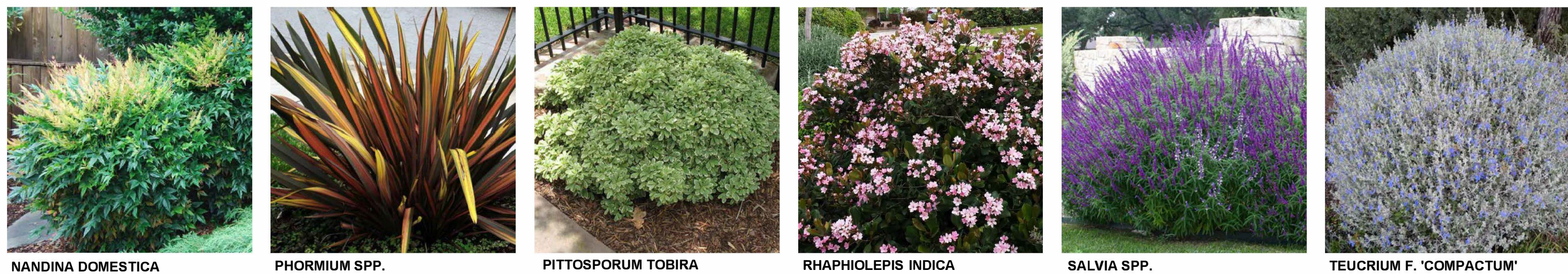
NOTE: THE PROPOSED SITE AMENITY DESIGNS ARE CONCEPTUAL. SIMILARLY THEMED DESIGN, COLORS & MATERIALS MAY BE SELECTED BASED UPON PRODUCT AVAILABILITY AT TIME OF INSTALLATION.



TREES



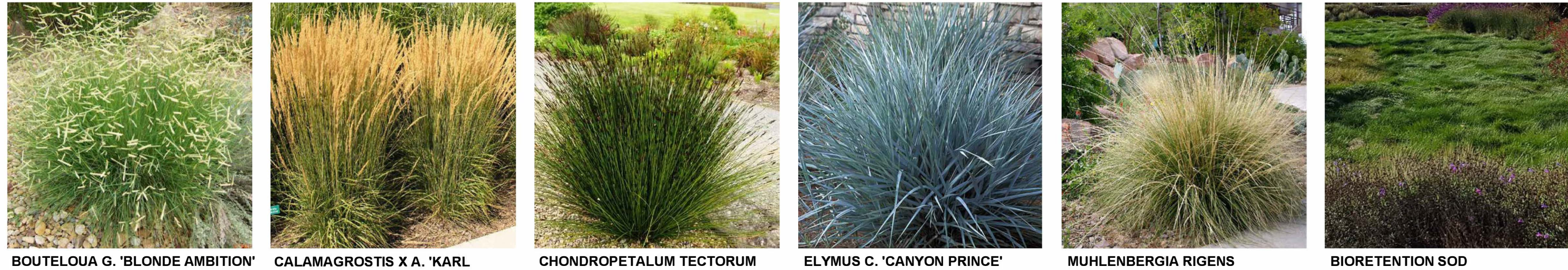
SHRUBS



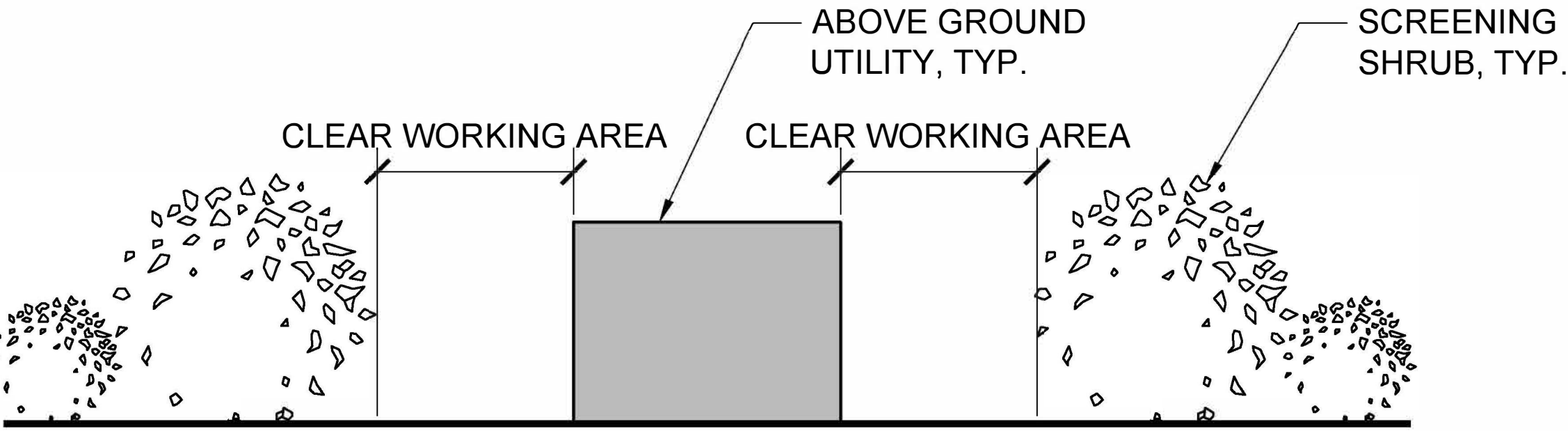
ACCENT SHRUBS & GROUNDCOVERS



GRASSES/BIORETENTION



SYMBOL	BOTANICAL NAME	COMMON NAME	WATER USE	NATIVE/ ADAPTIVE	SIZE	H X W
TREES (TIER 3)						
	ACER R. 'ARMSTRONG'	ARMSTRONG MAPLE	M	A	24" BOX	45' X 15'
	CARPINUS B. 'FRANS FONTAINE'	HORNBEAM	M	A	15 GAL	35' X 12'
	CERCIS OCCIDENTALIS	WESTERN REDBUD	VL	N	15 GAL	15' X 15'
	CORNUS KOUSA 'SATOMI'	DOGWOOD	M	A	15 GAL	20' X 15'
	GINKGO B. 'AUTUMN GOLD'	MAIDENHAIR TREE	L	A	15 GAL	30' X 30'
	LAGERSTROEMIA I. 'MUSKOGEE'	CRAPE MYRTLE	L	A	24" BOX	20' X 12'
	LAURUS 'SARATOGA'	SARATOGA SWEET BAY	L	A	15 GAL	30' X 25'
	PISTACIA C. 'KEITH DAVEY'	CHINESE PISTACHE	L	A	24" BOX	50' X 50'
	PLATANUS A. 'COLUMBIA'	LONDON PLANE TREE	M	A	24" BOX	75' X 35'
	PODOCARPUS M. 'MAKI'	SHRUBBY YEW PINE	M	A	15 GAL	10' X 3'
	PRUNUS C. 'KRAUTER VESUVIUS'	PURPLE LEAF PLUM	M	A	15 GAL	18' X 12'
	QUERCUS ILEX	HOLLY OAK	L	A	24" BOX	50' X 50'
	ZELKOVA S. 'MUSASHINO'	SAWLEAF ZELKOVA	M	A	15 GAL	40' X 15'
SHRUBS (TIER 2)						
	CALLISTEMON VIMINALIS 'LITTLE JOHN'	DWARF BOTTLEBRUSH	L	A	5 GAL	3' X 3'
	CISTUS SPP.	ROCKROSE	L	A	5 GAL	4' X 4'
	DIETES SPP.	FORTNIGHT LILY	L	A	5 GAL	3' X 3'
	DODONAEA V. 'PURPUREA'	HOPSEED BUSH	L	A	5 GAL	12" X 10"
	GALVEZIA SPECIOSA 'FIRECRACKER'	ISLAND SNAPDRAGON	L	N	5 GAL	2' X 3'
	GREVILLEA X 'NOELLI'	GREVILLEA	L	A	5 GAL	4' X 4'
	LIGUSTRUM JAPONICUM 'TEXANUM'	WAXLEAF PRIVET	L	A	5 GAL	9' X 5'
	LOROPETALUM C. 'RAZZLEBERRY'	CHINESE FRINGE FLOWER	M	A	5 GAL	5' X 4'
	NANDINA DOMESTICA	HEAVENLY BAMBOO	L	A	5 GAL	6' X 3'
	PHORMIUM SPP.	NEW ZEALAND FLAX	L	A	5 GAL	3' X 3'
	PITTOSPORUM TOBIRA	TOBIRA	L	A	5 GAL	5' X 5'
	RHAMNUS CALIFORNICA 'EVE CASE'	COFFEE BERRY	L	N	5 GAL	6' X 6'
	RHAPHIOLEPIS INDICA	INDIA HAWTHORN	M	A	5 GAL	4' X 4'
	SALVIA SPP.	SAGE	L	A	5 GAL	3' X 3'
	SOLLYA HETEROPHYLLA	AUSTRALIAN BLUEBELL CREEPER	L	A	5 GAL	4' X 4'
	TEUCRIUM FRUTICANS 'COMPACTUM'	BUSH GERMANDER	L	A	5 GAL	2' X 2'
	VERBENA LILACINA 'DE LA MINA'	CEDEROS ISLAND VERBENA	L	A	5 GAL	2' X 2'
	WESTRINGIA FRUTICOSA	COASTAL ROSEMARY	L	A	5 GAL	5' X 8'
ACCENT SHRUBS & GROUNDCOVERS (TIER 1)						
	ACHILLEA SPP.	YARROW	L	N	5 GAL	24" X 24"
	ANIGOZANTHOS 'KANGA RED'	KANGAROO PAW	L	A	5 GAL	24" X 24"
	ERIGERON KARVINSKIANUS	SANTA BARBARA DAISY	L	A	5 GAL	12" X 36"
	LIRIOPE SPICATA	CREEPING LILY TURF	M	A	5 GAL	12" X 12"
	MYOPORUM PARVIFOLIUM	MYOPORUM	L	A	5 GAL	12" X 96"
	NEPETA FAASSENII 'WALKER'S LOW'	CATMINT	L	A	5 GAL	12" X 24"
	PENSTEMON H. 'MARGARITA BOP'	PENSTEMON	L	N	5 GAL	24" X 24"
	SCAEVOLA 'MAUVE CLUSTERS'	SCAEVOLA	L	A	5 GAL	6" X 48"
GRASSES (TIER 1)						
	BOUTELOUA G. 'BLONDE AMBITION'	BLUE GRAMA	L	N	5 GAL	2' X 2'
	CALAMAGROSTIS X A. 'KARL FOERSTER'	FEATHER REED GRASS	L	A	5 GAL	3' X 2'
	CHONDROPETALUM TECTORUM	SMALL CAPE RUSH	L	A	5 GAL	3' X 3'
	FESTUCA MAIREI	ATLAS FESCUE	L	A	5 GAL	2' X 3'
	HELIOTOTRICHON SEMPERVIRENS	BLUE OAT GRASS	L	A	5 GAL	2' X 3'
BIORETENTION (TIER 1)						
	CAREX TUMULICOLA	BERKELEY SEDGE	I	N	5 GAL	1' X 2'
	ELYMUS C. 'CANYON PRINCE'	WILD RYE GRASS	L	N	5 GAL	2' X 3'
	JUNCUS PATENS	CALIFORNIA GRAY RUSH	L	N	5 GAL	2' X 2'
	MUHLENBERGIA RIGENS	DEER GRASS	L	N	5 GAL	4' X 4'
	BIOFILTRATION SOD	AVAILABLE AT DELTA BLUEGRASS (800) 637-8873	L	N	SOD	N/A
TURF (TIER 1)						
	RHIZOMATOUS TALL FESCUE (RTF)	SELF-HEALING TURF	H	A	SOD	N/A
NOTES:						
1. LANDSCAPE WILL UTILIZE LOW WATER USE PLANT MATERIALS, HYDRO ZONING AND EMPLOY WATER EFFICIENT LOW VOLUME DRIP AND SPRAY IRRIGATION AND SMART CONTROLLERS, SEE L-7 FOR HYDROZONE PLAN AND WATER USE CALCULATIONS.						
2. LANDSCAPED AREAS SHALL INCORPORATE PLANTINGS UTILIZING THREE-TIER SYSTEM: (1) GRASSES AND GROUNDCOVERS, (2) SHRUBS AND VINES, AND (3) TREES PER CITY OF ANTIOCH ODS 3.3.1.C.						
3. NOT ALL PLANT MATERIAL MAY BE AVAILABLE IN 5 GALLON CONTAINER SIZE. IF PROPOSED PLANT IS UNAVAILABLE IN 5 GALLON CONTAINER SIZE, 1 GALLON, 2 GALLON, OR 3 GALLON SUBSTITUTION MAY BE ACCEPTED. ALL SUBSTITUTIONS SHALL BE APPROVED BY LANDSCAPE ARCHITECT OR OWNER.						



SCREEN PLANTING FOR UTILITIES




# WILDFLOWER TOWNHOMES

ANTIOCH, CA | FEBRUARY 2, 2024



307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

 **DeNOVA HOMES**  
1500 Willow Pass Ct., Concord, CA 94520  
925.685.0110

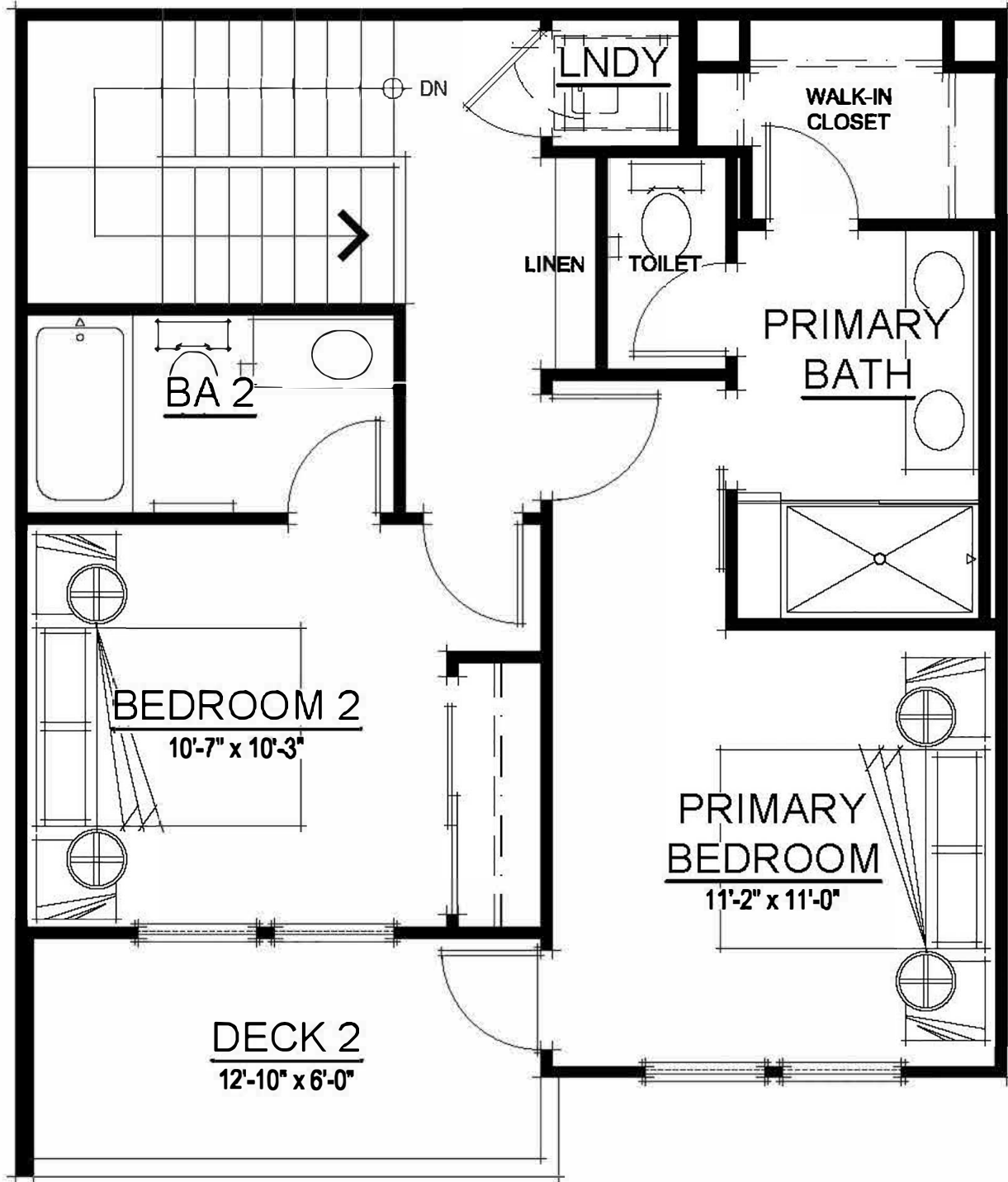
COVER SHEET  
A000

SDG Architects, Inc.  
3361 Walnut Blvd. Suite 120  
Brentwood, CA 94513  
925.634.7000 | sdgarchitectsinc.com

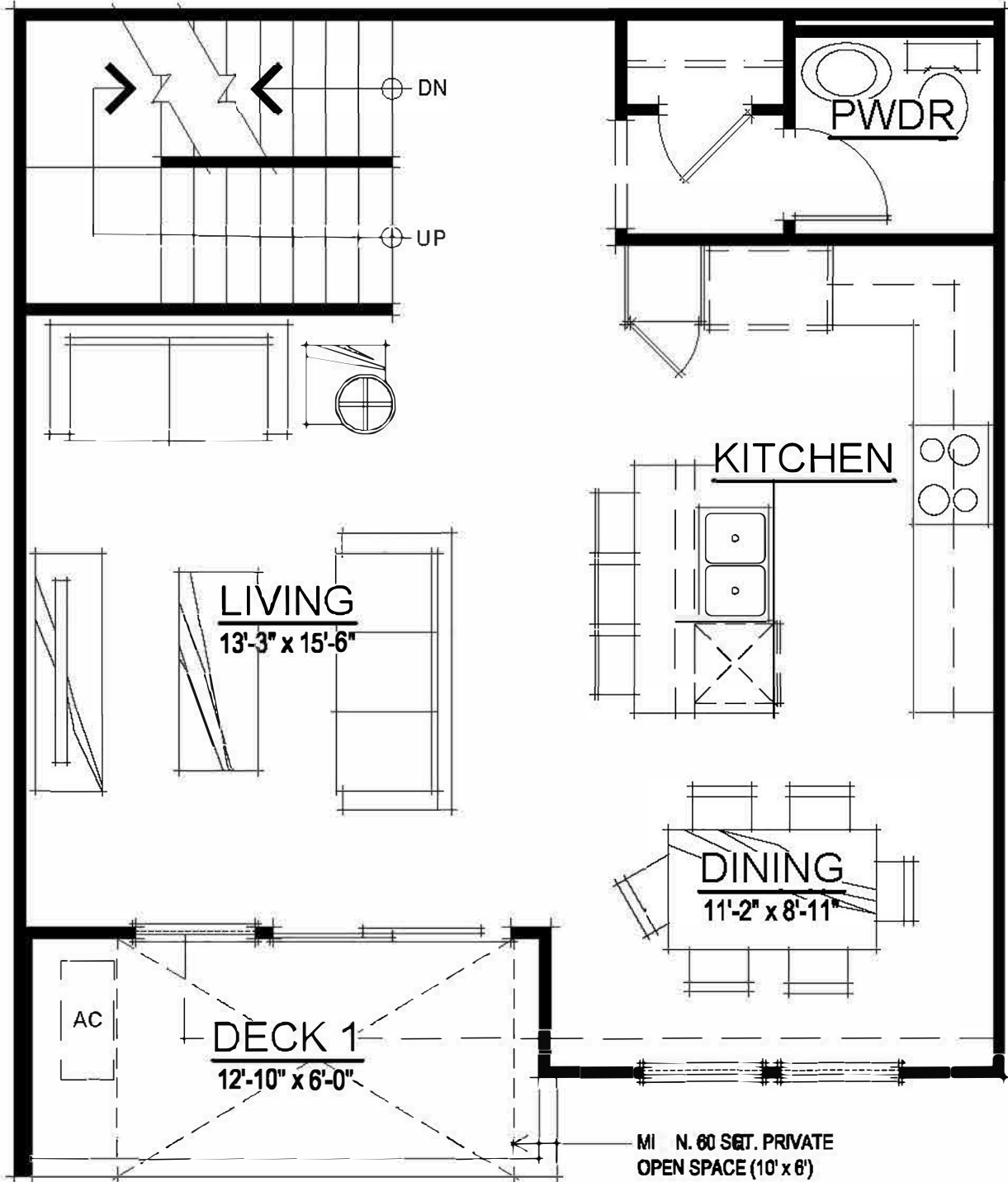


A58

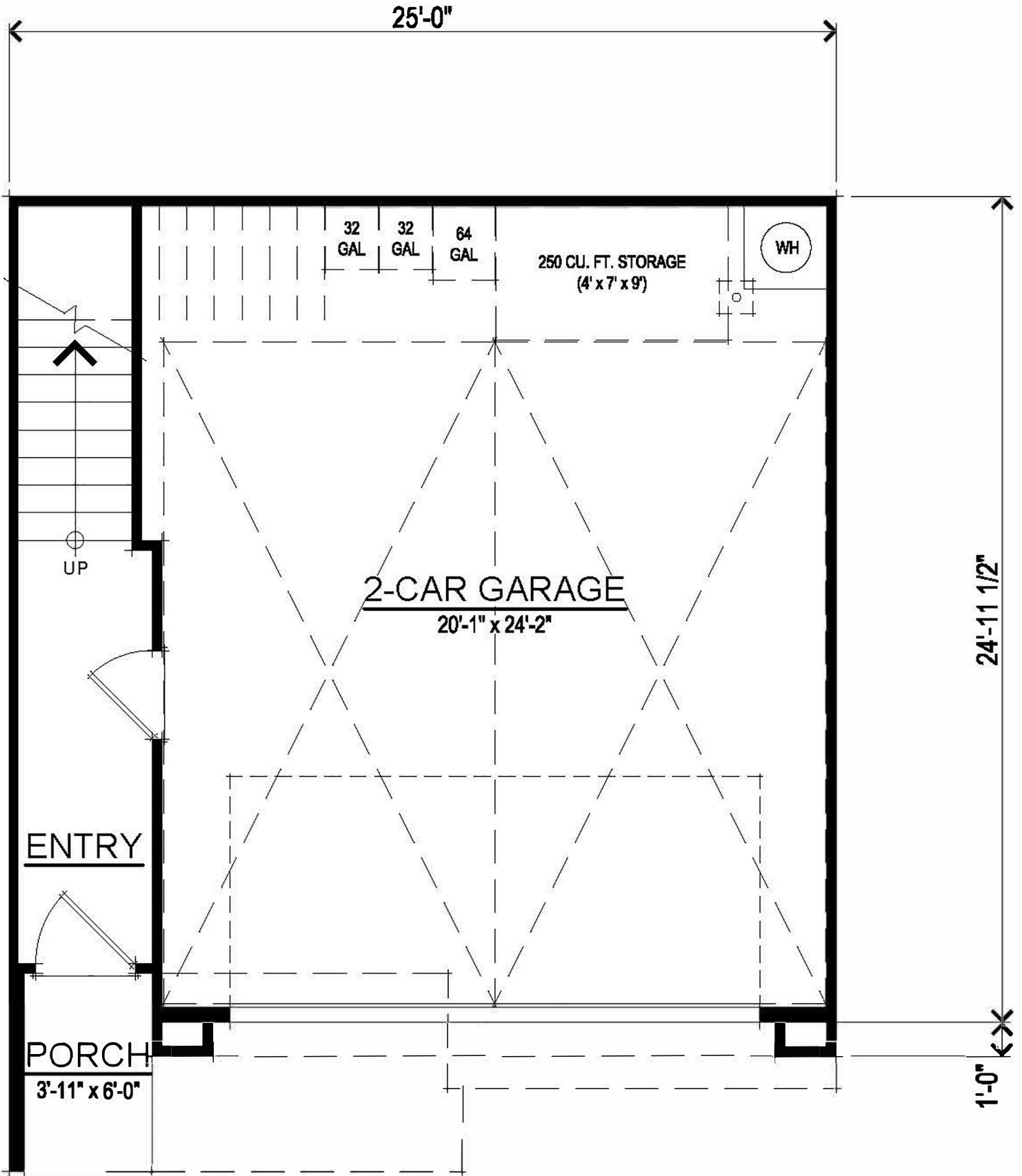




THIRD FLOOR PLAN



SECOND FLOOR PLAN



FIRST FLOOR PLAN

SQUARE FOOTAGES	
FIRST FLOOR	103 SQ. FT.
SECOND FLOOR	631 SQ. FT.
THIRD FLOOR	559 SQ. FT.
TOTAL LIVING	1293 SQ. FT.
2-CAR GARAGE	517 SQ. FT.
DECK 1	81 SQ. FT.
DECK 2	81 SQ. FT.

BACK TO BACK TOWNHOMES

307.071 Wildflower Townhomes  
 Antioch, CA  
 February 02, 2024

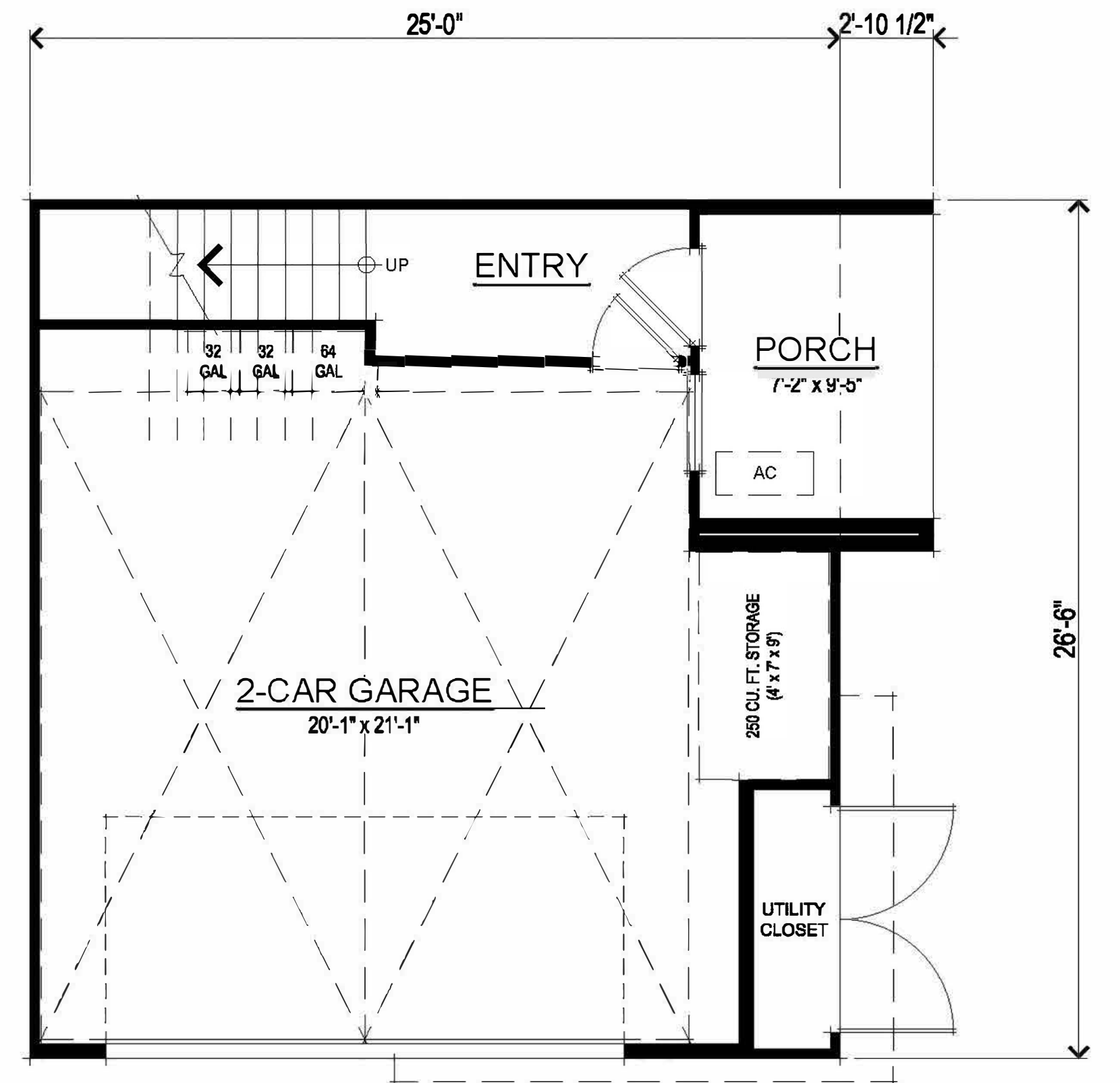
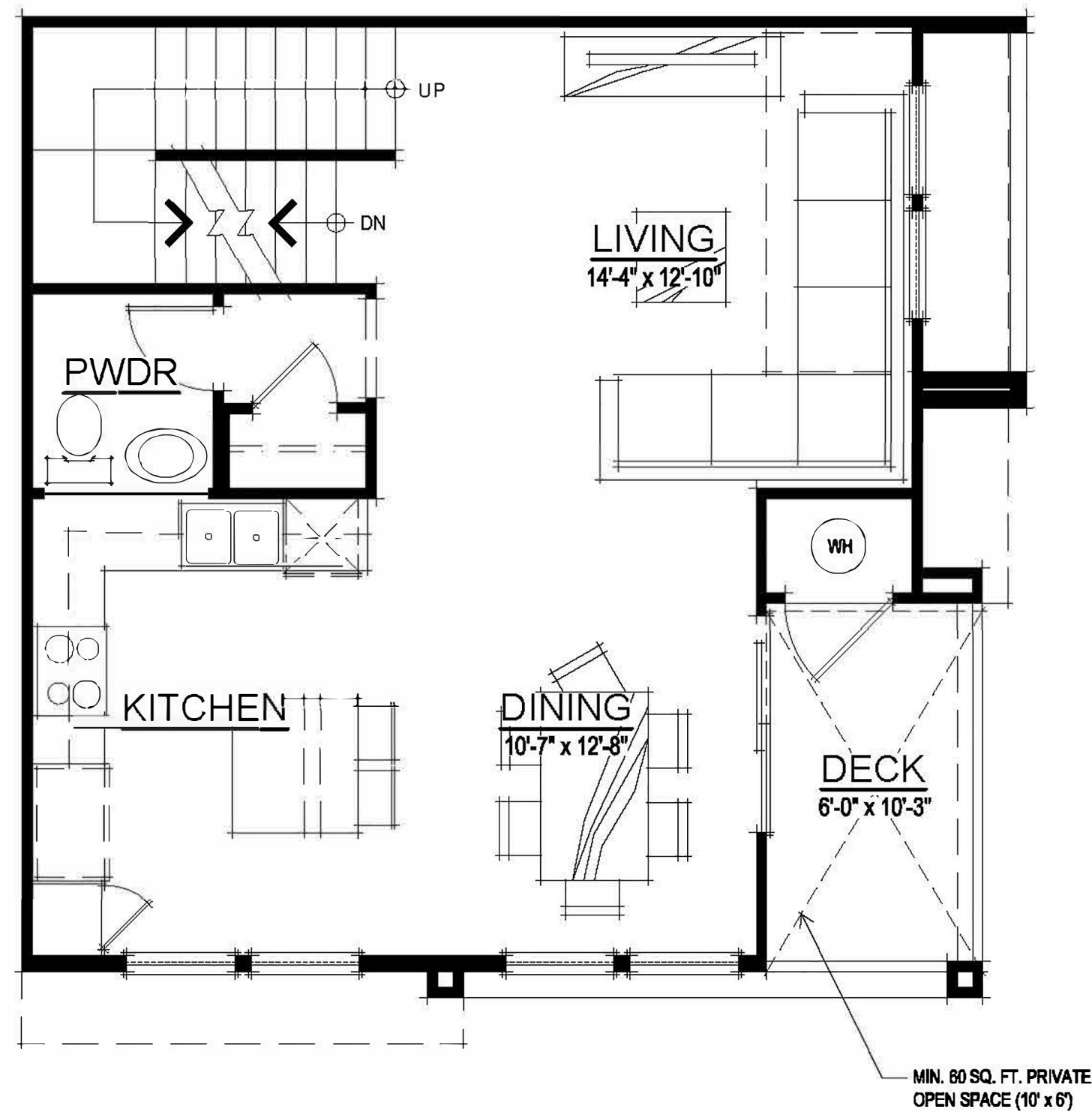
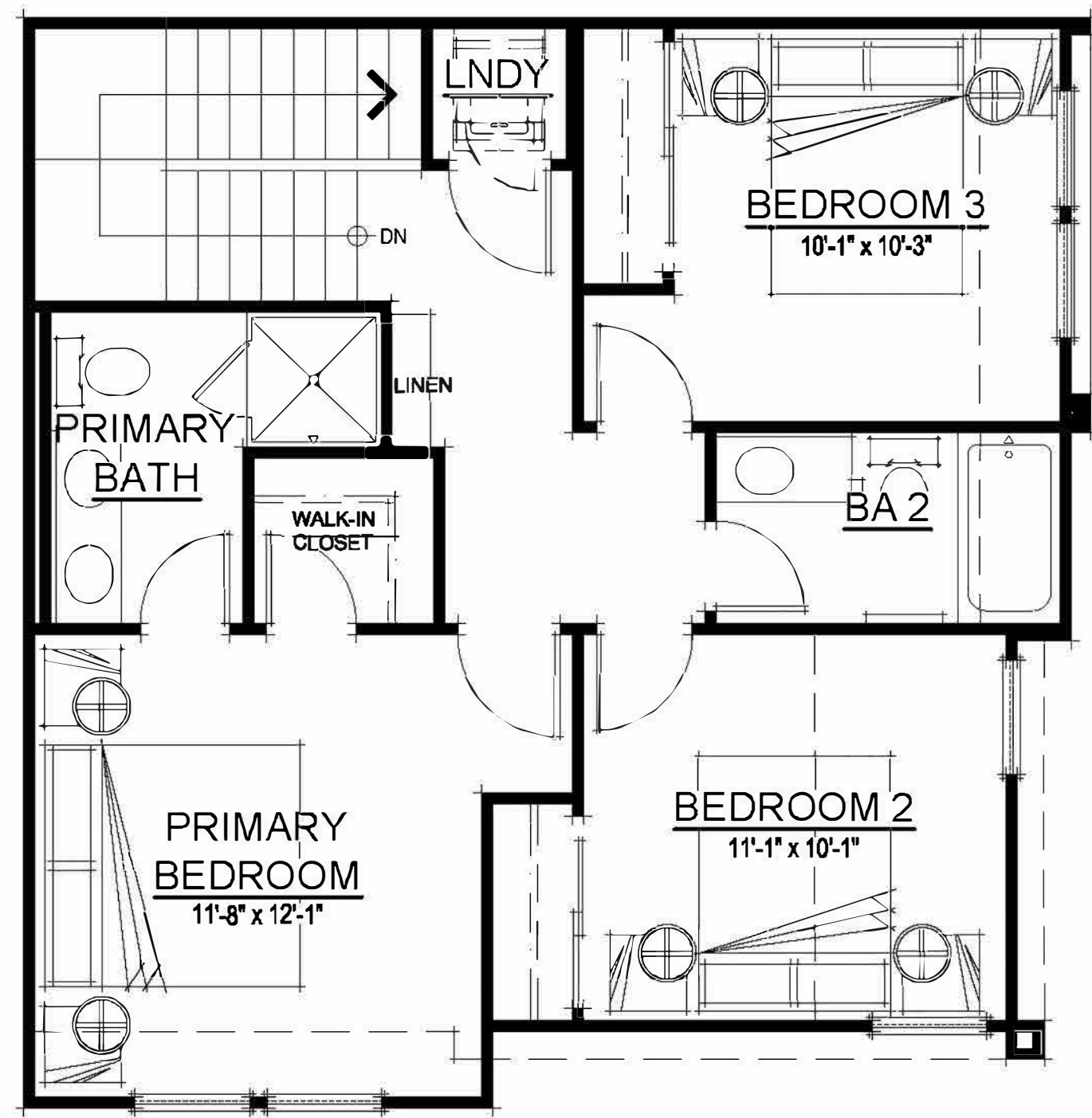
**dh** DENOVA HOMES  
 1500 Willow Pass Ct., Concord, CA 94520  
 925.685.0110

UNIT BB1 FLOOR PLANS  
 A001

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 3361 Walnut Blvd. Suite 120  
 Brentwood, CA 94513  
 925.634.7000 | sdgarchitectsinc.com







SQUARE FOOTAGES	
FIRST FLOOR	95 SQ. FT.
SECOND FLOOR	620 SQ. FT.
THIRD FLOOR	666 SQ. FT.
TOTAL LIVING	1381 SQ. FT.
2-CAR GARAGE	502 SQ. FT.
DECK	61 SQ. FT.

BACK TO BACK TOWNHOMES

307.071 Wildflower Townhomes  
 Antioch, CA  
 February 02, 2024

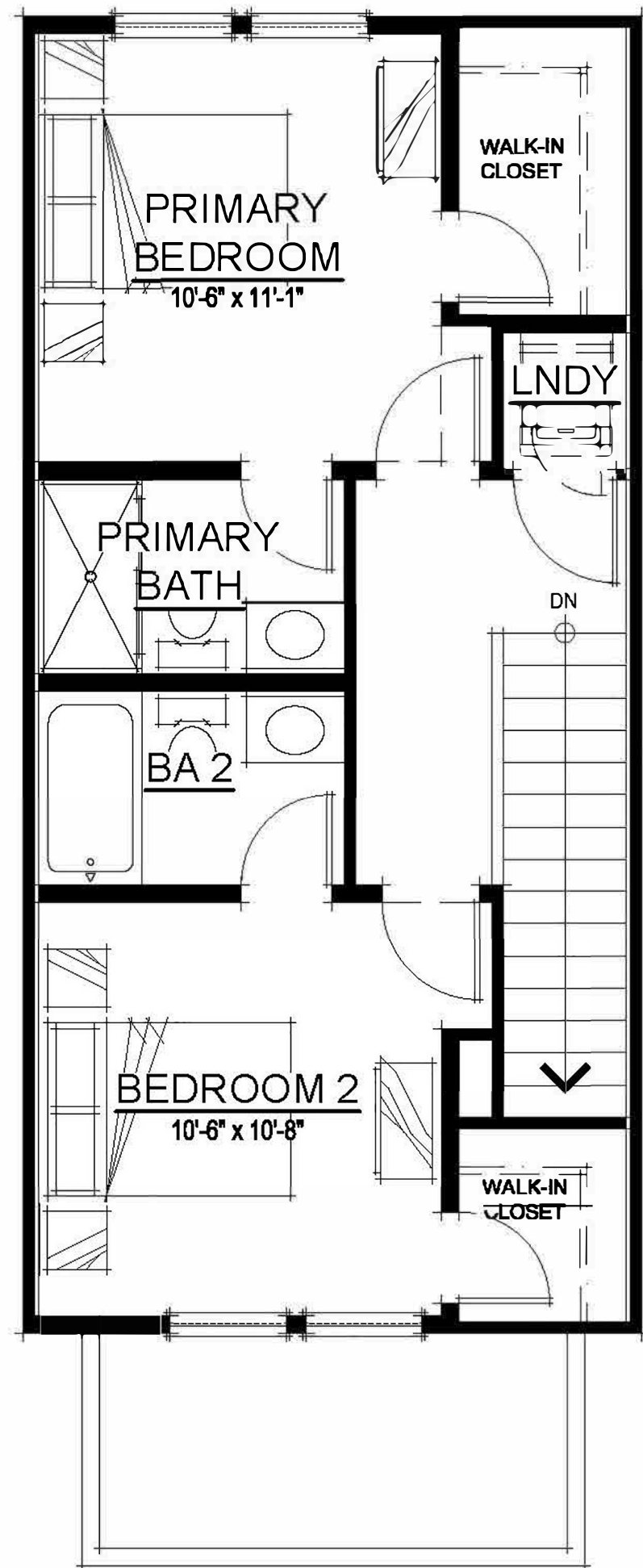
**dh** DENOVA HOMES  
 1500 Willow Pass Ct., Concord, CA 94520  
 925.685.0110

UNIT BB2 FLOOR PLANS  
 A002

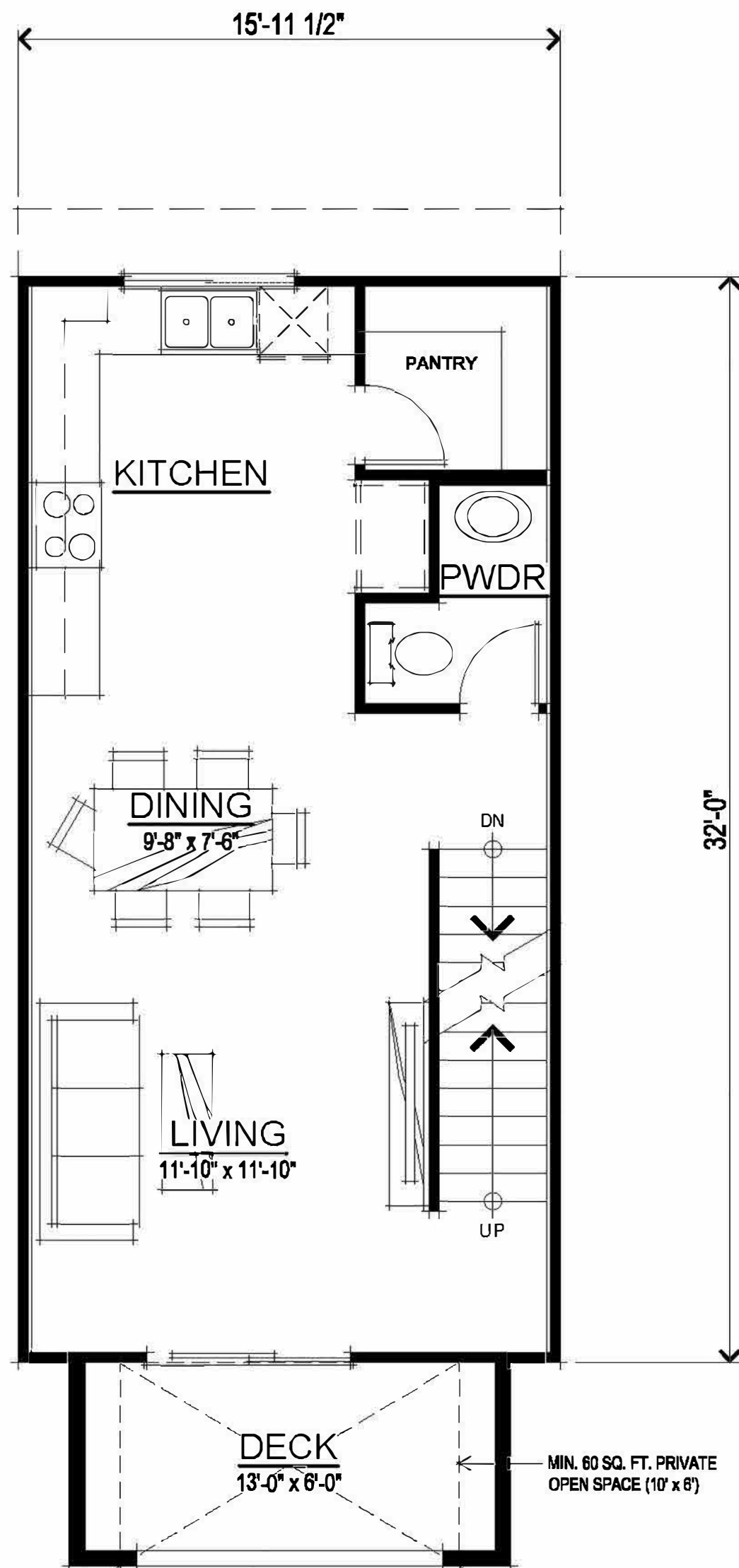
SDG Architects, Inc.  
 3361 Walnut Blvd. Suite 120  
 Brentwood, CA 94513  
 925.634.7000 | sdgarchitectsinc.com



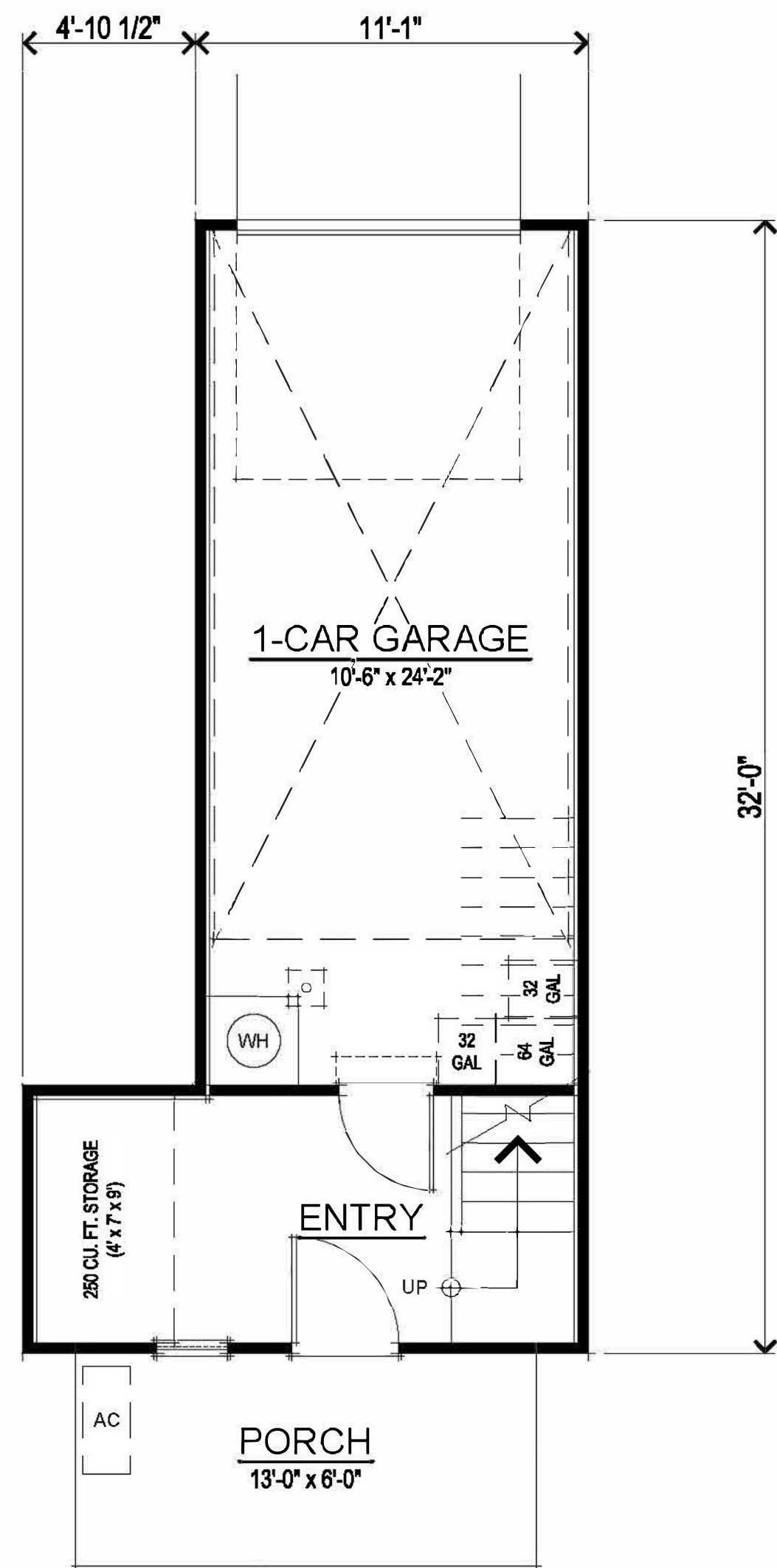




THIRD FLOOR PLAN



SECOND FLOOR PLAN



FIRST FLOOR PLAN  
w/ 1-CAR GARAGE

SQUARE FOOTAGES		GARAGE SQUARE FOOTAGES	
FIRST FLOOR	122 SQ. FT.	1-CAR GARAGE	273 SQ. FT.
SECOND FLOOR	513 SQ. FT.		
THIRD FLOOR	500 SQ. FT.		
TOTAL LIVING	1135 SQ. FT.		
DECK	78 SQ. FT.		

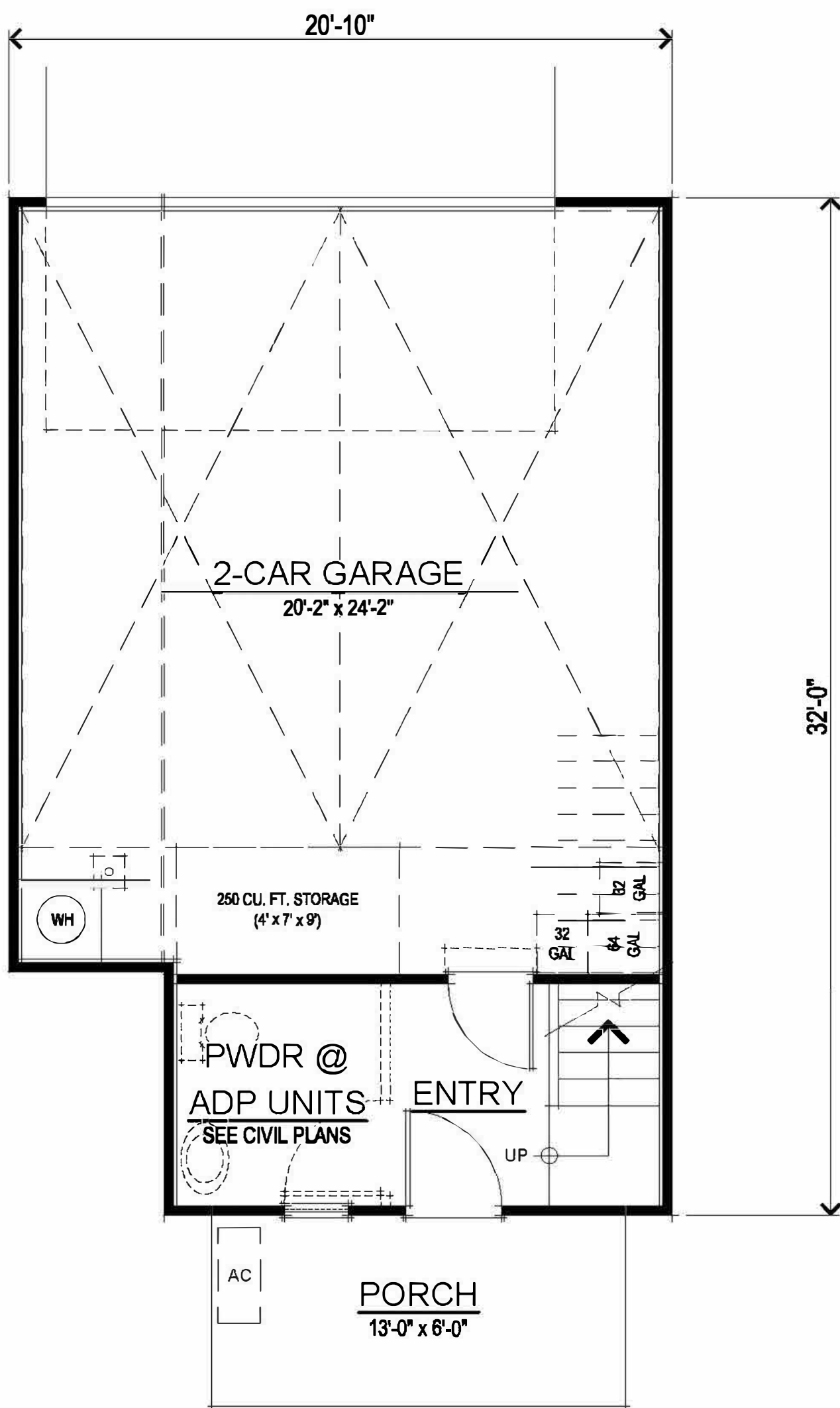
ROW TOWNHOMES



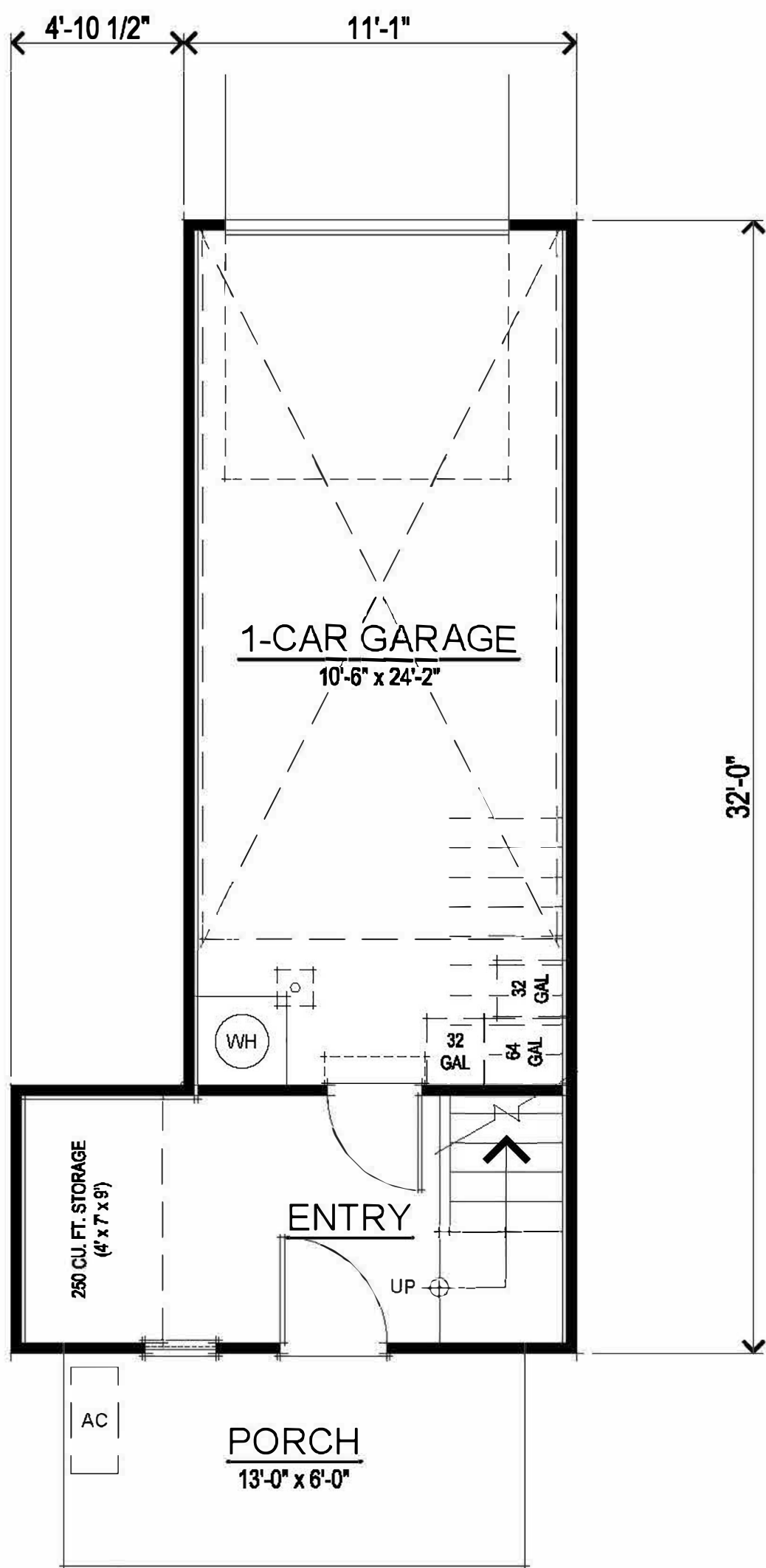


THIRD FLOOR PLAN

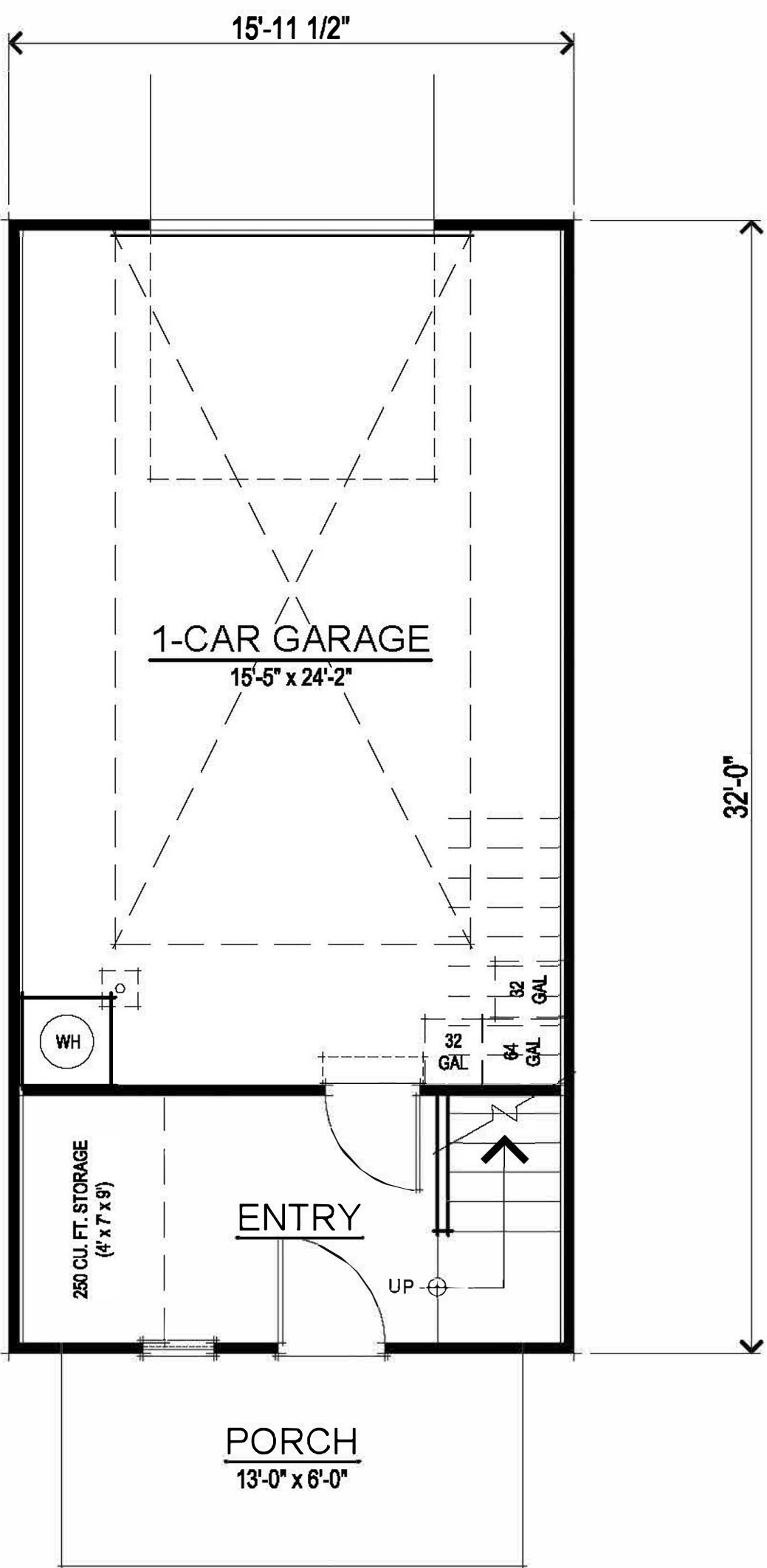
SECOND FLOOR PLAN



FIRST FLOOR PLAN  
w/ 2-CAR GARAGE



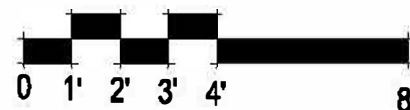
FIRST FLOOR PLAN  
w/ 1-CAR GARAGE



FIRST FLOOR PLAN  
w/ 1-CAR PLUS GARAGE

SQUARE FOOTAGES		GARAGE SQUARE FOOTAGES	
FIRST FLOOR	122 SQ. FT.	1-CAR PLUS GARAGE	392 SQ. FT.
SECOND FLOOR	544 SQ. FT.	1-CAR GARAGE	273 SQ. FT.
THIRD FLOOR	512 SQ. FT.	2-CAR GARAGE	511 SQ. FT.
TOTAL LIVING	1178 SQ. FT.		
DECK	78 SQ. FT.		

ROW TOWNHOMES



UNIT RT2 FLOOR PLANS  
A004

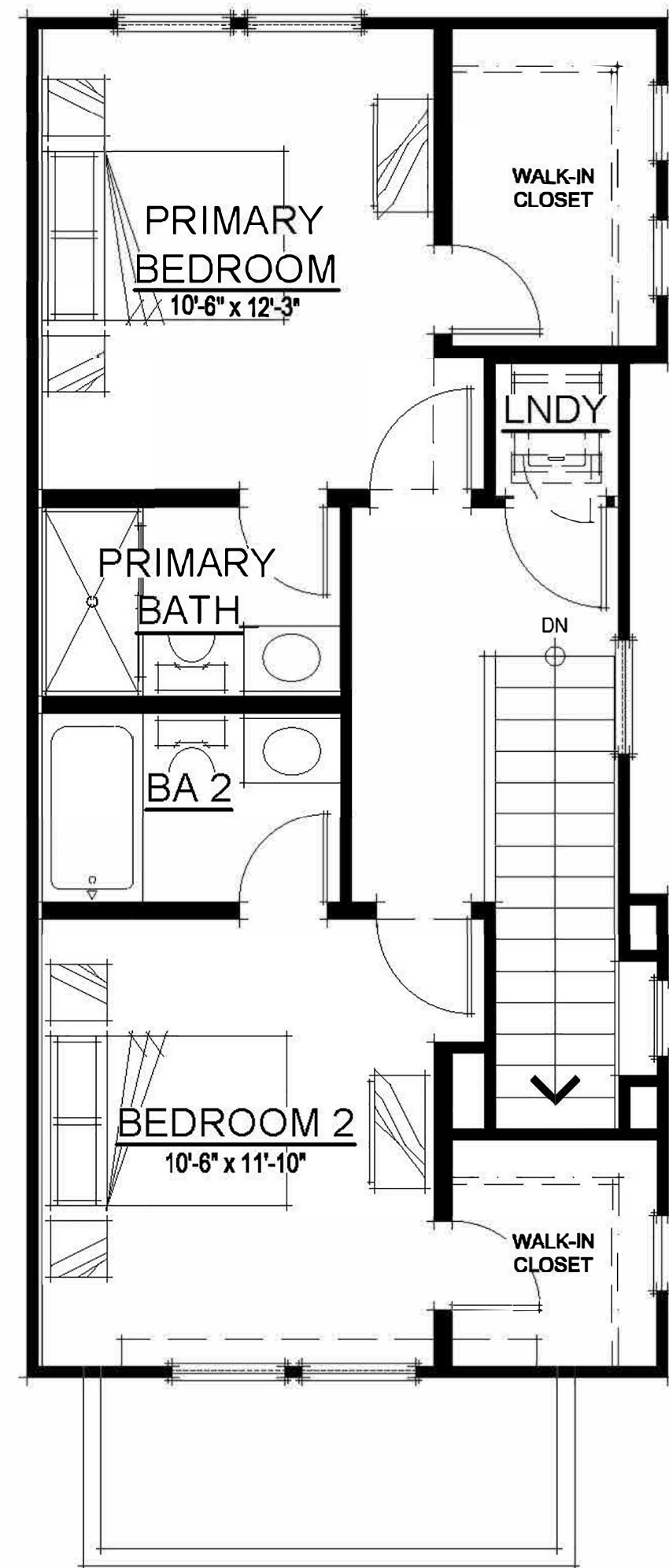
307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

**DeNOVA HOMES**  
1500 Willow Pass Ct., Concord, CA 94520  
925.685.0110

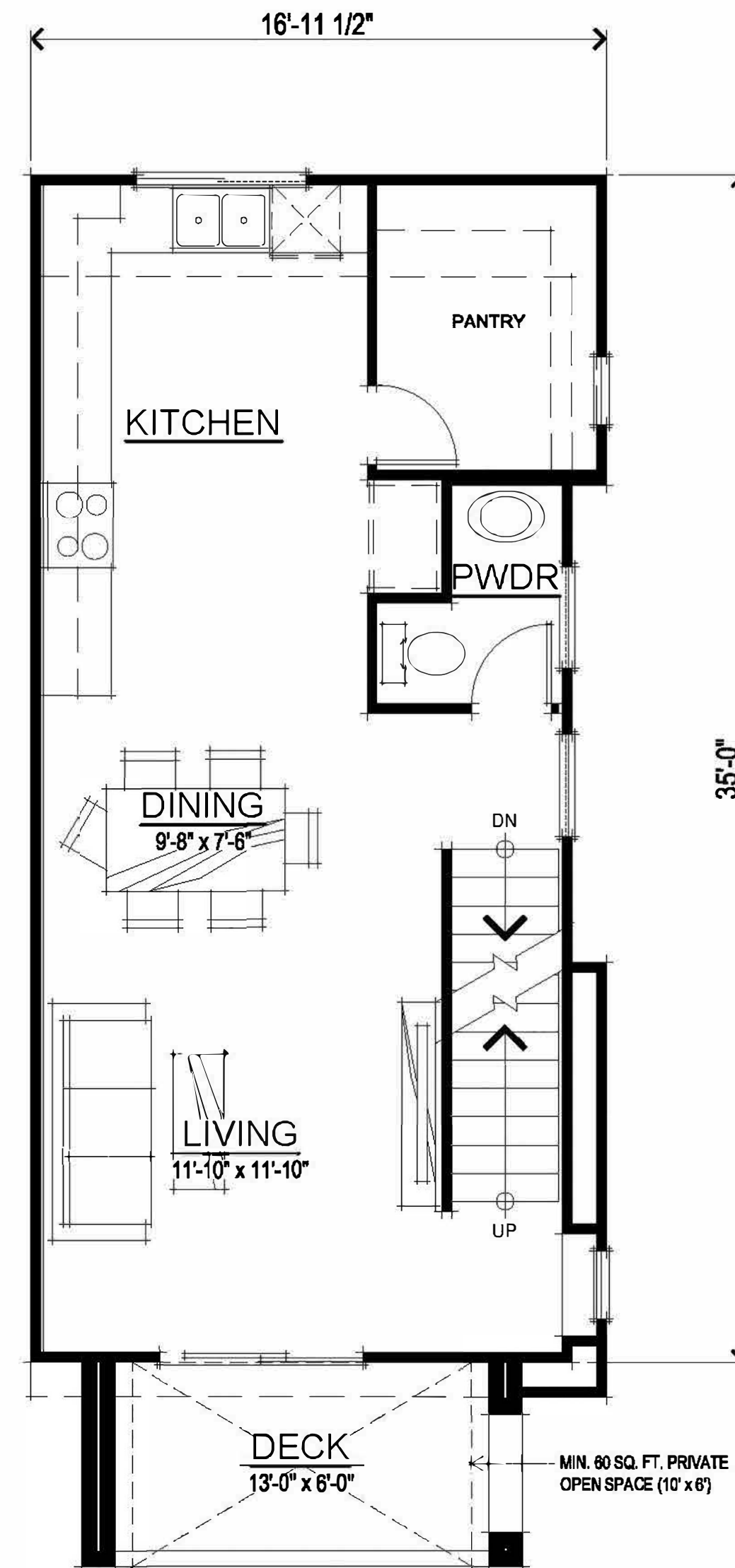
SDG Architects, Inc.  
3361 Walnut Blvd. Suite 120  
Brentwood, CA 94513  
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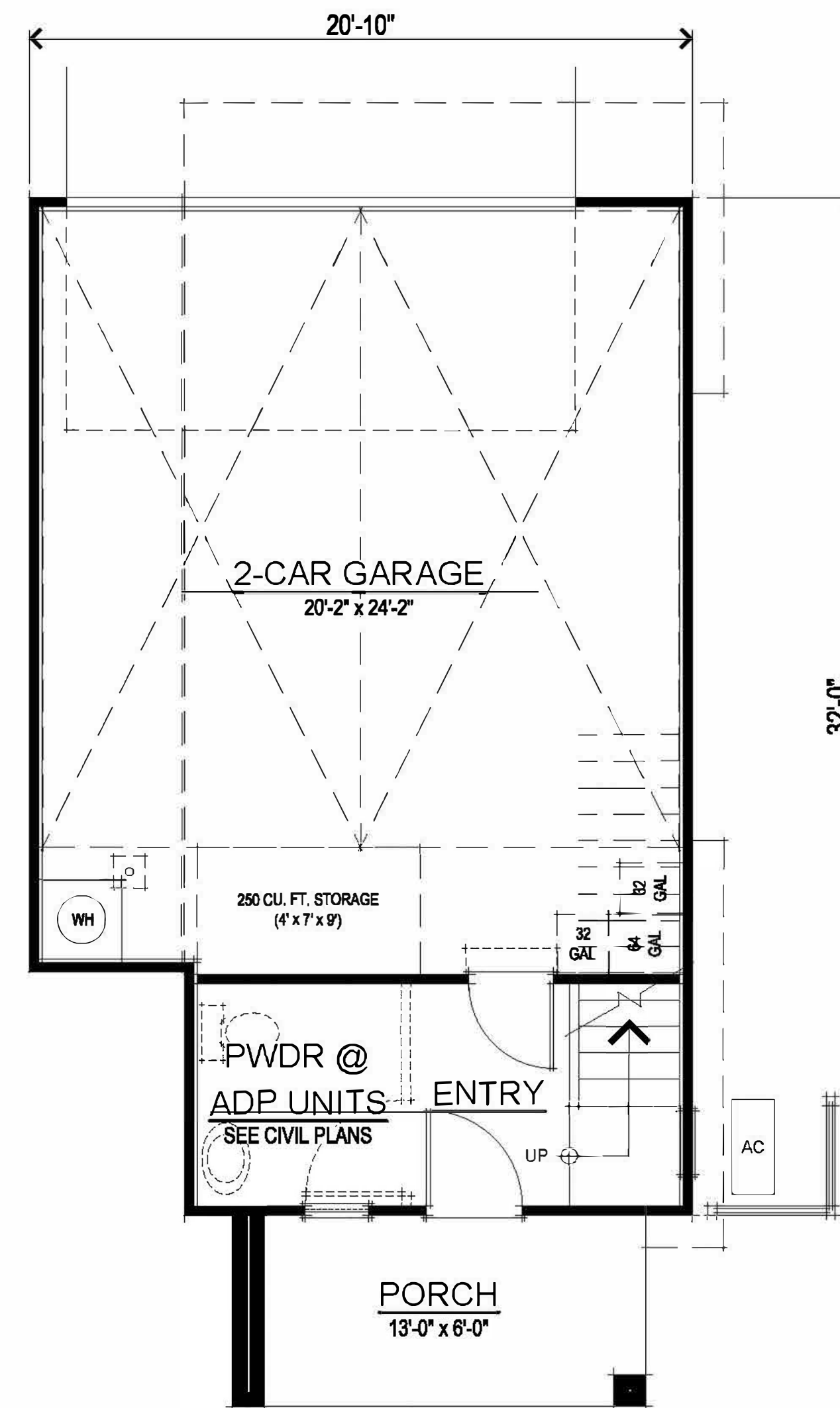




THIRD FLOOR PLAN



SECOND FLOOR PLAN



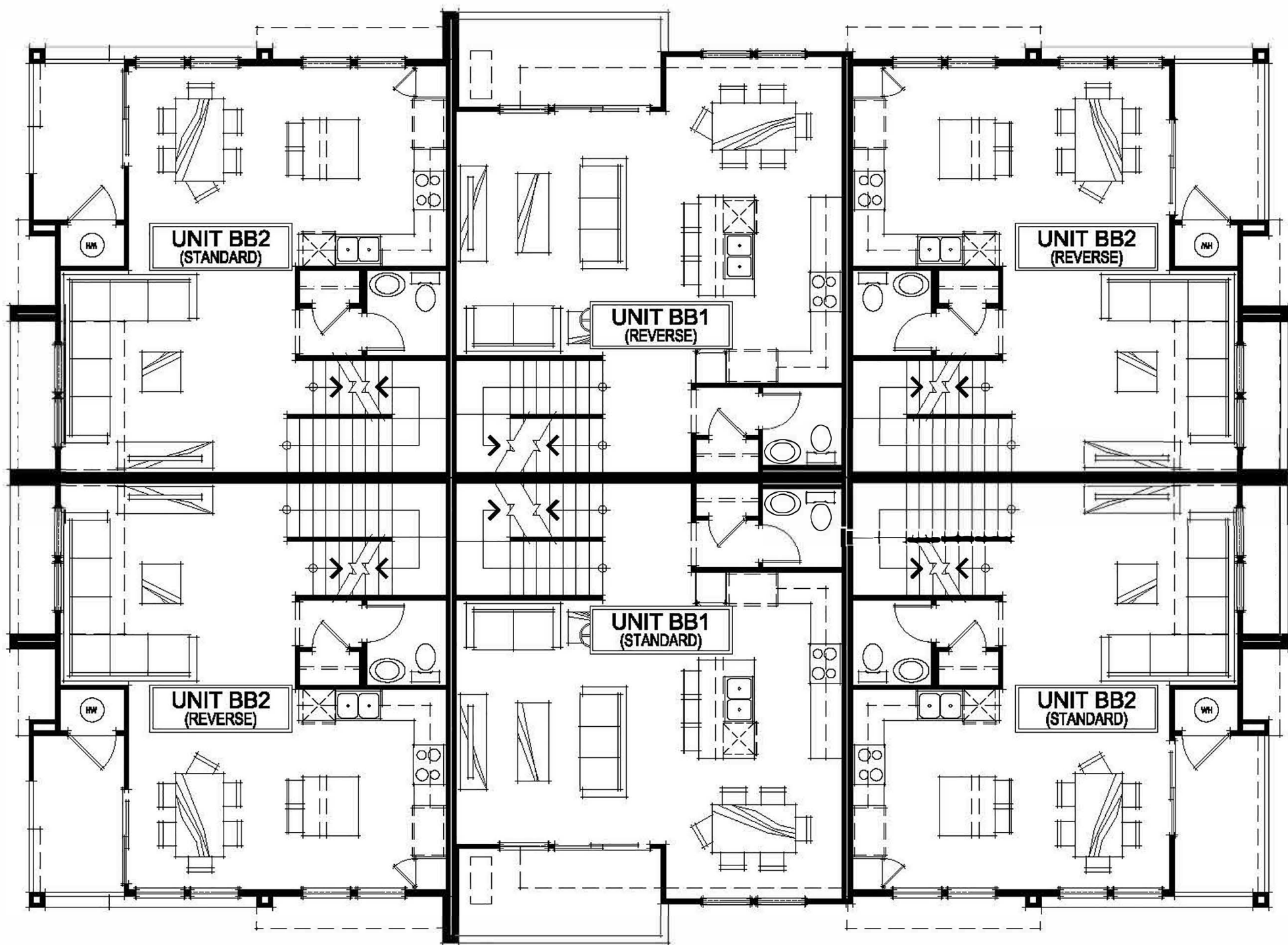
FIRST FLOOR PLAN

SQUARE FOOTAGES		GARAGE SQUARE FOOTAGES	
FIRST FLOOR	122 SQ. FT.	1-CAR PLUS GARAGE	392 SQ. FT.
SECOND FLOOR	569 SQ. FT.	2-CAR GARAGE	511 SQ. FT.
THIRD FLOOR	547 SQ. FT.		
TOTAL LIVING	1238 SQ. FT.		
DECK	78 SQ. FT.		

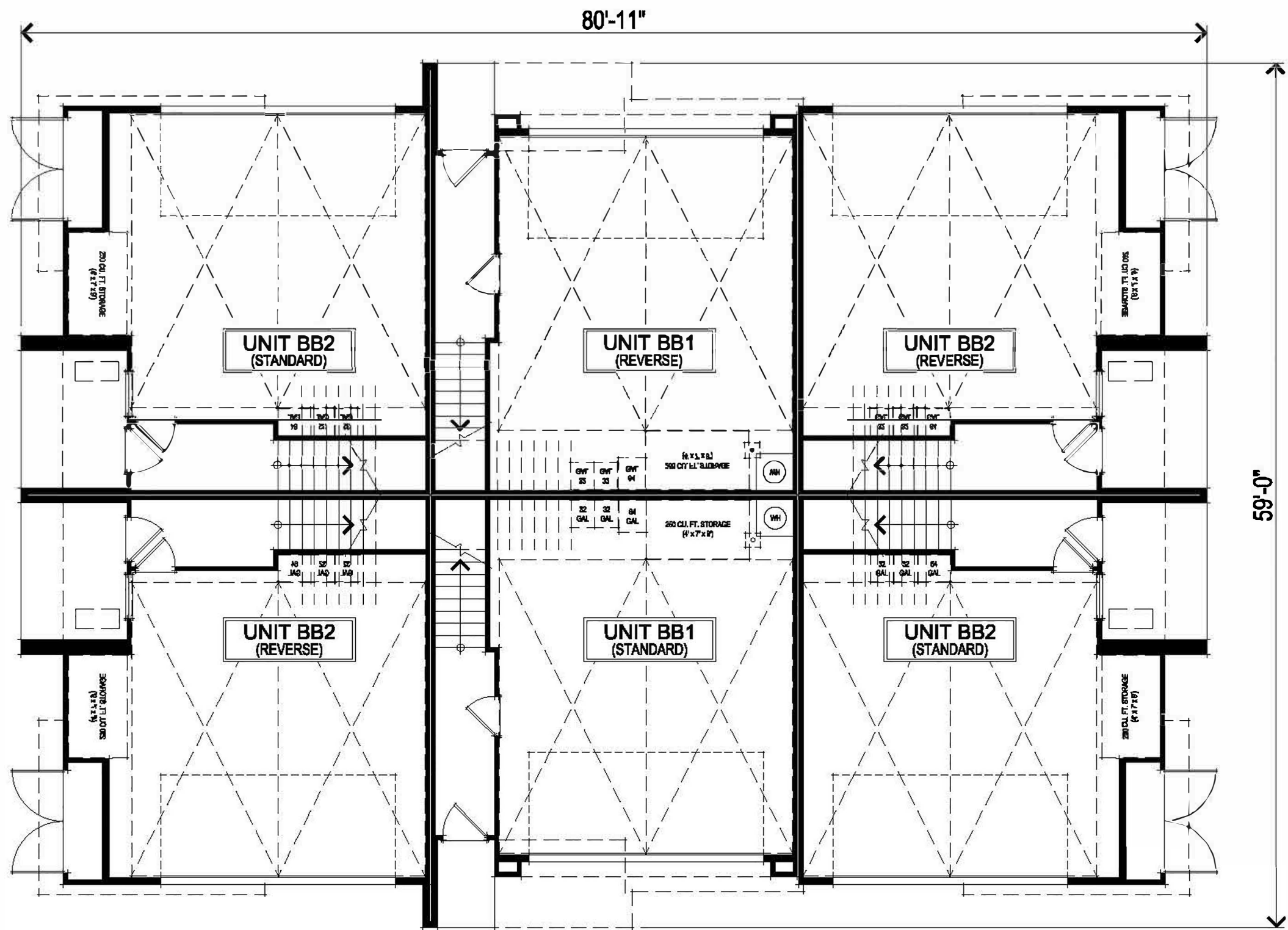
ROW TOWNHOMES

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024





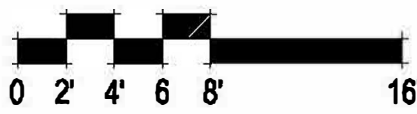
SECOND FLOOR PLAN



FIRST FLOOR PLAN

BACK TO BACK TOWNHOMES

6 UNIT B2B BLDG FIRST & SECOND FLOOR PLANS  
A006



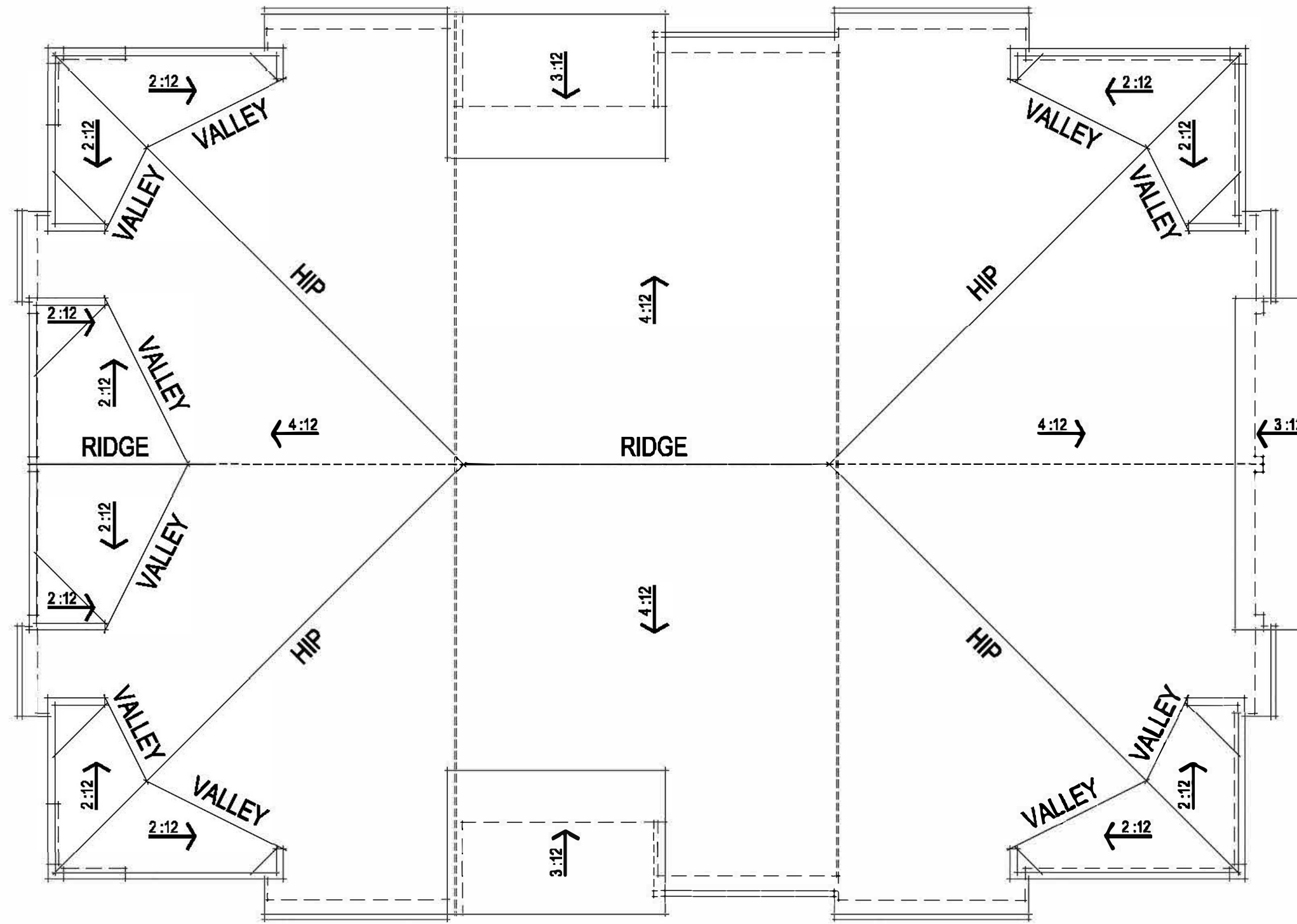
307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

**dh** DENOVA HOMES  
1500 Willow Pass Ct., Concord, CA 94520  
925.685.0110

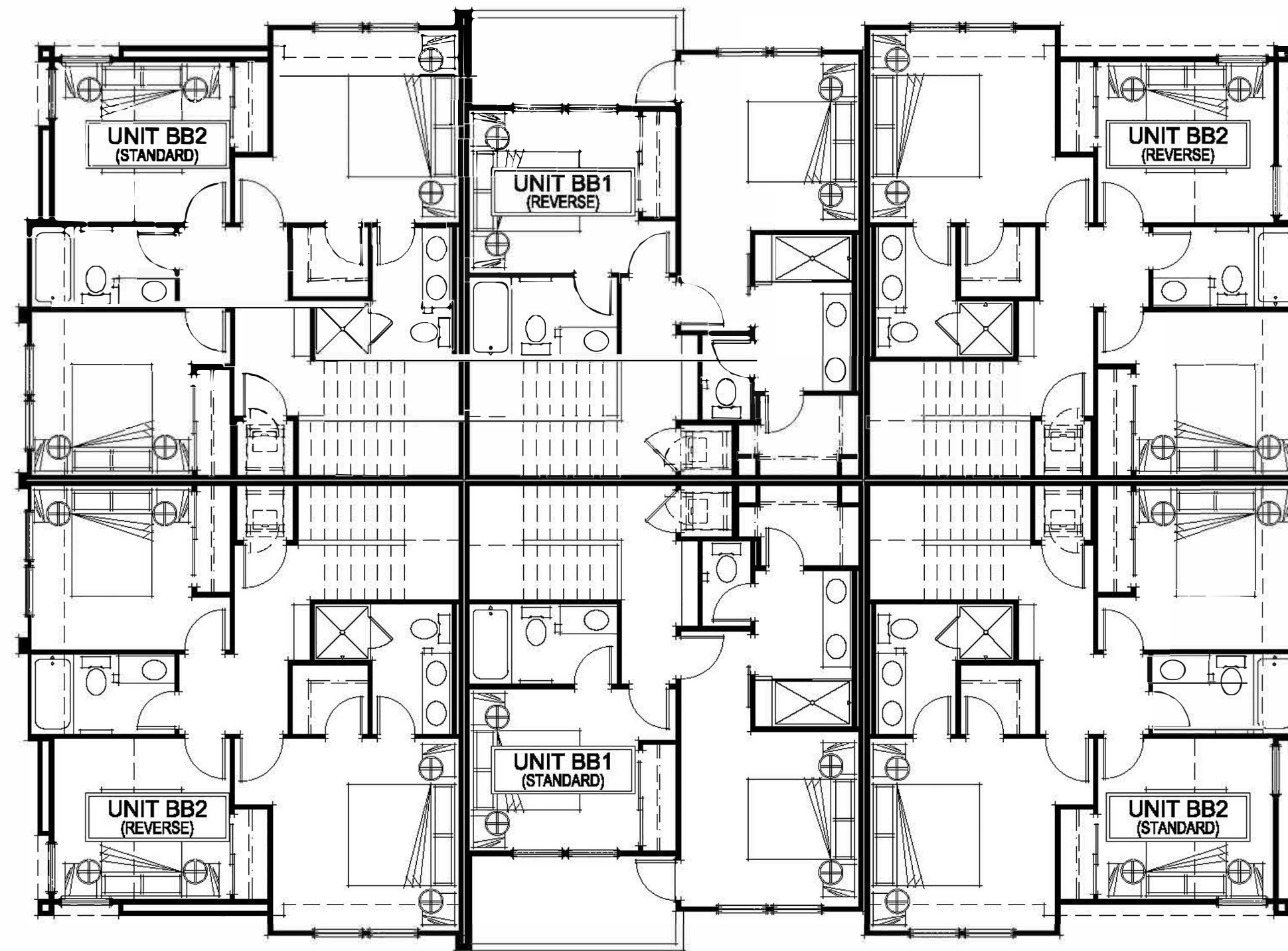
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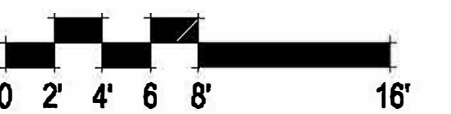


ROOF PLAN



THIRD FLOOR PLAN

BACK TO BACK TOWNHOMES



6 UNIT B2B BLDG THIRD FLOOR & ROOF PLANS  
A007

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024





LIGHTING PER  
PHOTOMETRICS

REAR ELEVATION



LEFT ELEVATION



RIGHT ELEVATION



COMP. SHINGLE  
ROOFING

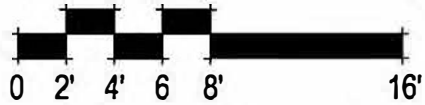
STUCCO FINISH

HARDI LAP SIDING

STUCCO FINISH

FRONT ELEVATION

BACK TO BACK TOWNHOMES



6 UNIT B2B BLDG ELEVATIONS  
A008

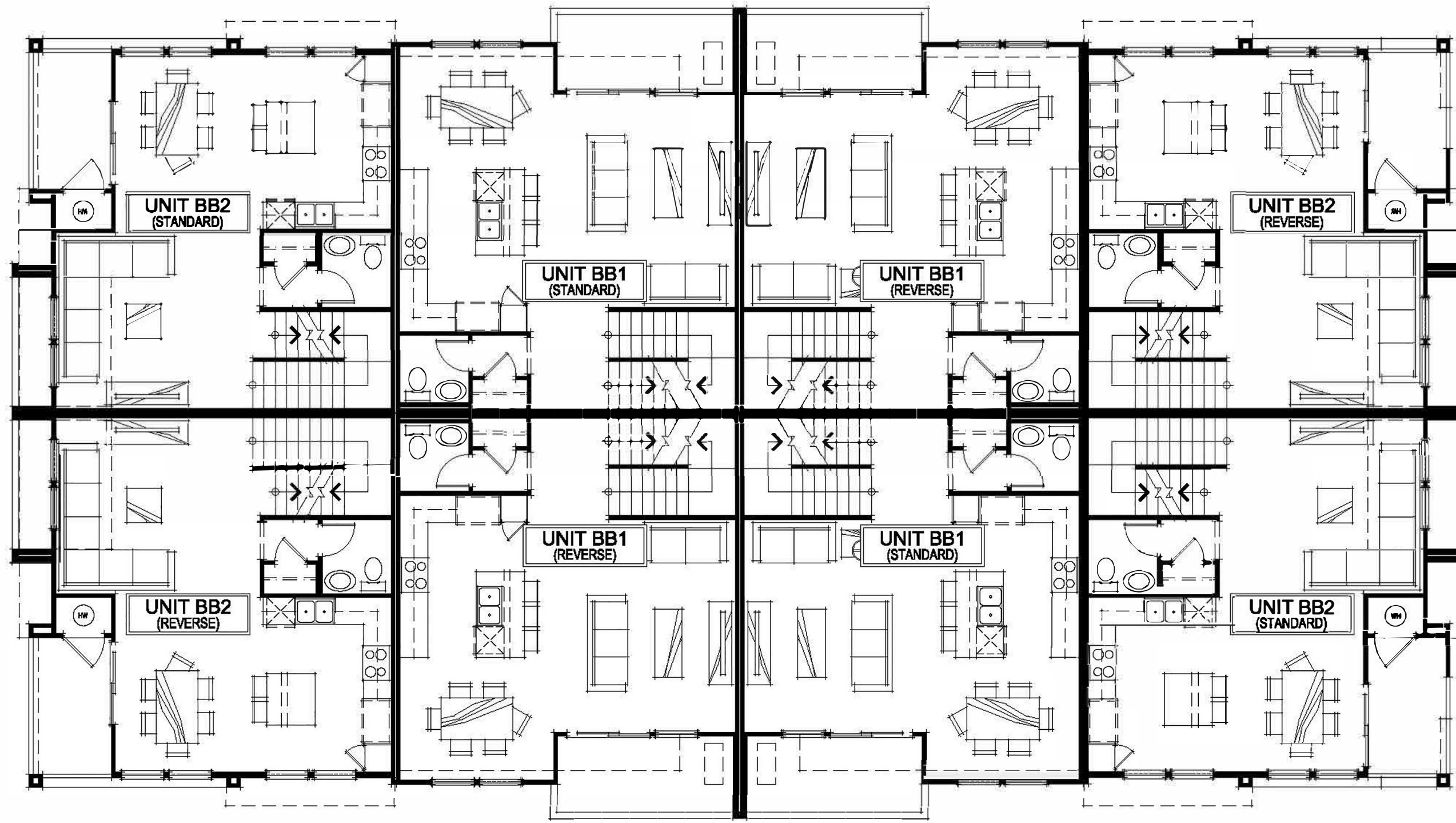
307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

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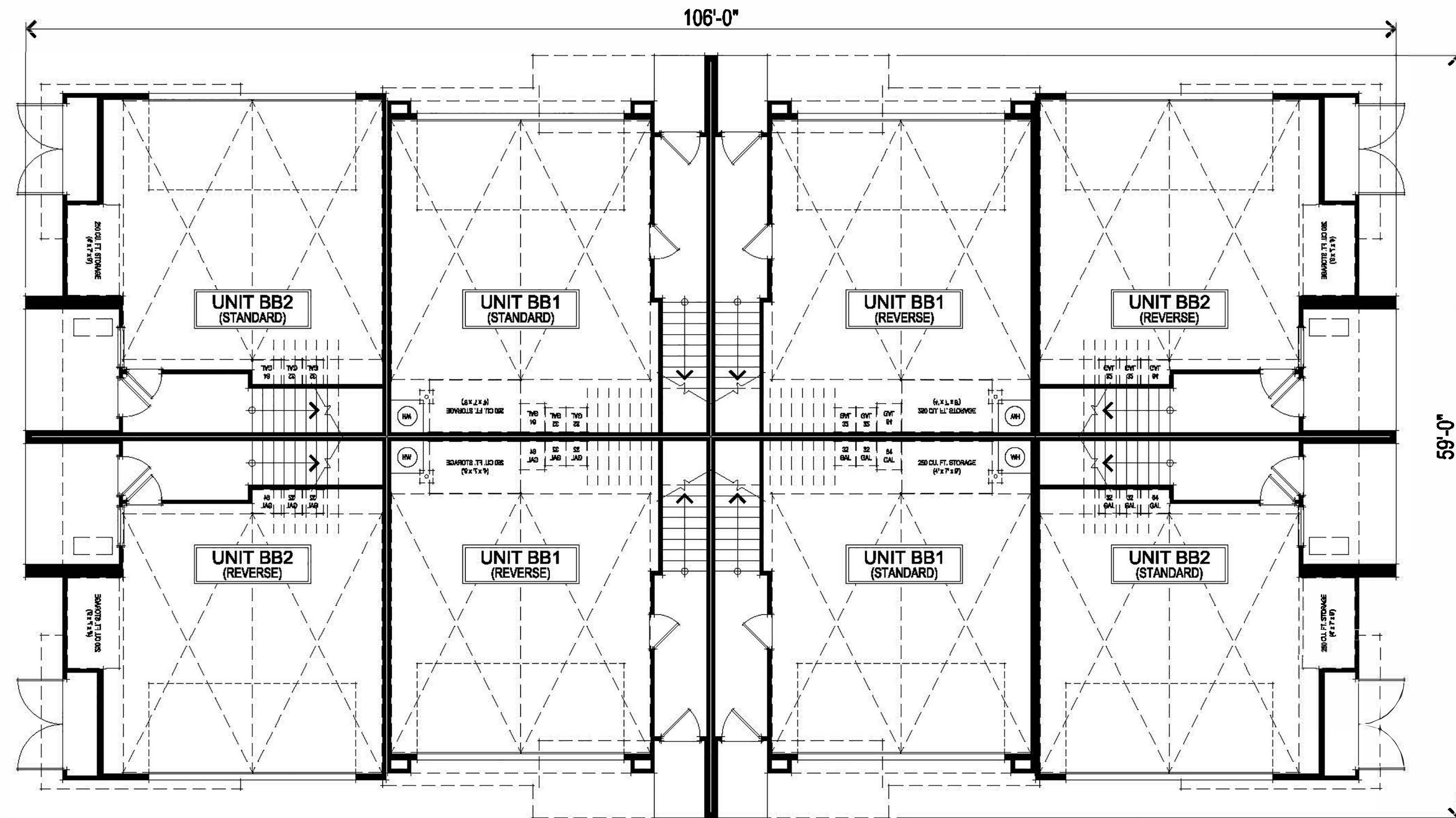
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Brentwood, CA 94513  
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SECOND FLOOR PLAN



FIRST FLOOR PLAN

BACK TO BACK TOWNHOMES

8 UNIT B2B BLDG FIRST & SECOND FLOOR PLANS

A009

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

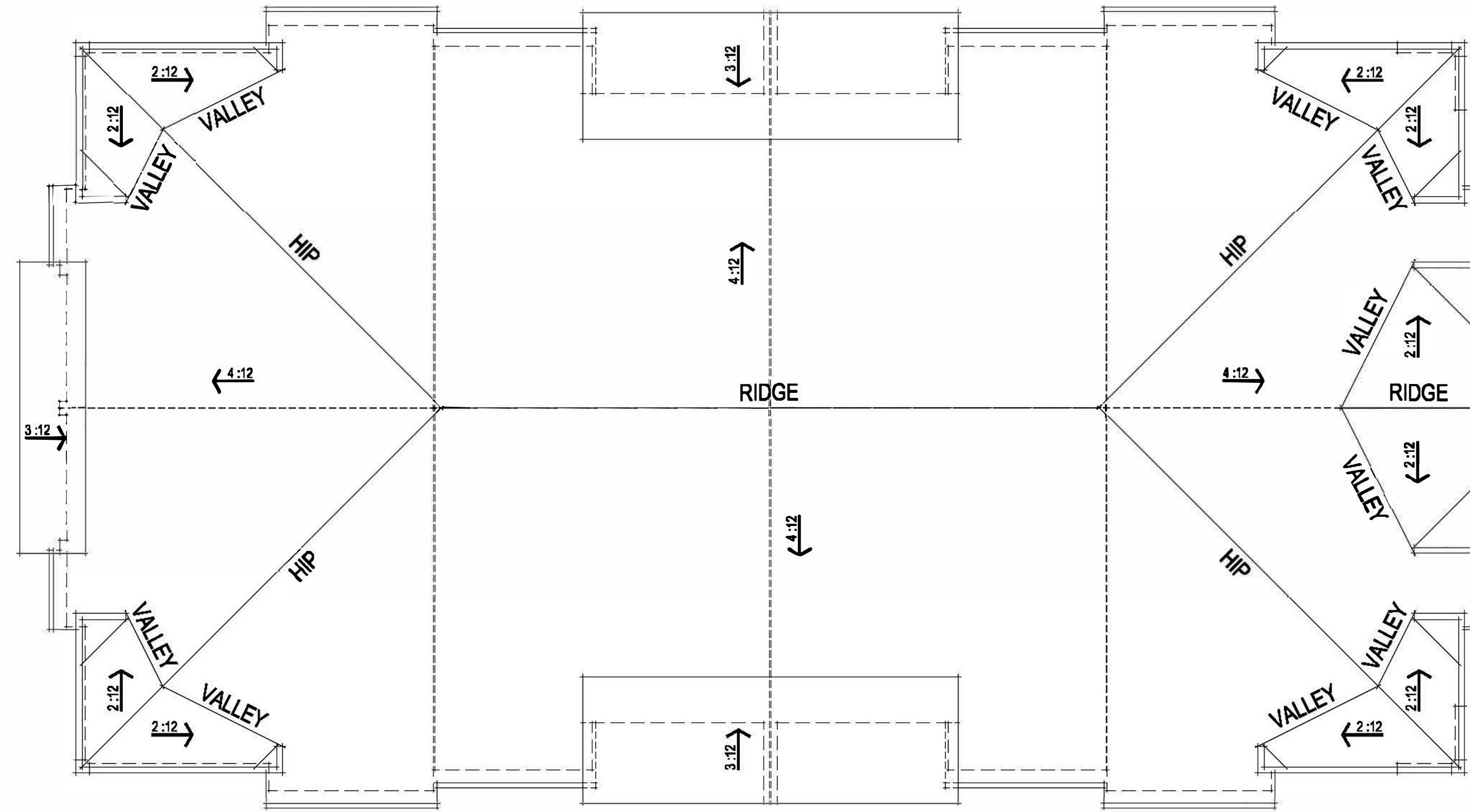
**dh** DENOVA HOMES  
1500 Willow Pass Ct., Concord, CA 94520  
925.685.0110

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Brentwood, CA 94513  
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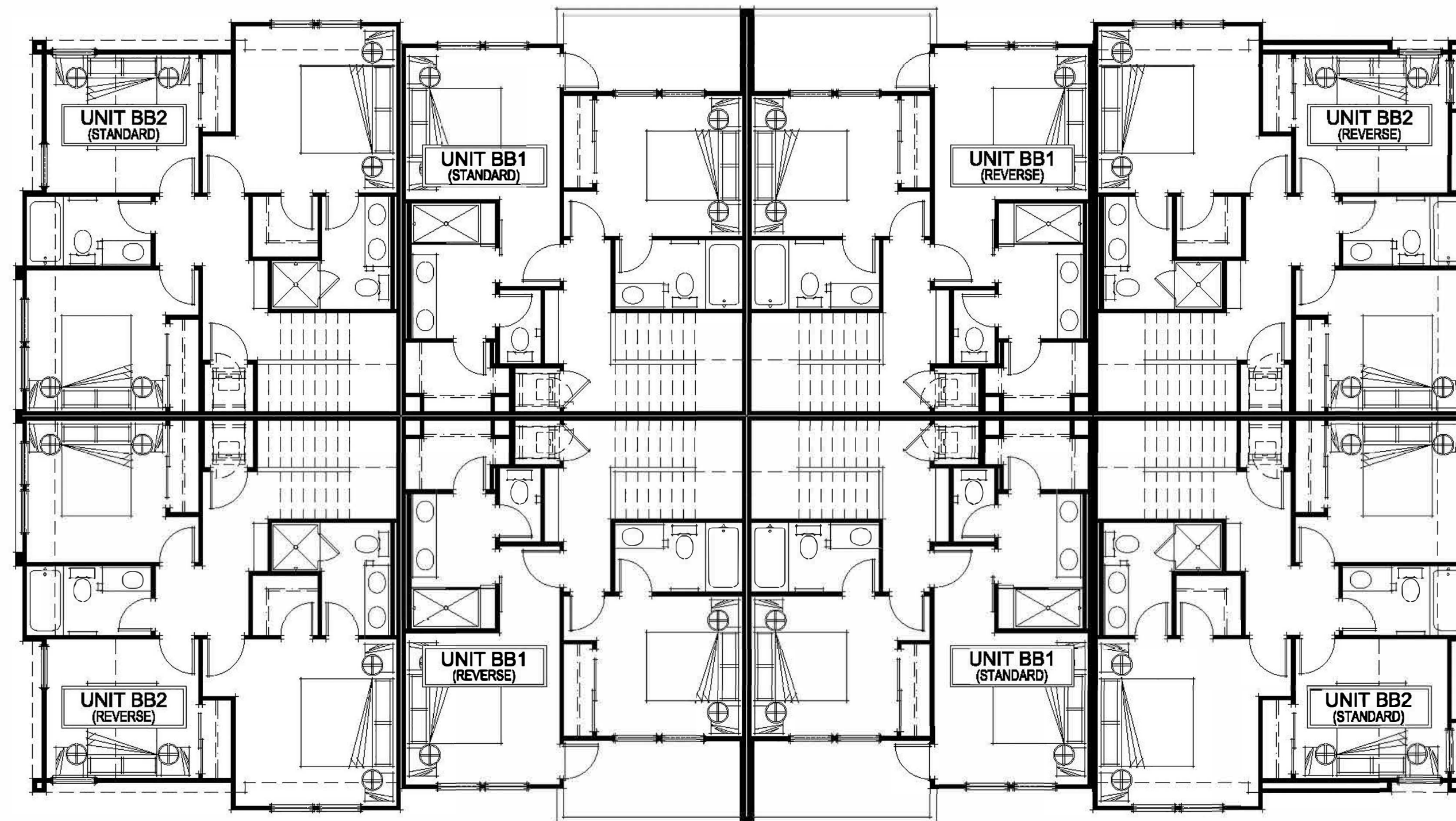


A67



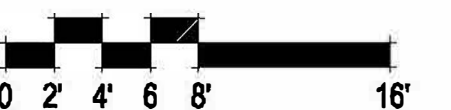


ROOF PLAN



THIRD FLOOR PLAN

BACK TO BACK TOWNHOMES



8 UNIT B2B BLDG THIRD FLOOR & ROOF PLANS  
A010

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

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A68





LIGHTING PER  
PHOTOMETRICS

REAR ELEVATION



LEFT ELEVATION



RIGHT ELEVATION



COMP. SHINGLE  
ROOFING

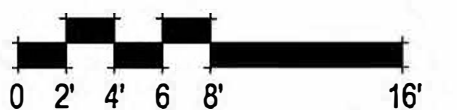
STUCCO FINISH

HARDI LAP SIDING

STUCCO FINISH

FRONT ELEVATION

BACK TO BACK TOWNHOMES



8 UNIT B2B BLDG ELEVATIONS

A011

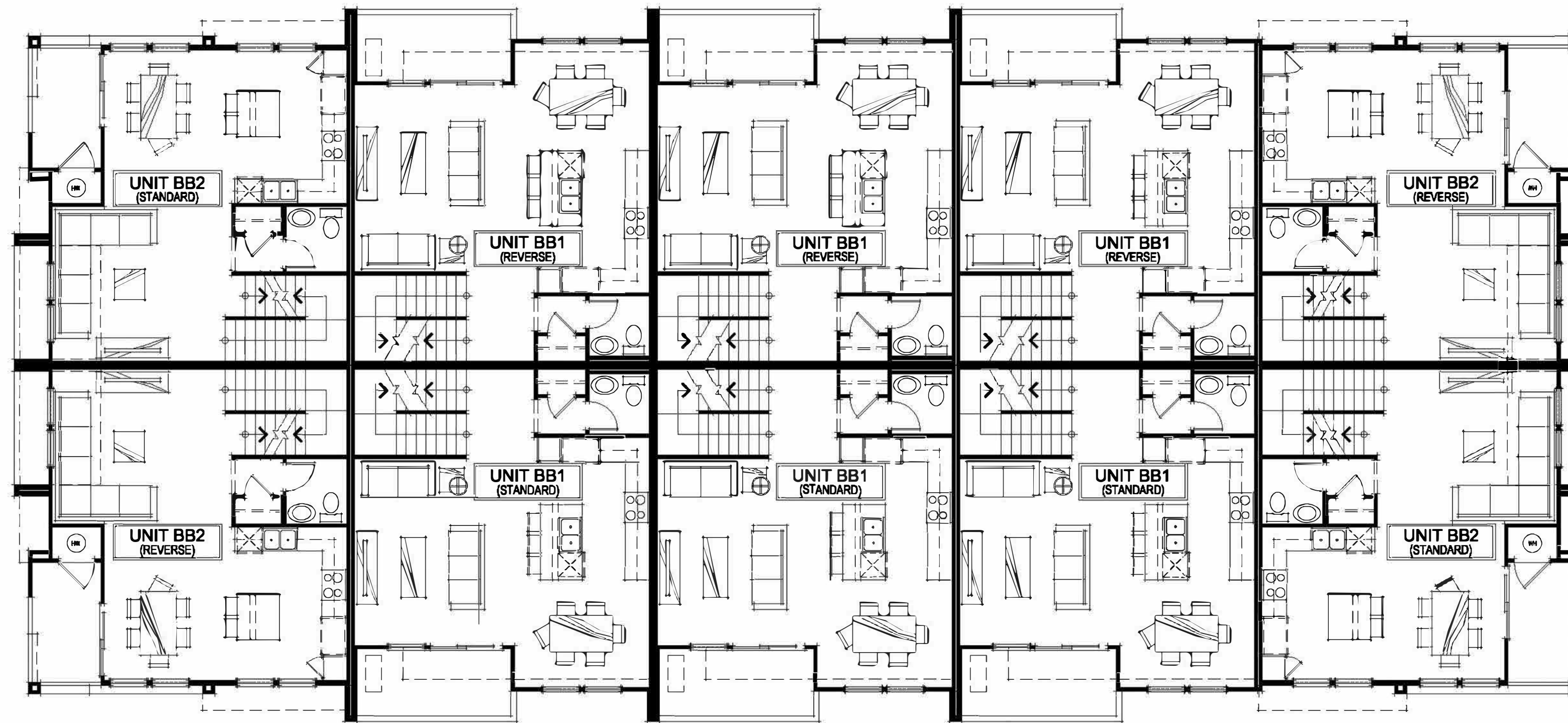
307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

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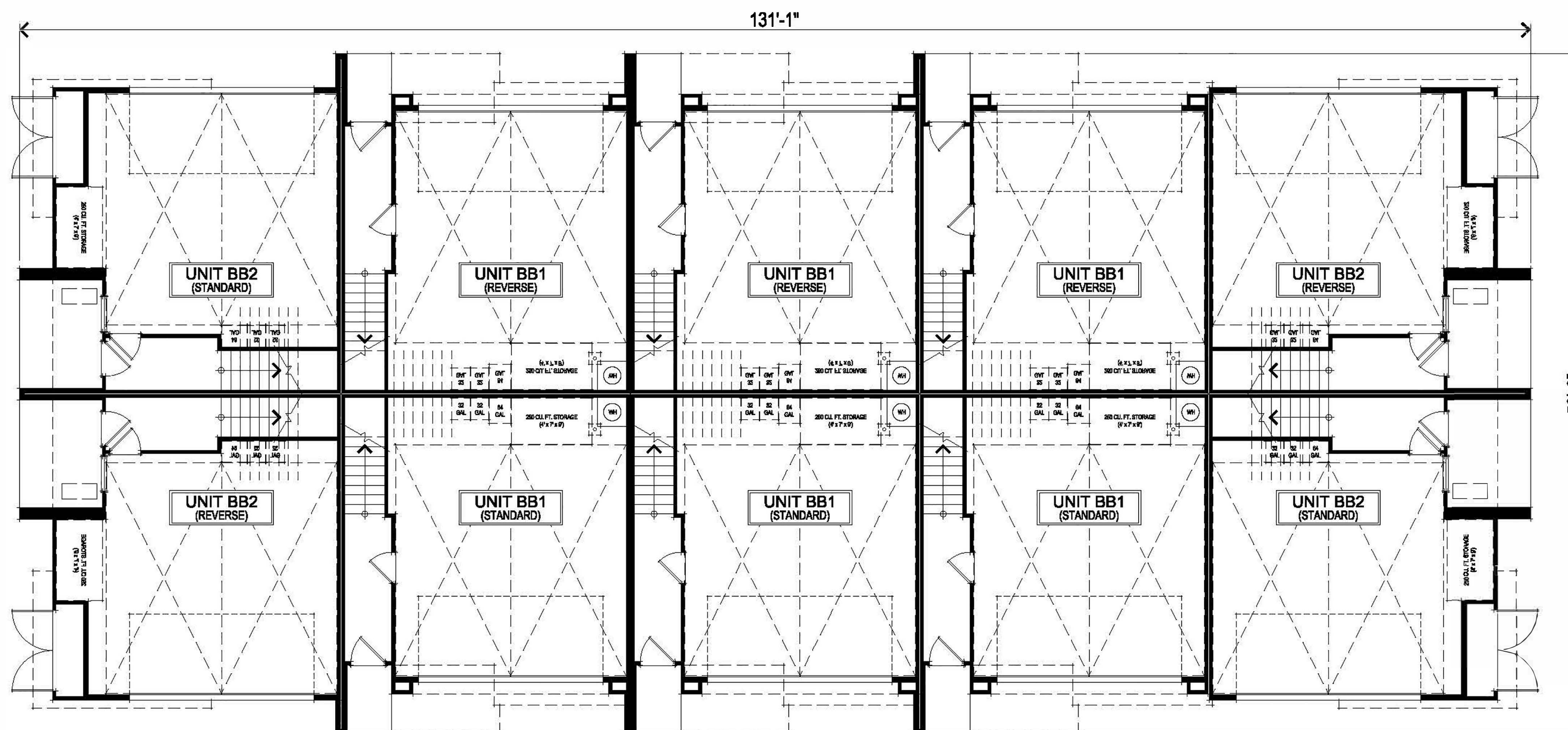
SDG Architects, Inc.  
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Brentwood, CA 94513  
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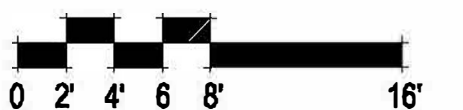




SECOND FLOOR PLAN



FIRST FLOOR PLAN



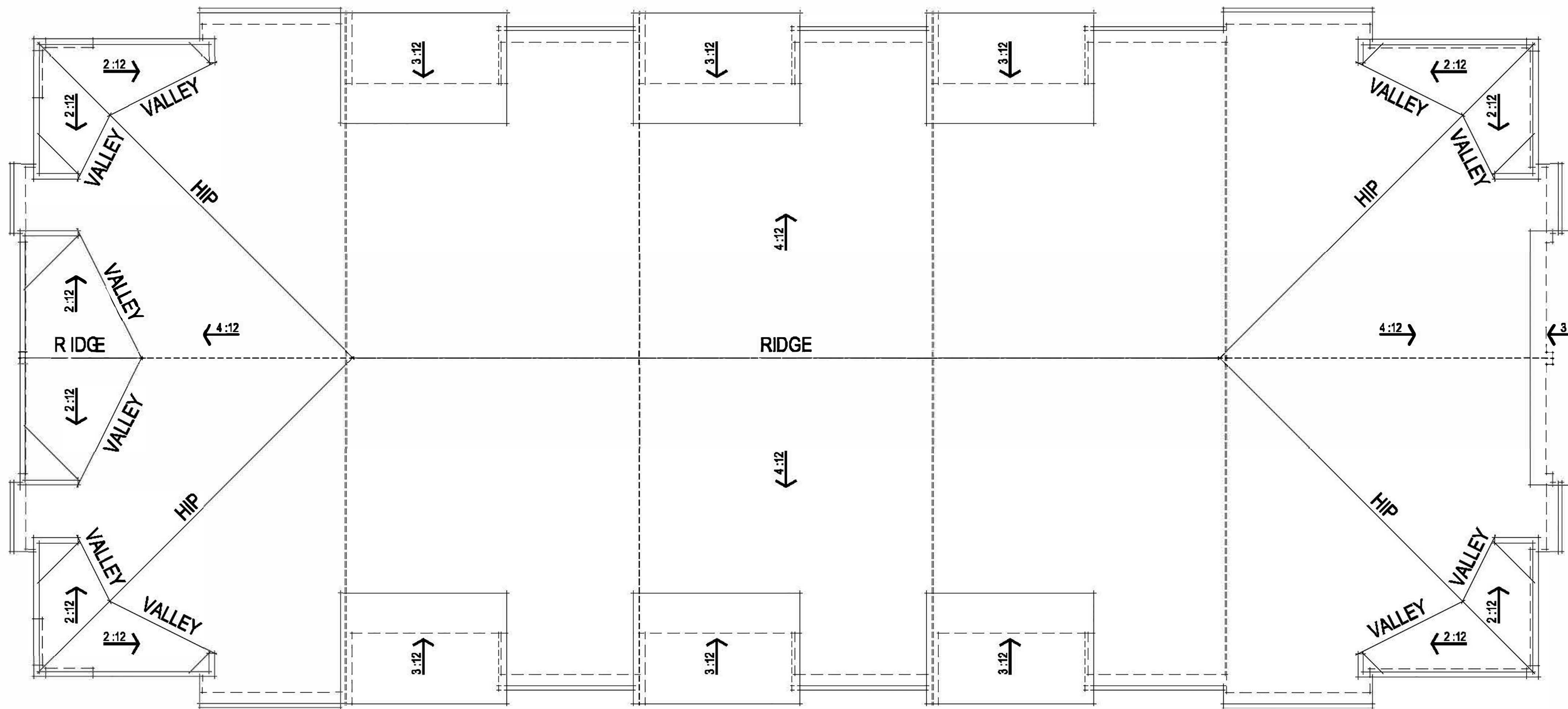
10 UNIT B2B BLDG FIRST & SECOND FLOOR PLANS

A012

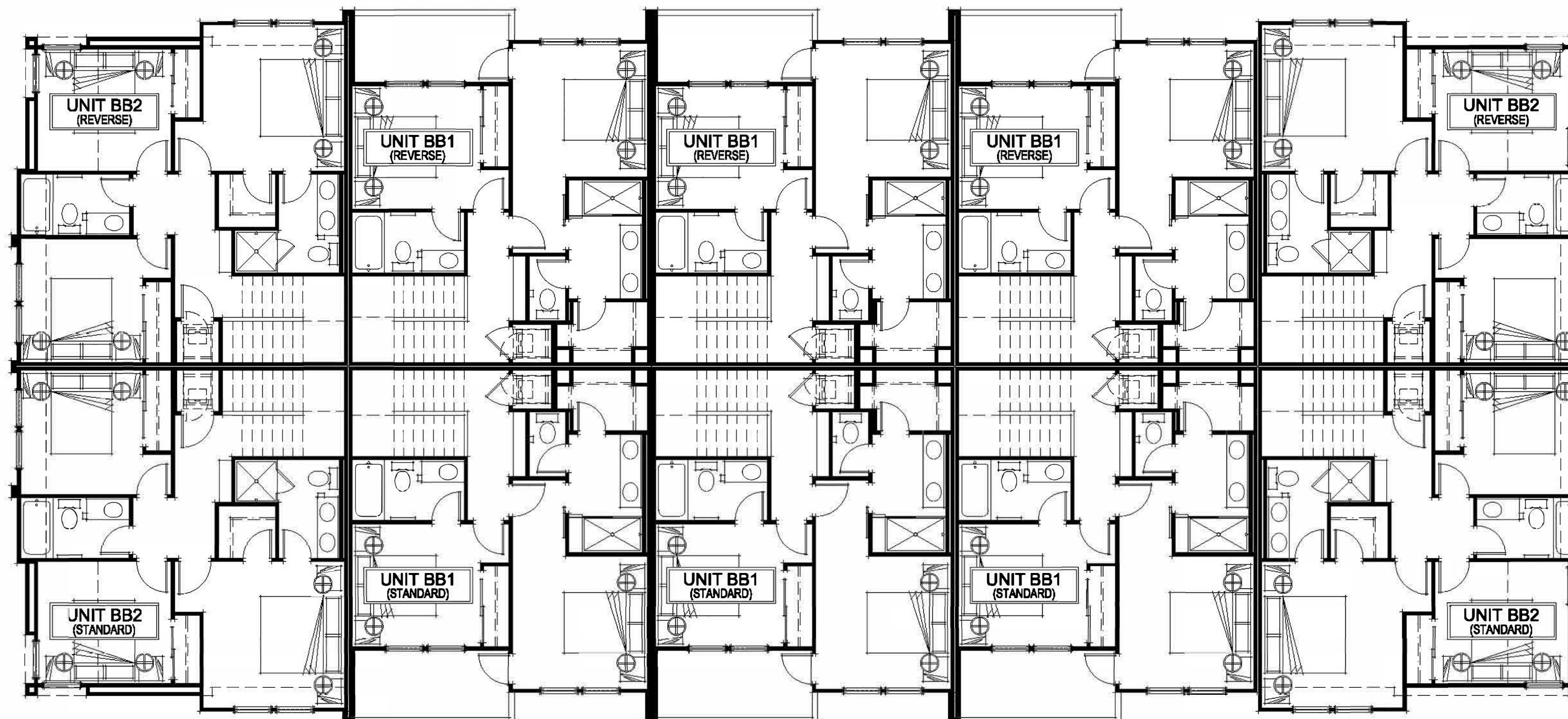
BACK TO BACK TOWNHOMES

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

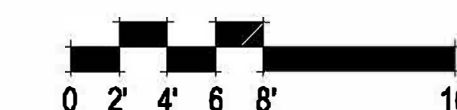




ROOF PLAN



THIRD FLOOR PLAN



10 UNIT B2B BLDG THIRD FLOOR & ROOF PLANS  
A013

BACK TO BACK TOWNHOMES

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

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A71





LIGHTING PER  
PHOTOMETRICS

REAR ELEVATION



LEFT ELEVATION



RIGHT ELEVATION



COMP. SHINGLE  
ROOFING

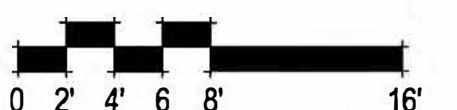
STUCCO FINISH

HARDI LAP SIDING

STUCCO FINISH

FRONT ELEVATION

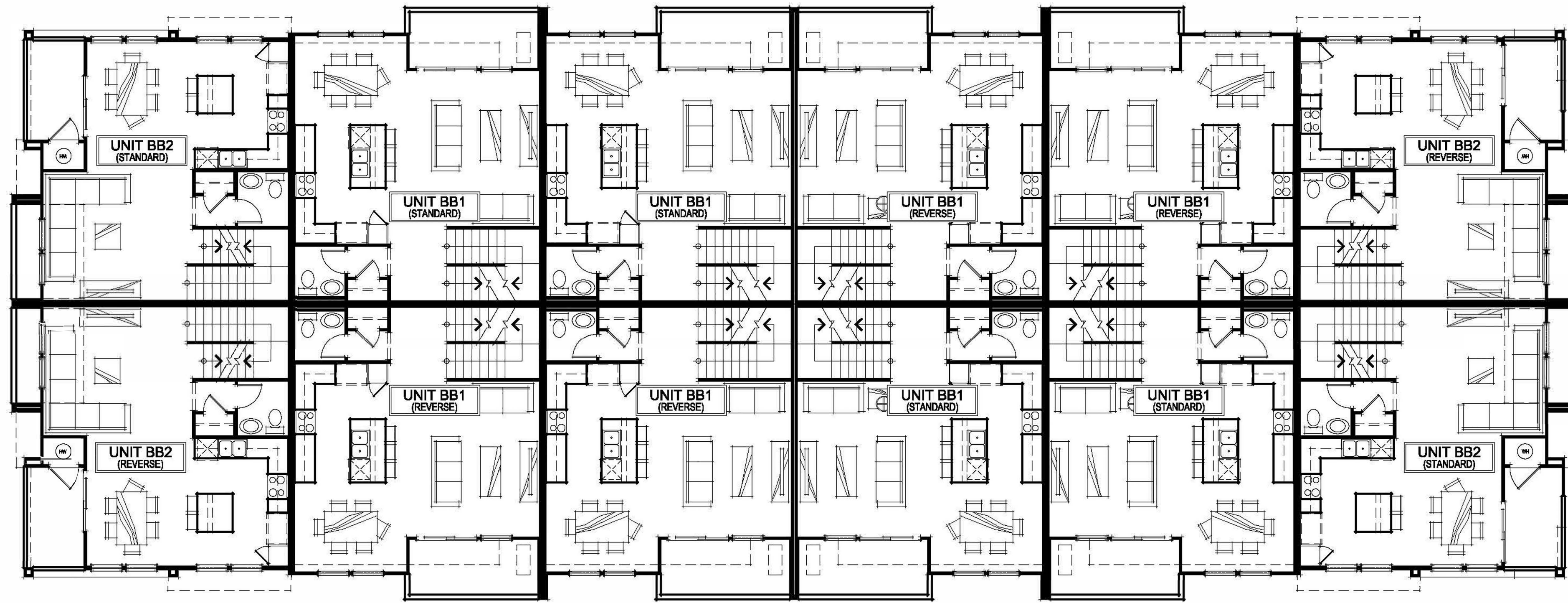
BACK TO BACK TOWNHOMES



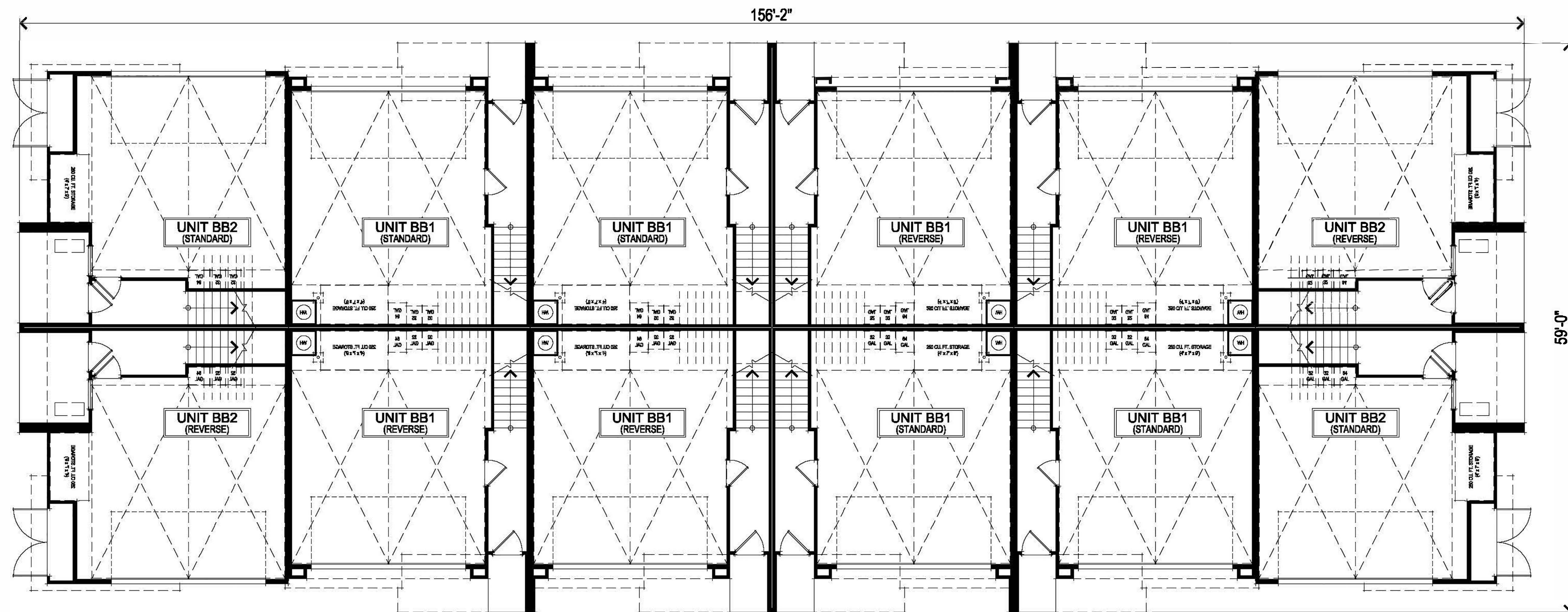
10 UNIT B2B BLDG ELEVATIONS  
A014

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

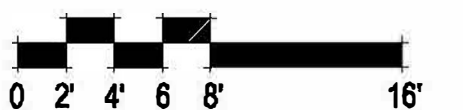




SECOND FLOOR PLAN



FIRST FLOOR PLAN



307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

**DENOVA HOMES**  
1500 Willow Pass Ct., Concord, CA 94520  
925.685.0110

BACK TO BACK TOWNHOMES

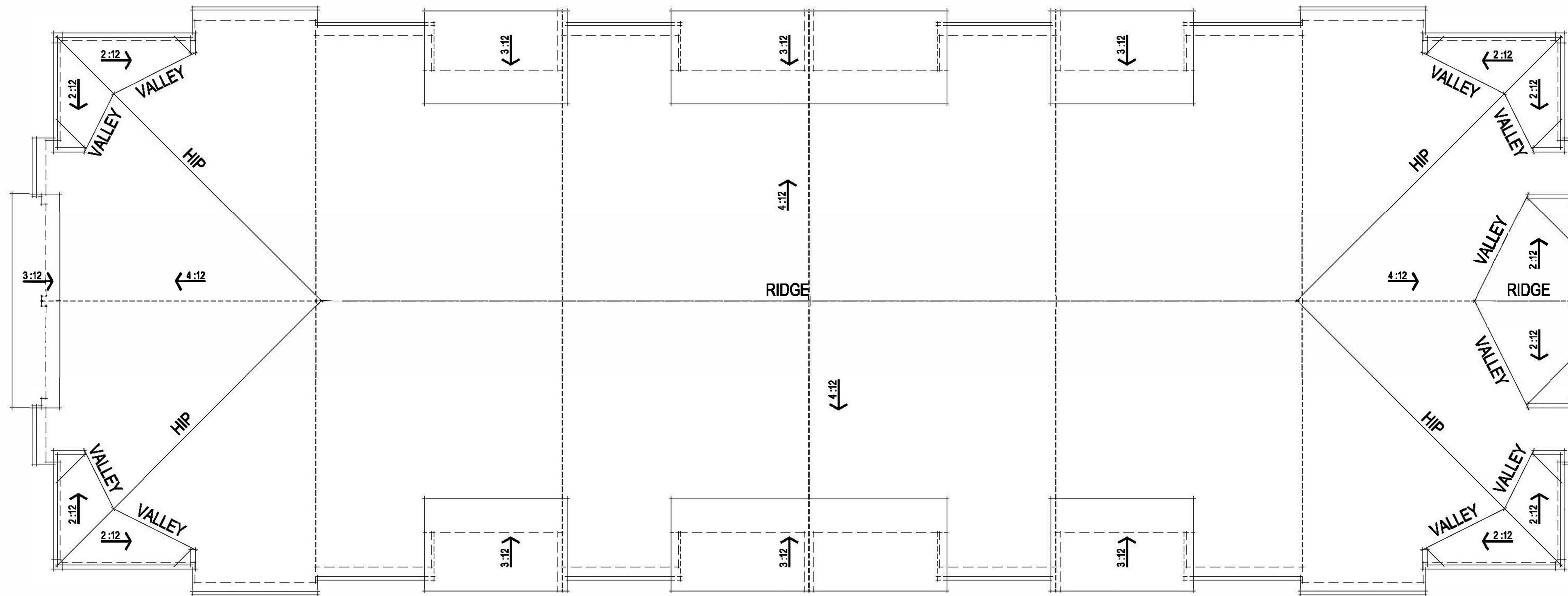
12 UNIT B2B BLDG FIRST & SECOND FLOOR PLANS  
A015

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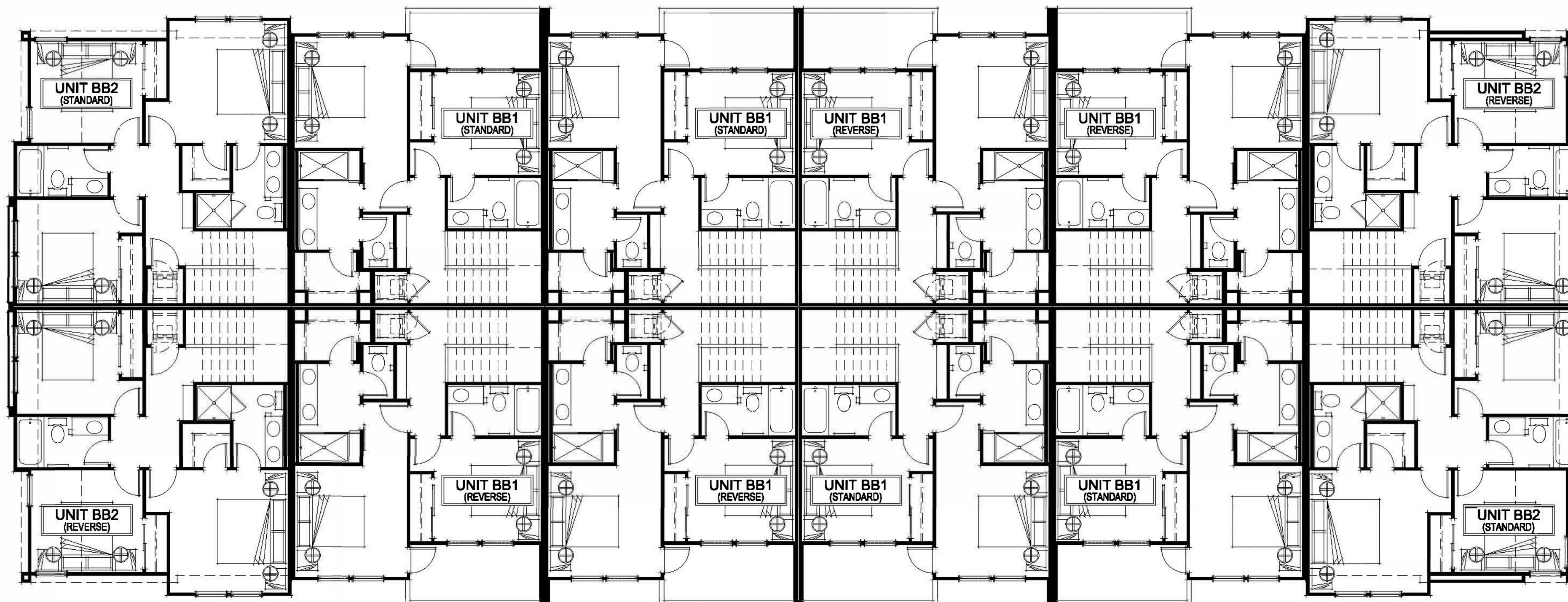


A73

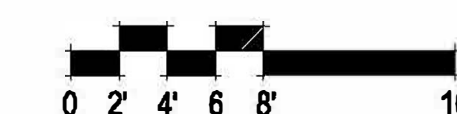




ROOF PLAN



THIRD FLOOR PLAN



12 UNIT B2B BLDG THIRD FLOOR & ROOF PLANS  
A016

BACK TO BACK TOWNHOMES

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024





LIGHTING PER  
PHOTOMETRICS

REAR ELEVATION



LEFT ELEVATION



RIGHT ELEVATION



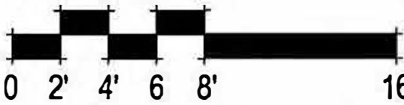
COMP. SHINGLE  
ROOFING

STUCCO FINISH

HARDI LAP SIDING

STUCCO FINISH

FRONT ELEVATION



12 UNIT B2B BLDG ELEVATIONS  
A017

BACK TO BACK TOWNHOMES

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

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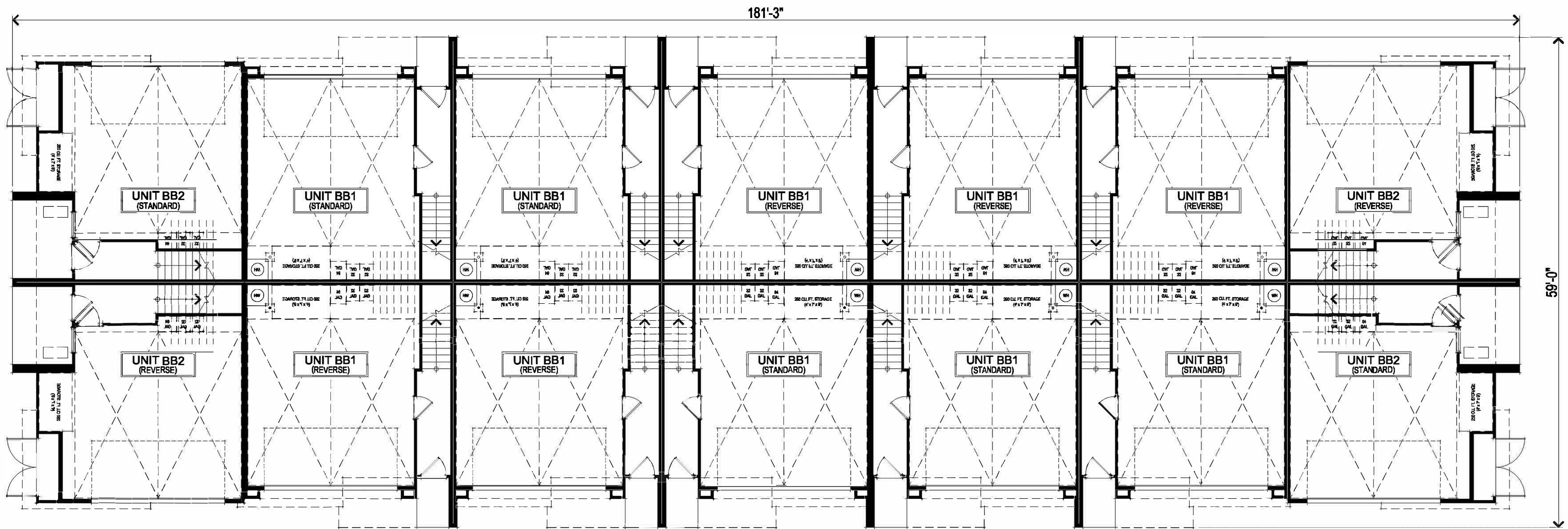
SDG Architects, Inc.  
3361 Walnut Blvd. Suite 120  
Brentwood, CA 94513  
925.634.7000 | sdgarchitectsinc.com



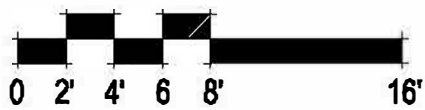




SECOND FLOOR PLAN



FIRST FLOOR PLAN

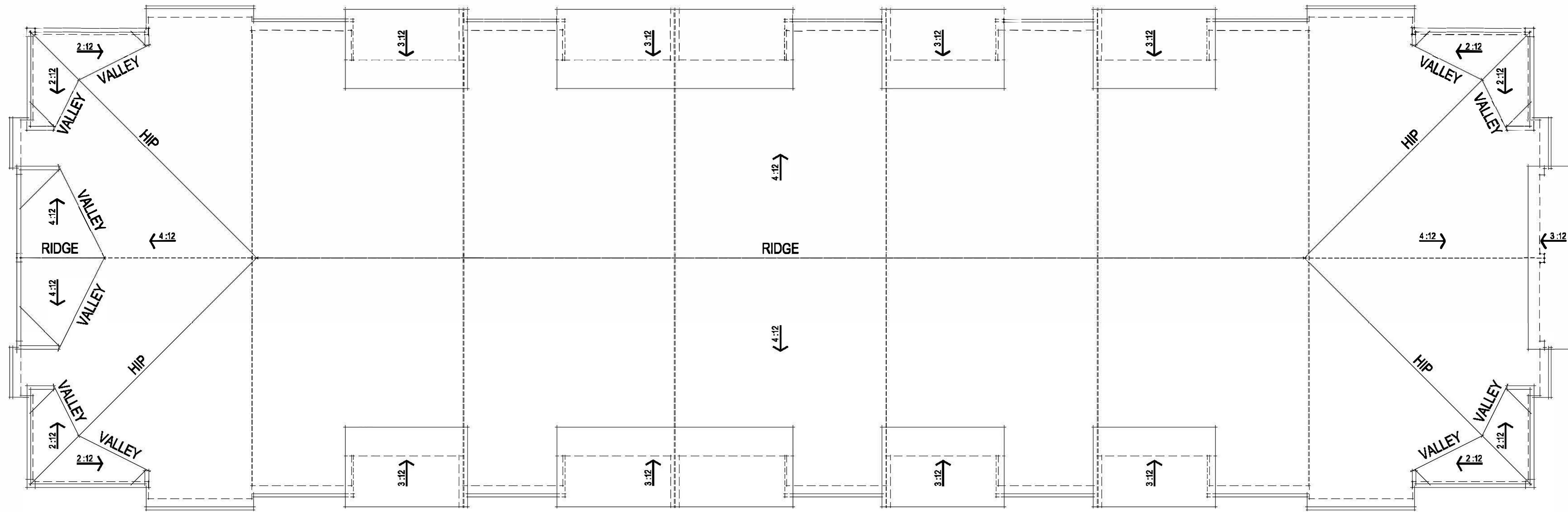


14 UNIT B2B BLDG FIRST & SECOND FLOOR PLANS  
A018

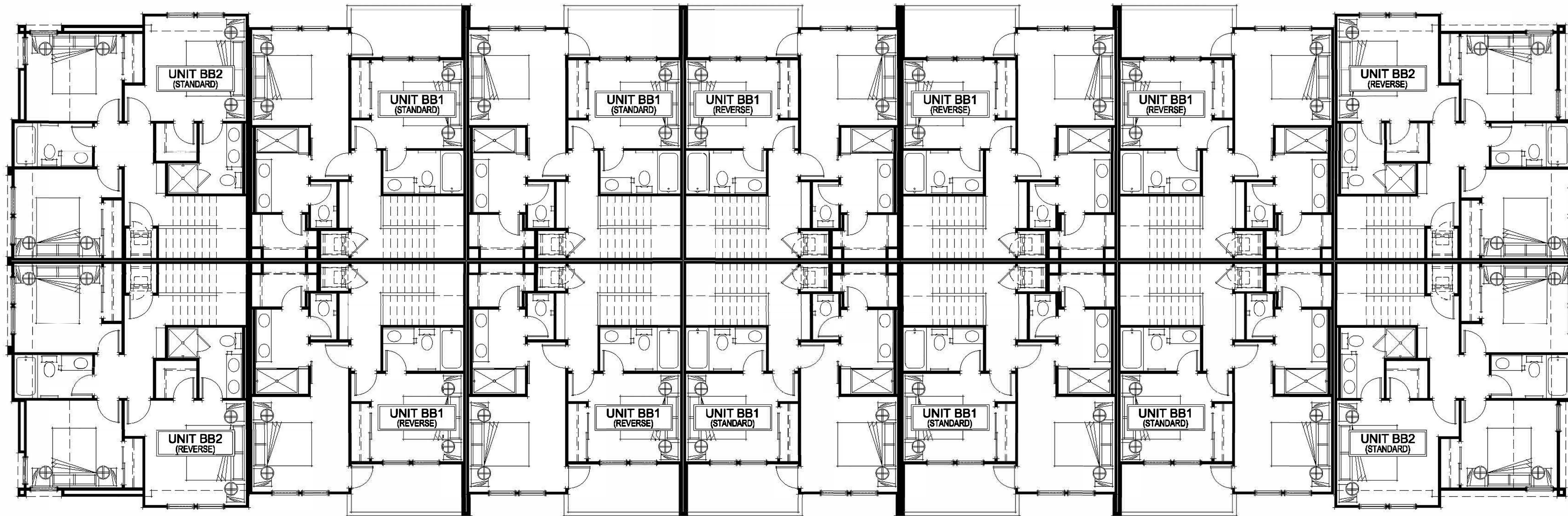
BACK TO BACK TOWNHOMES

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

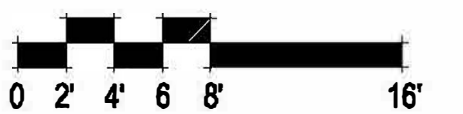




ROOF PLAN



THIRD FLOOR PLAN



14 UNIT B2B BLDG THIRD FLOOR & ROOF PLANS  
A019

BACK TO BACK TOWNHOMES

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024





LIGHTING PER  
PHOTOMETRICS

REAR ELEVATION



LEFT ELEVATION



RIGHT ELEVATION

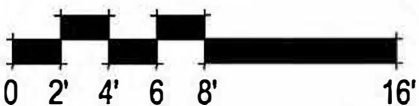


COMP. SHINGLE  
ROOFING  
STUCCO FINISH

HARDI LAP SIDING

STUCCO FINISH

FRONT ELEVATION

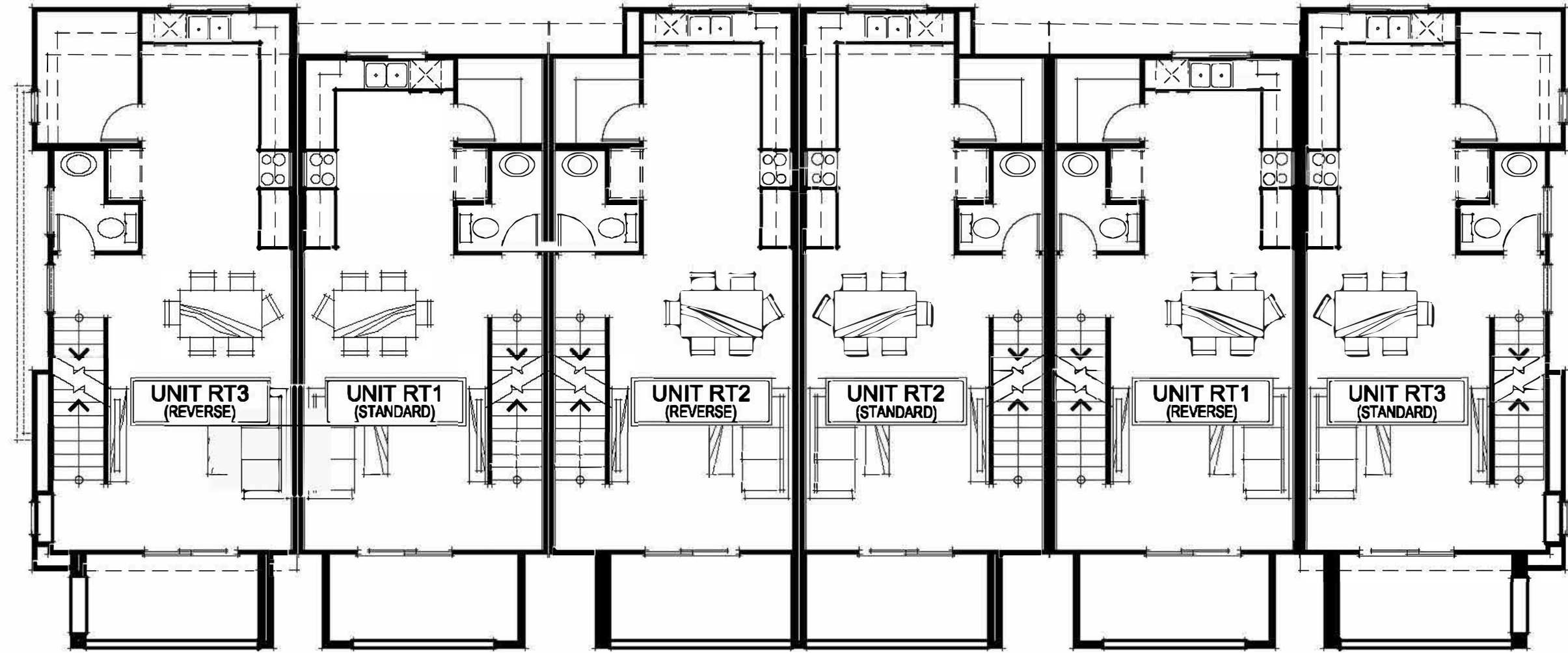


14 UNIT B2B BLDG ELEVATIONS  
A020

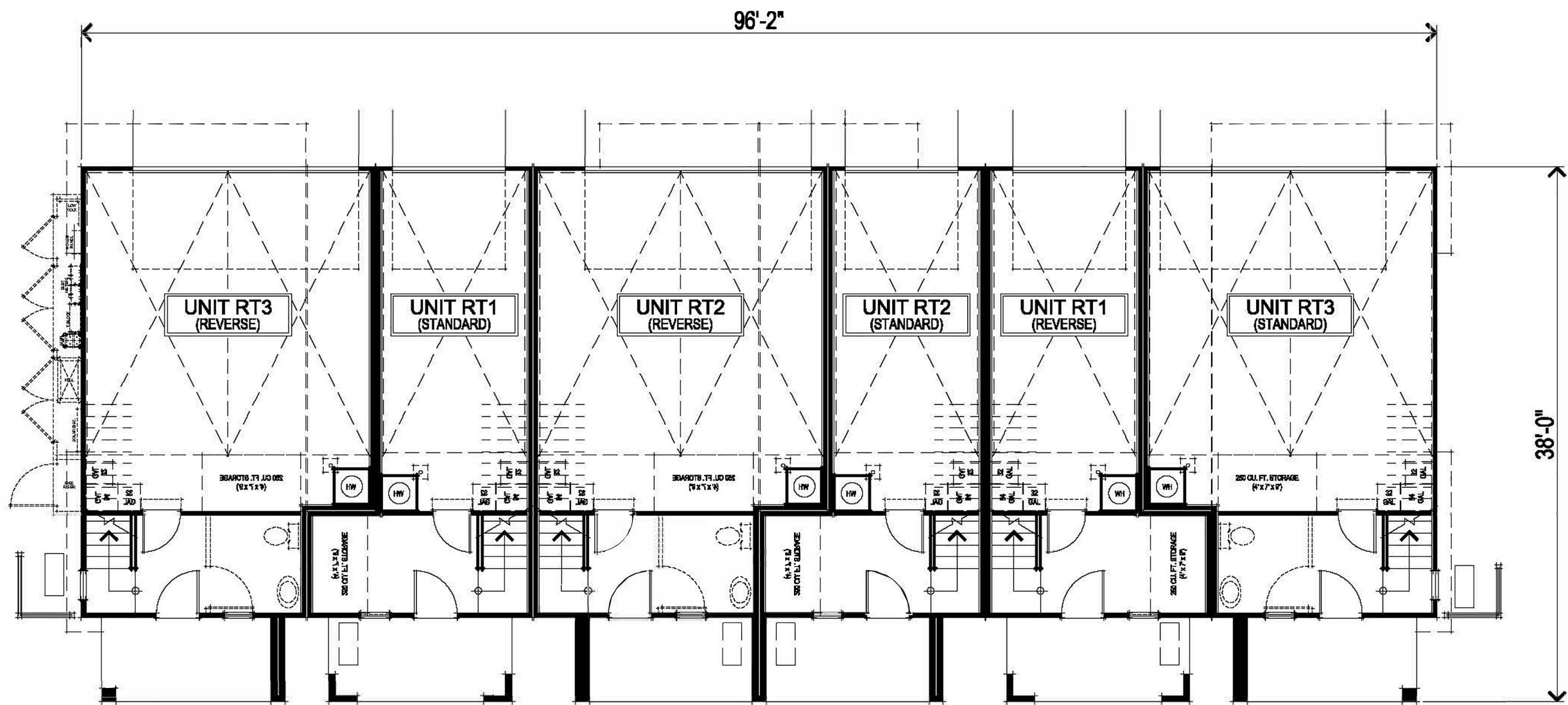
BACK TO BACK TOWNHOMES

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024



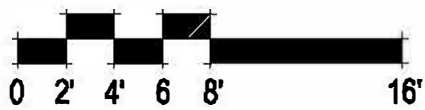


SECOND FLOOR PLAN



FIRST FLOOR PLAN

ROW TOWNHOMES



6 UNIT RT BLDG FIRST & SECOND FLOOR PLANS  
A021

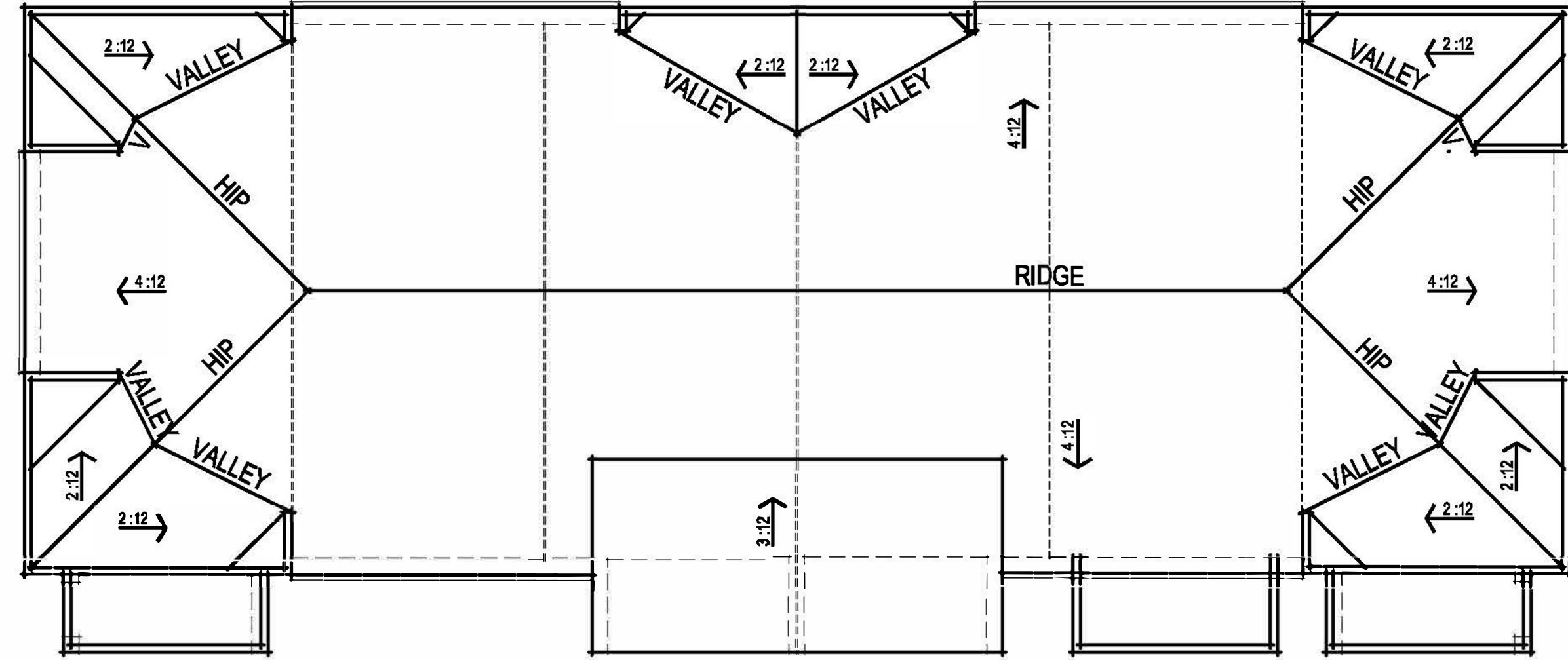
307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

**dh** DENOVA HOMES  
1500 Willow Pass Ct., Concord, CA 94520  
925.685.0110

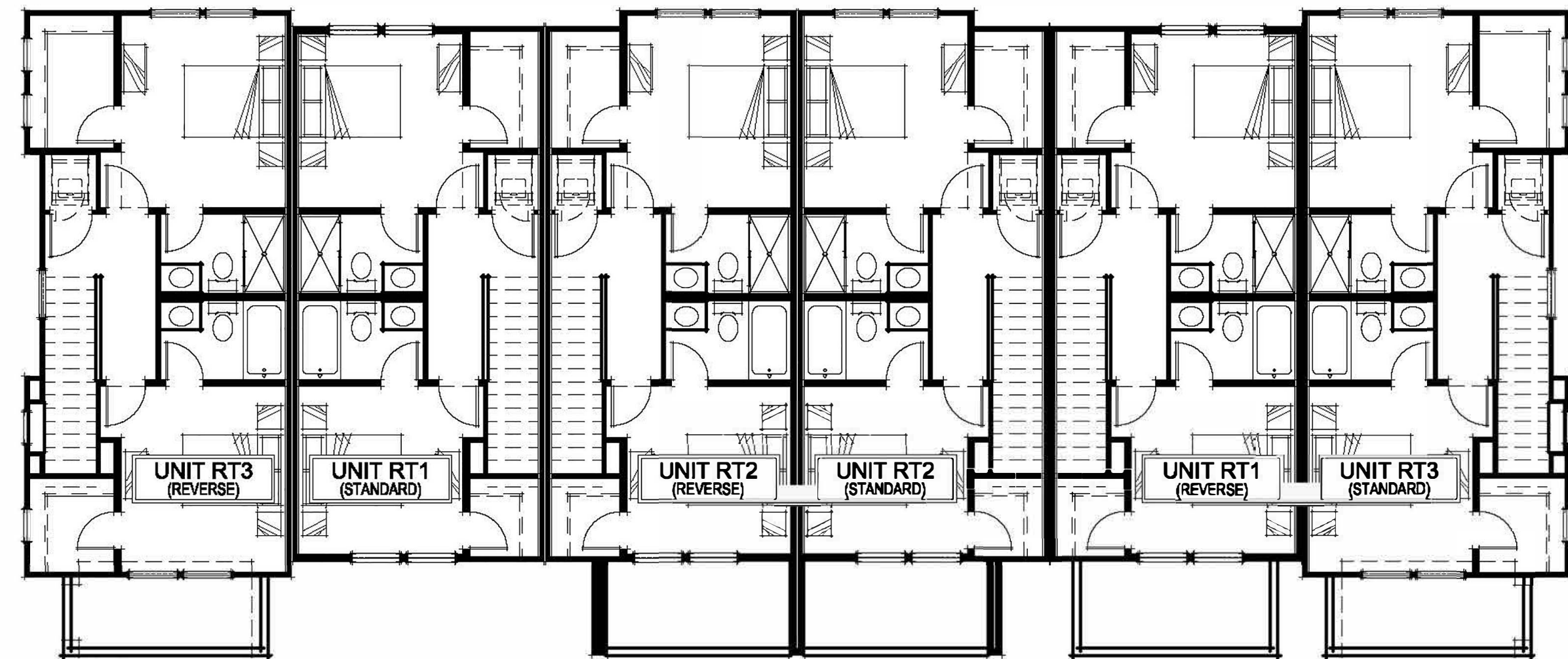
SDG Architects, Inc.  
3361 Walnut Blvd. Suite 120  
Brentwood, CA 94513  
925.634.7000 | sdgarchitectsinc.com





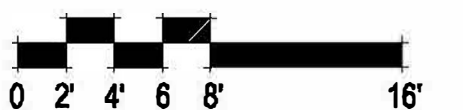


ROOF PLAN



THIRD FLOOR PLAN

ROW TOWNHOMES



6 UNIT RT BLDG THIRD FLOOR & ROOF PLANS  
A022





LIGHTING PER  
PHOTOMETRICS

REAR ELEVATION



LEFT ELEVATION



RIGHT ELEVATION



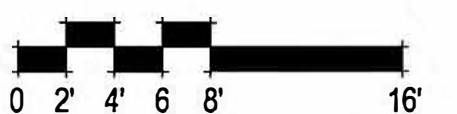
COMP. SHINGLE  
ROOFING

HARDI LAP SIDING

STUCCO FINISH

FRONT ELEVATION

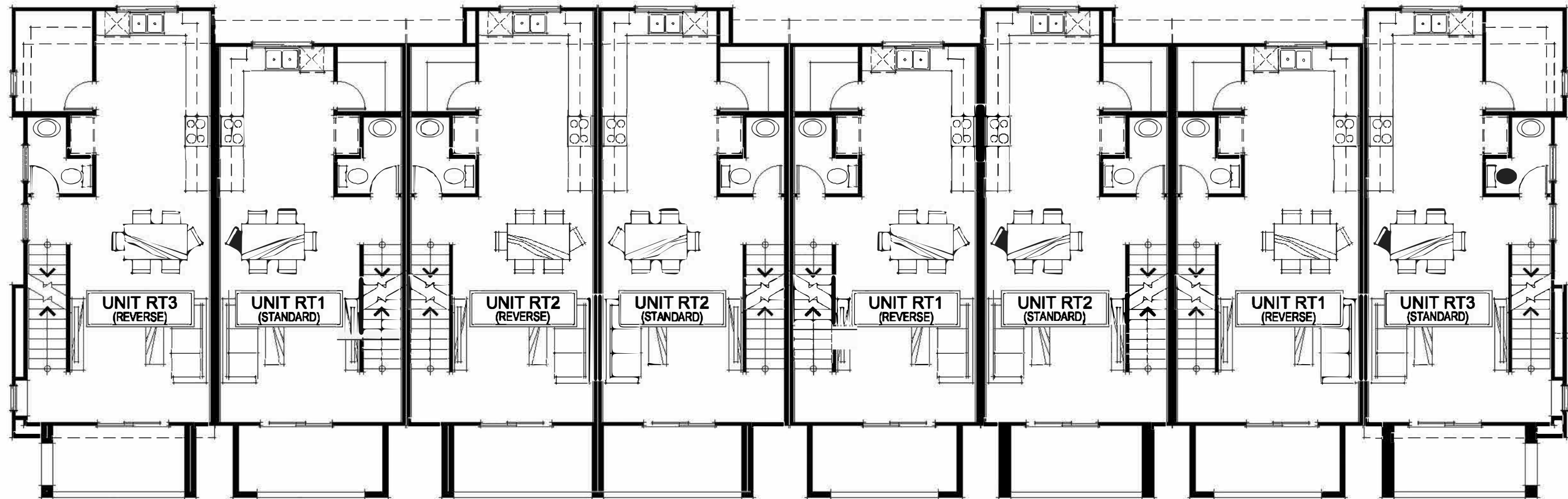
ROW TOWNHOMES



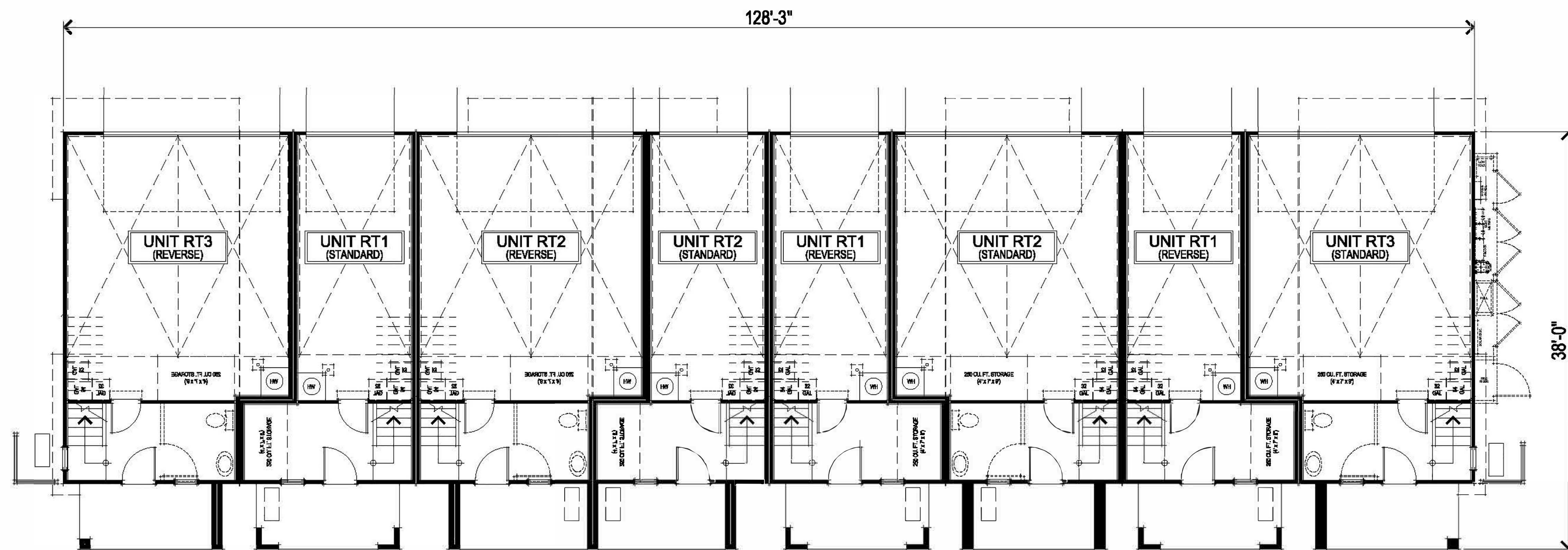
6 UNIT RT BLDG ELEVATIONS  
A023

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

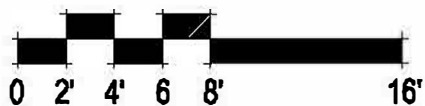




SECOND FLOOR PLAN



FIRST FLOOR PLAN

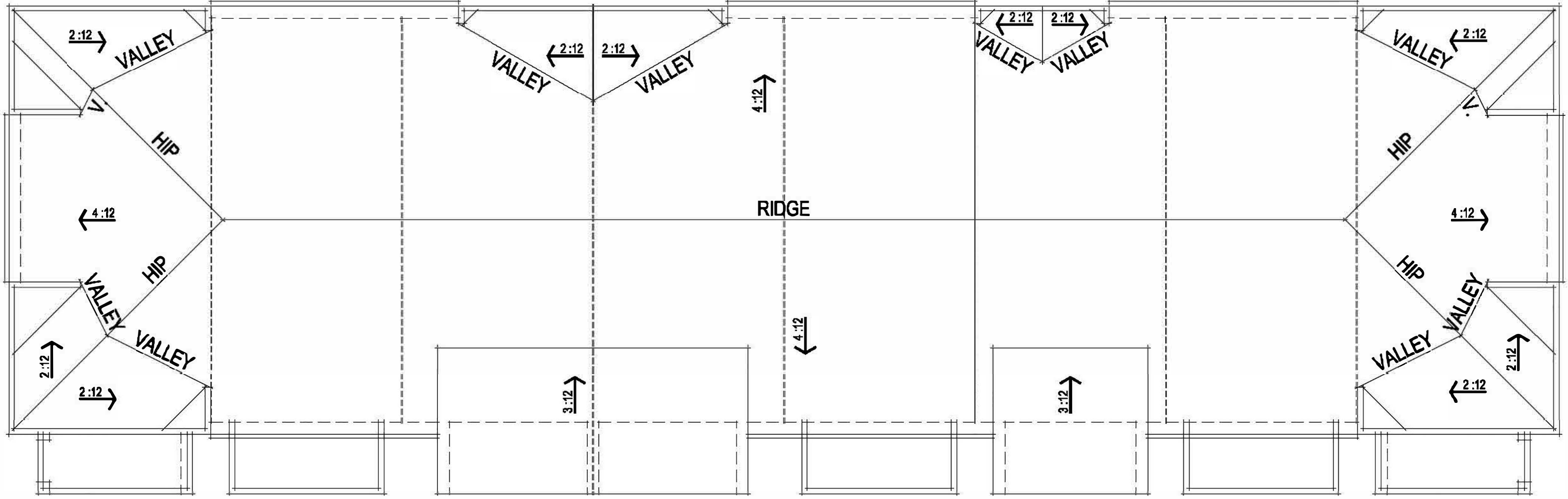


8 UNIT RT BLDG FIRST & SECOND FLOOR PLANS  
A024

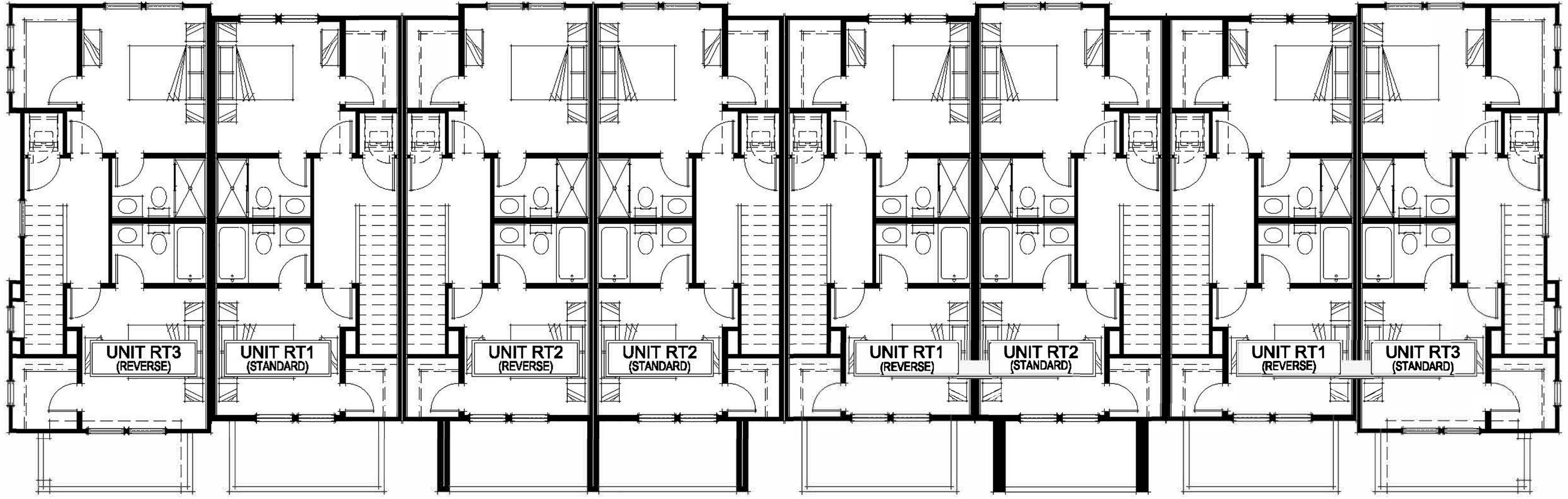
307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

ROW TOWNHOMES





ROOF PLAN



THIRD FLOOR PLAN

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

**dh** DENOVA HOMES  
1500 Willow Pass Ct., Concord, CA 94520  
925.685.0110

ROW TOWNHOMES

0 2' 4' 6' 8' 16'

8 UNIT RT BLDG THIRD FLOOR & ROOF PLANS  
A025

SDG Architects, Inc.  
3361 Walnut Blvd. Suite 120  
Brentwood, CA 94513  
925.634.7000 | sdgarchitectsinc.com







LIGHTING PER  
PHOTOMETRICS

REAR ELEVATION



LEFT ELEVATION



RIGHT ELEVATION

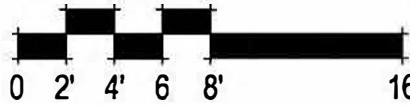


COMP. SHINGLE  
ROOFING

HARDI LAP SIDING

STUCCO FINISH

FRONT ELEVATION



8 UNIT RT BLDG ELEVATIONS  
A026

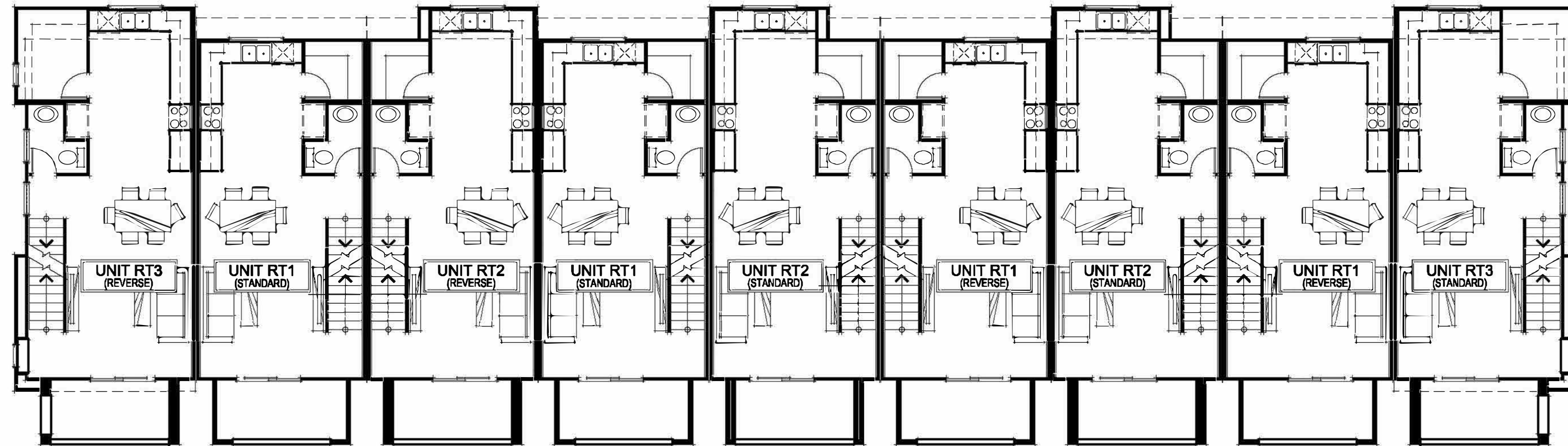
307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

**DeNOVA HOMES**  
1500 Willow Pass Ct., Concord, CA 94520  
925.685.0110

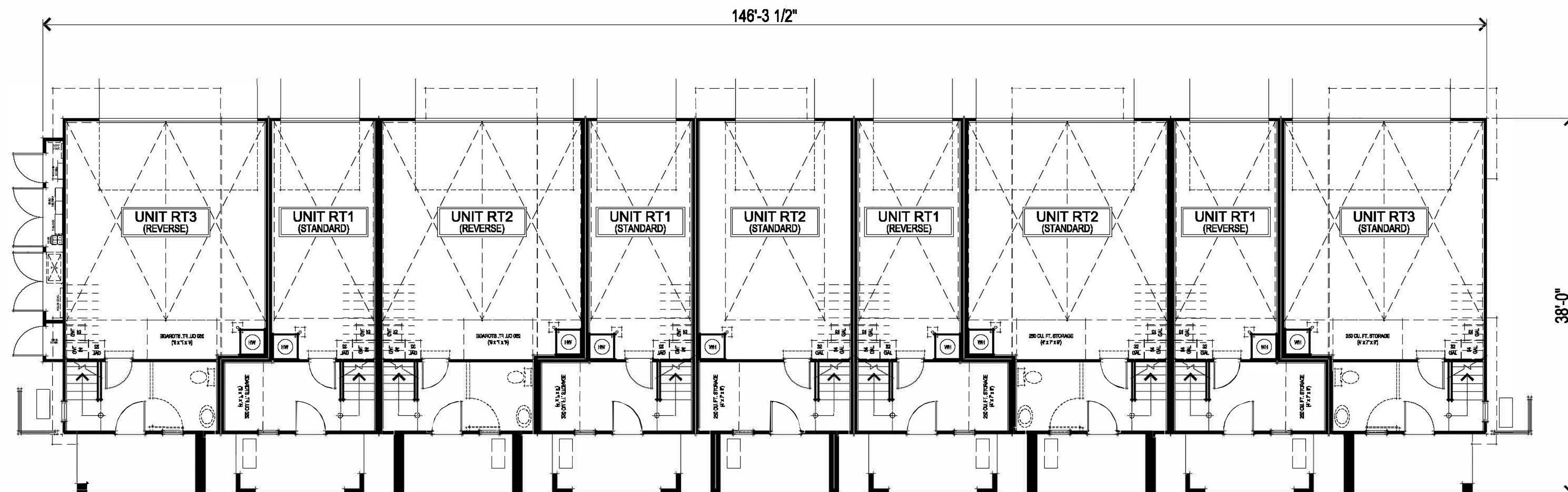
SDG Architects, Inc.  
3361 Walnut Blvd. Suite 120  
Brentwood, CA 94513  
925.634.7000 | sdgarchitectsinc.com



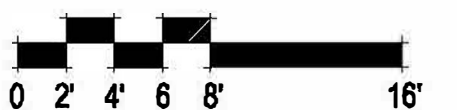




SECOND FLOOR PLAN



FIRST FLOOR PLAN



307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

**DENOVA HOMES**  
1500 Willow Pass Ct., Concord, CA 94520  
925.685.0110

ROW TOWNHOMES

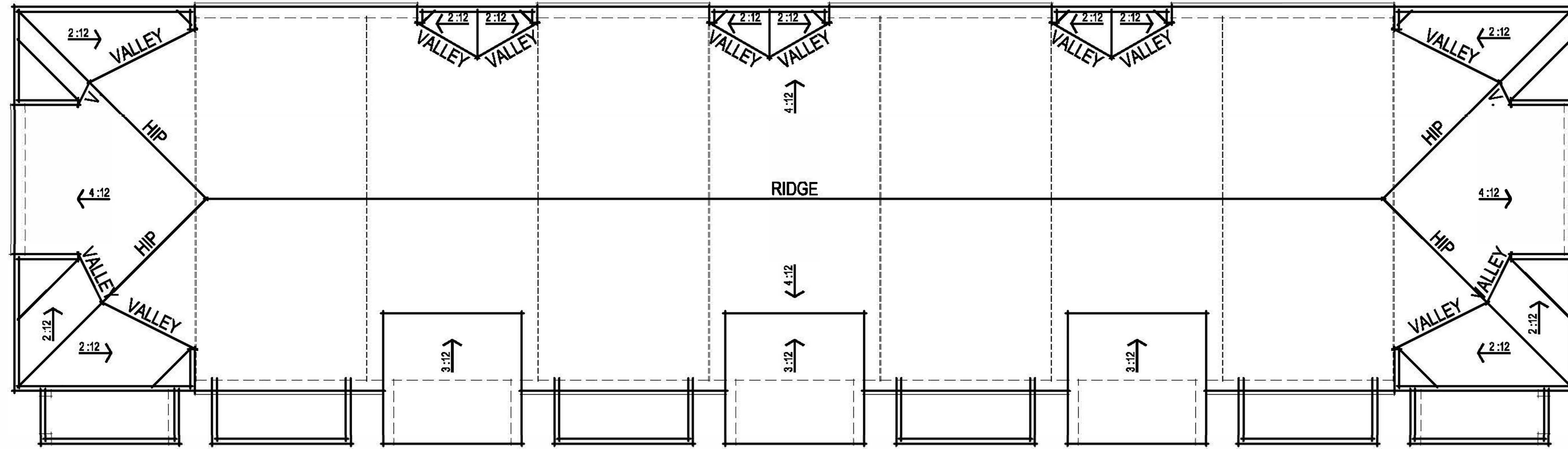
9 UNIT RT BLDG FIRST & SECOND FLOOR PLANS  
A027

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3361 Walnut Blvd. Suite 120  
Brentwood, CA 94513  
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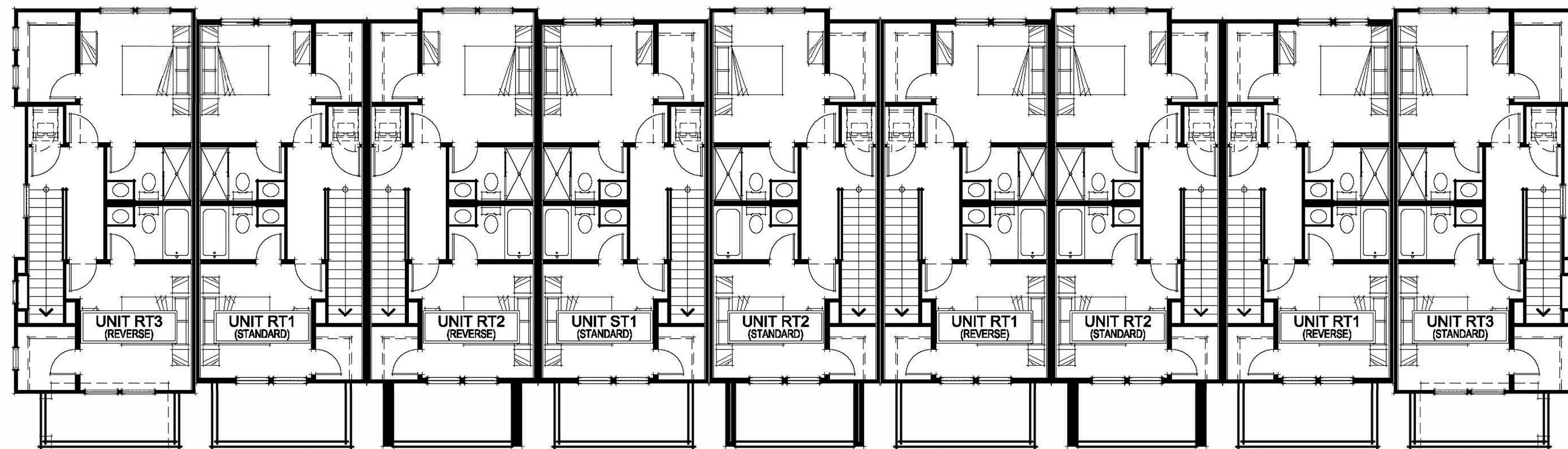


A85

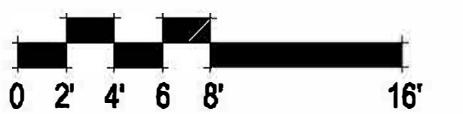




SECOND FLOOR PLAN



FIRST FLOOR PLAN



307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

**DENOVA HOMES**  
1500 Willow Pass Ct., Concord, CA 94520  
925.685.0110

ROW TOWNHOMES

9 UNIT RT BLDG THIRD FLOOR & ROOF PLANS  
A028

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3361 Walnut Blvd. Suite 120  
Brentwood, CA 94513  
925.634.7000 | sdgarchitectsinc.com



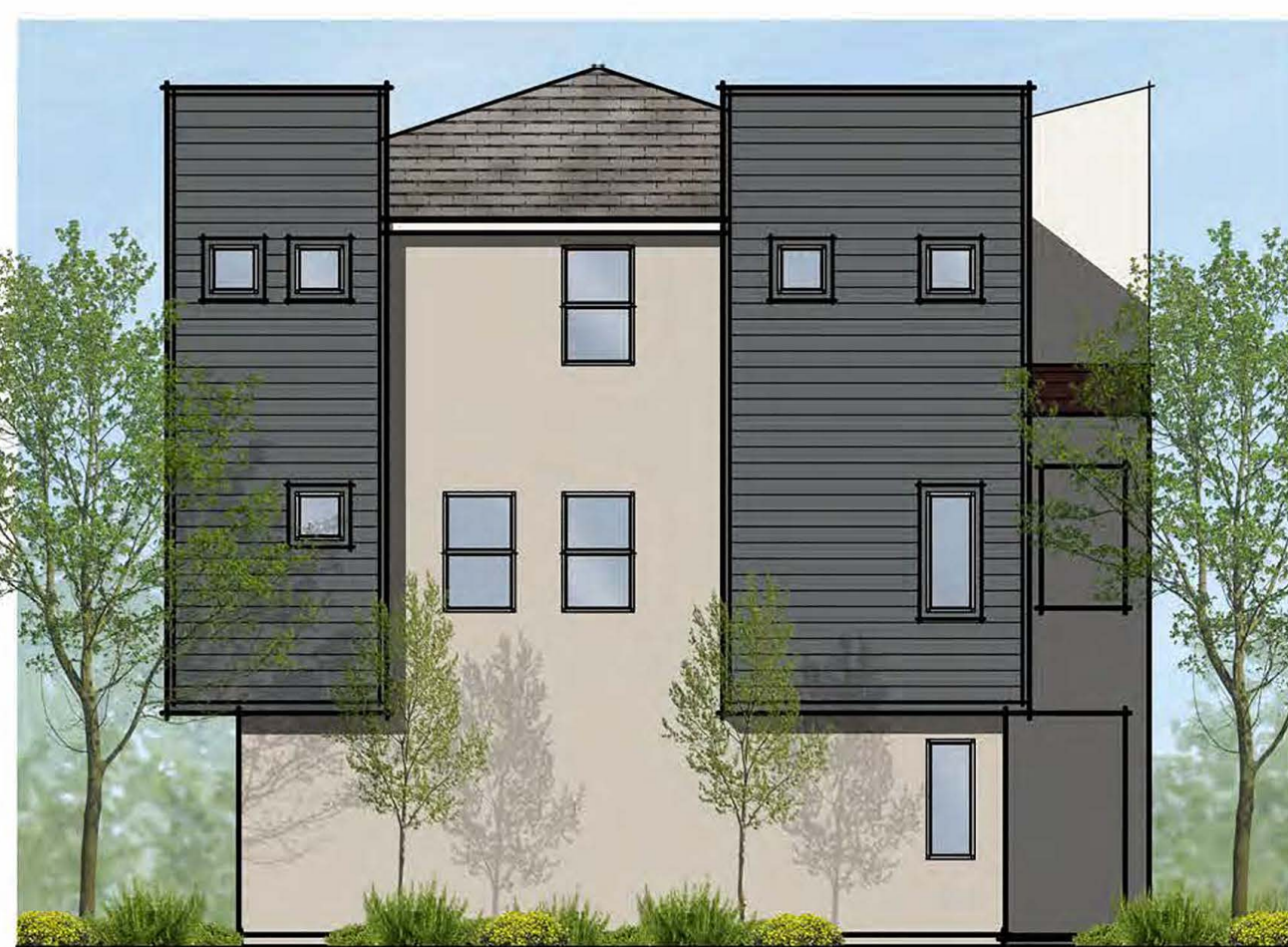
A86





LIGHTING PER  
PHOTOMETRICS

REAR ELEVATION



LEFT ELEVATION



RIGHT ELEVATION



COMP. SHINGLE  
ROOFING

HARDI LAP SIDING

STUCCO FINISH

FRONT ELEVATION



9 UNIT RT BLDG ELEVATIONS  
A029

307.071 Wildflower Townhomes  
Antioch, CA  
February 02, 2024

**DeNOVA HOMES**  
1500 Willow Pass Ct., Concord, CA 94520  
925.685.0110

ROW TOWNHOMES

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**A87**



COLOR SCHEME 1



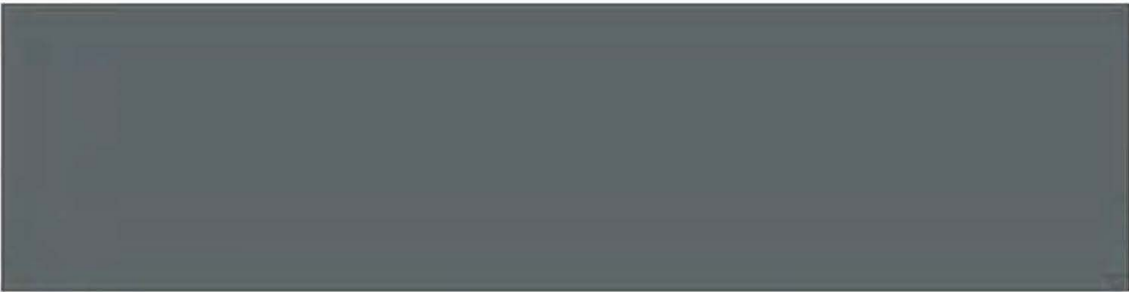
Roofing  
CertainTeed - Weathered Wood



Stucco 1  
SW 7632 Modern Gray (283-C1)



Stucco 2  
SW 7008 Alabaster (255-C2)



Siding 1  
SW 7075 Web Grey (235-C5)



Siding 2  
SW 7593 Rustic Red (275-C6)



Entry Door  
SW 7069 Iron Ore (251-C7)



Accent Stucco  
SW 7026 Griffin (241-C6)



Garage Door  
SW 7026 Griffin (241-C6)

*Note: All colors and textures are representative samples only, pending verification of actual material suppliers and manufacturers for this particular project.*

COLOR SCHEME 2



Roofing  
CertainTeed - Weathered Wood



Stucco 1  
SW 7632 Modern Gray (283-C1)



Stucco 2  
SW 7008 Alabaster (255-C2)



Siding 1  
SW 7075 Web Grey (235-C5)



Siding 2  
SW 7728 Green Sprout (298-C4)



Entry Door / Accent  
SW 7069 Iron Ore (251-C7)

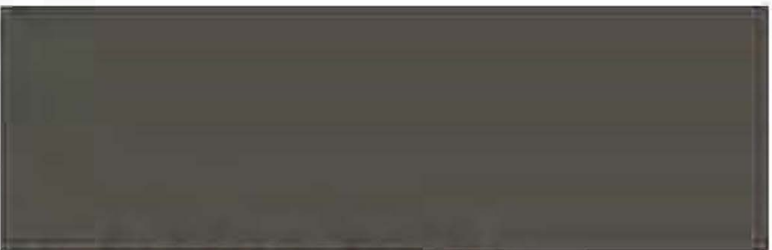


Accent Stucco  
SW 6400 Lucent Yellow (143-C1)



Garage Door  
SW 7026 Griffin (241-C6)



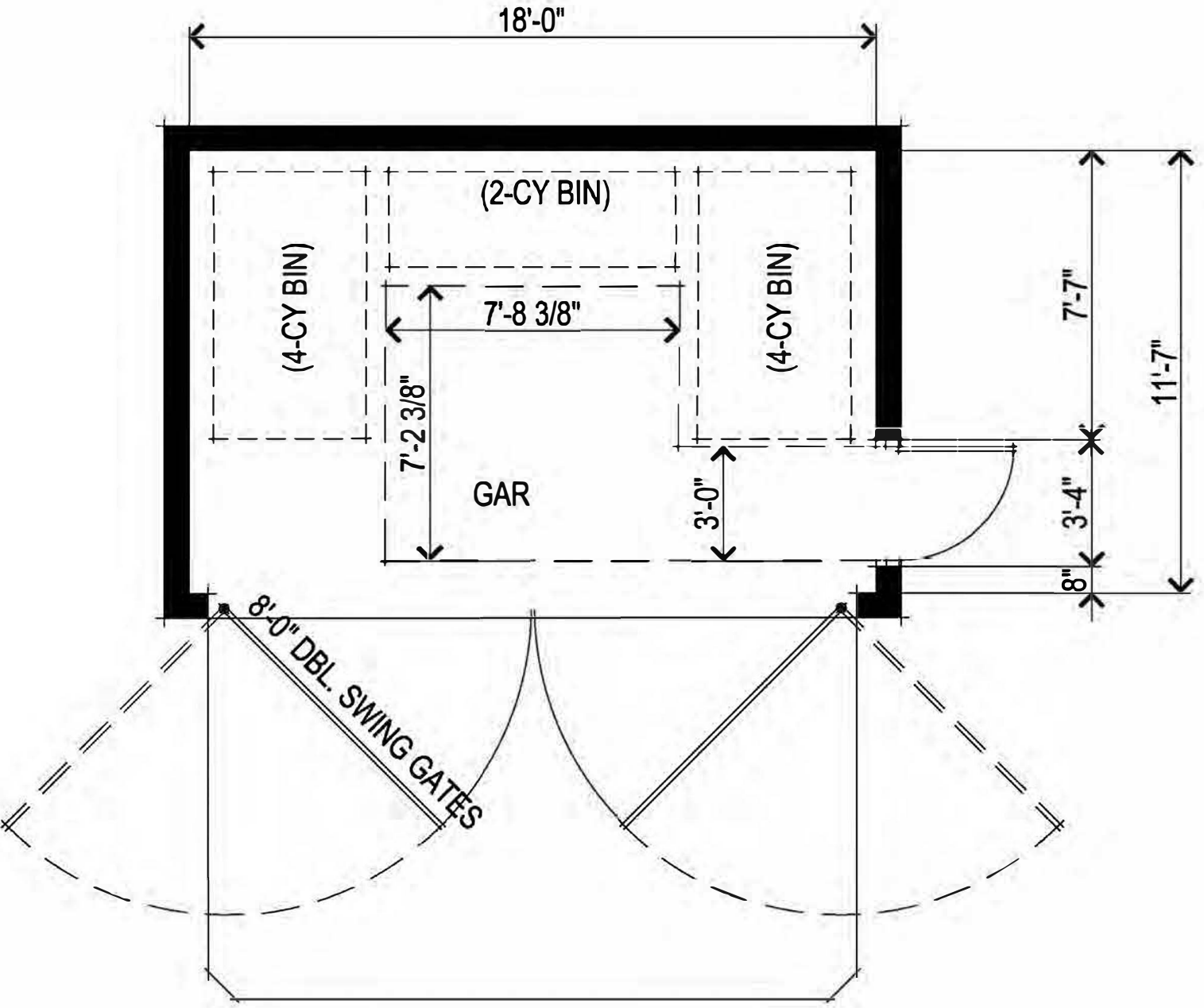


SW 7048 URBANE BRONZE

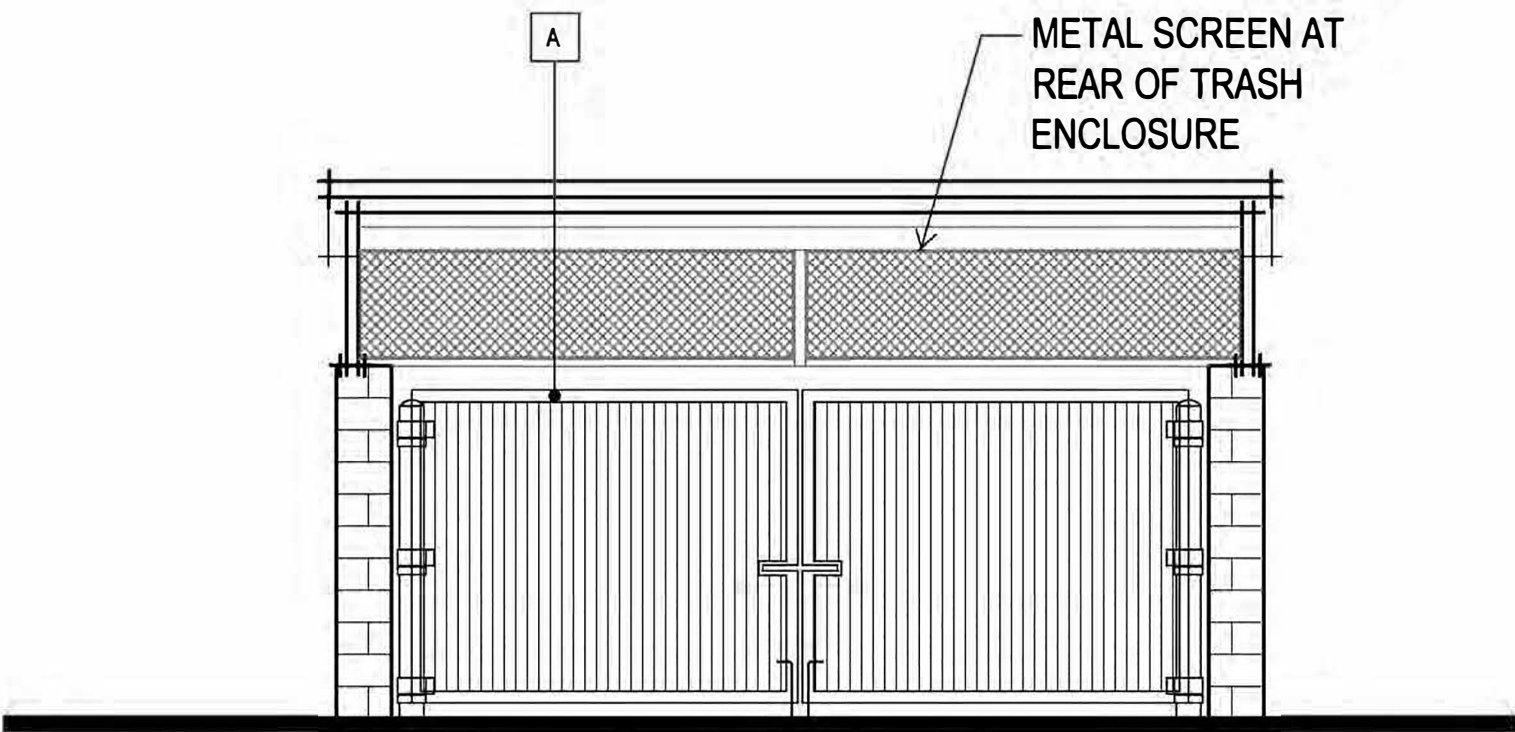


CALSTONE - AB CLASSIC STONE: TAN

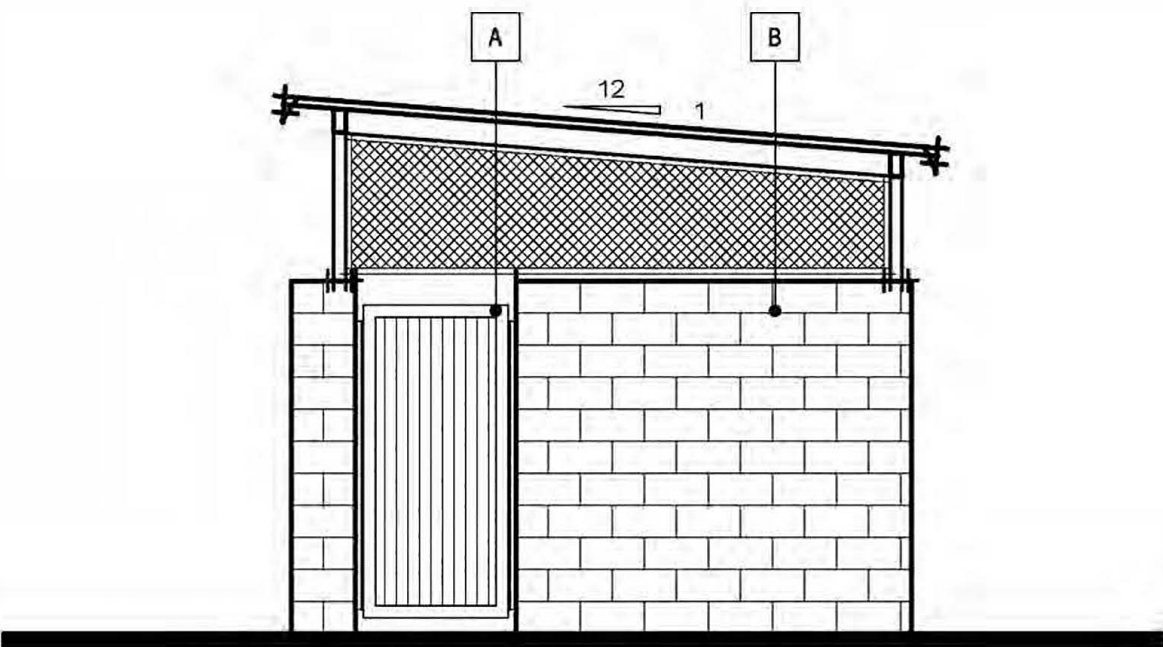
EXTERIOR FINISH KEY NOTES	
A	SHERWIN-WILLIAMS: 7048 URBANE BRONZE
B	CALSTONE-AB CLASSIC STONE: TAN



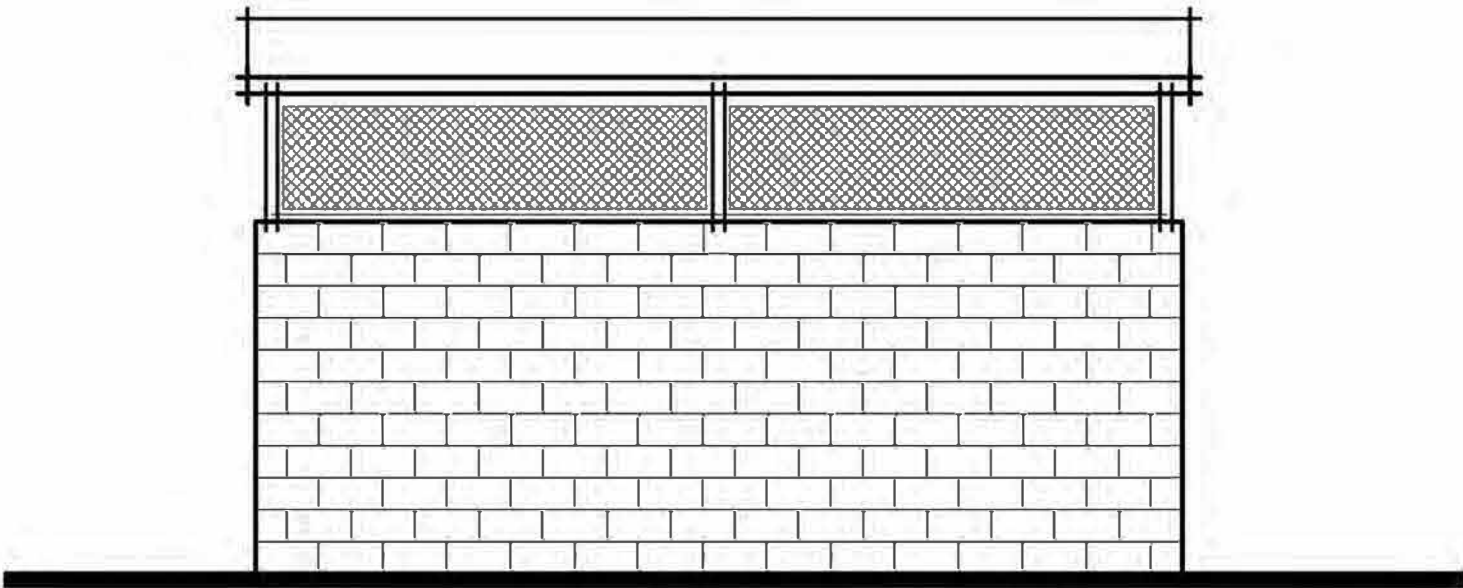
TRASH ENCLOSURE  
FLOOR PLAN



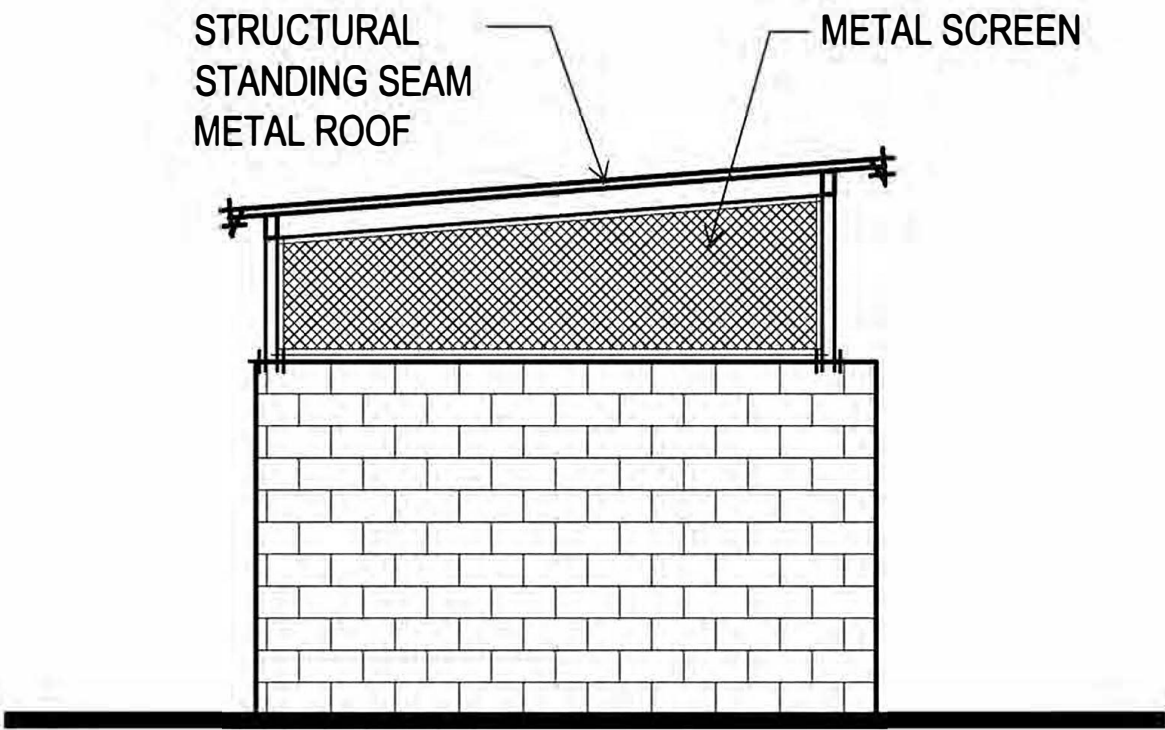
TYPICAL  
FRONT ELEVATION



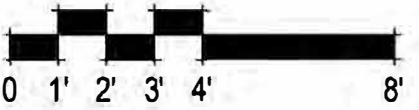
TYPICAL  
RIGHT ELEVATION



TYPICAL  
REAR ELEVATION



TYPICAL  
LEFT ELEVATION





**VWM** LED  
Voltaire Mini Architectural Wall Pack

**FIXTURE DETAILS**  
MOUNTING DETAILS  
BOLT PATTERN DETAIL  
VWMH

**PC OPTION**  
Factory-installed button-style photocell, left side when viewed from behind fixture.  
(120V, 208V, or 277V only; must specify voltage)

**VWMH**  
Weight: 6.5 lbs

**VWMV**  
Weight: 6.5 lbs

**FINISH OPTIONS**

WHITE	BLACK	GREEN	MEDIUM BRONZE	DARK BRONZE	SATIN ALUMINUM GRAY

For custom color, please specify RAL color or a manufacturer code with description. All custom colors other than RAL require two sample swatches, minimum 1" square.

H.E. Williams, Inc. • Carthage, Missouri • www.hew.com • 417-358-4005 • Designed and Manufactured in the USA  
Information contained herein is subject to change without notice. Wall Mount Page 1 of 3

**VWM** LED  
Voltaire Mini Architectural Wall Pack

**FEATURES**

- Designed to illuminate sidewalks, courtyards, patios, porches, or facades.
- Intended for use in both uplight and downlight applications.
- Savings of up to 80% energy compared to HID systems.
- Blends seamlessly with a variety of architectural styles.
- Available on QuickShip.
- Meets Right to Buy in the USA.

**ORDERING EXAMPLE** VWM H - L20/240 - T3 - DBZ - SDGL - OPTIONS - DIM - UNV

**ORDERING INFO**

SERIES	TYPE <sup>(1)</sup>	LUMENS <sup>(2)</sup>	CR/CCCT	DISTRIBUTION <sup>(3)</sup>
VWMH	H Horizontal	70 CRI 120 1,700lm 125 2,000lm 130 2,000lm 135 2,000lm	70 CRI 750 3000K 750 4000K 750 5000K 800K	TL Laminar distribution <sup>(1)</sup> T3 Type II <sup>(1)</sup>

**FINISH OPTIONS<sup>(1)</sup>**

BLK	Black (RAL #9004)
DBZ <td>Dark bronze</td>	Dark bronze
DBR <td>Medium bronze</td>	Medium bronze
DBG <td>Dark gray</td>	Dark gray
DBH <td>Medium gray</td>	Medium gray
DBI <td>Light gray</td>	Light gray
DBJ <td>White (RAL #9003)</td>	White (RAL #9003)

**SHIELDING**

SDGL	Micro-prismatic tempered glass lens
SDL <th>Clear tempered glass lens</th>	Clear tempered glass lens

**OPTIONS**

PC	Button-style photocell <sup>(1)</sup>
SP10 <th>10A/10V surge protection</th>	10A/10V surge protection
TP <th>Temper-resistant top head screws<sup>(1)</sup></th>	Temper-resistant top head screws <sup>(1)</sup>

**CONTROL**

DIM	Dimming driver provided for 0-10V low voltage applications
VOLTAGE	120 120V 208 208V 277 277V UNV 120-277V

**ACCESSORIES**

TPX-25 TOOL	Temper-resistant tool for Torx head screws <sup>(1)</sup>
VWMH-L20/240-TL-DBZ-COL-OS-DIM-UNV	
VWMH-L20/240-TL-DBZ-COL-OS-DIM-UNV	

**QUICKSHIP**

VWMH-L20/240-TL-DBZ-COL-OS-DIM-UNV

**NOTES**

- See page 3 for FIXTURE DETAILS.
- Lumen output based on 5000 CCT. Actual lumens may vary.
- CR/CCCT, availability & 277V/100V/120V/208V/240V/277V/480V/500V/575V/600V/660V/720V/780V/850V/900V/960V/1000V/1050V/1100V/1150V/1200V/1250V/1300V/1350V/1400V/1450V/1500V/1550V/1600V/1650V/1700V/1750V/1800V/1850V/1900V/1950V/2000V/2050V/2100V/2150V/2200V/2250V/2300V/2350V/2400V/2450V/2500V/2550V/2600V/2650V/2700V/2750V/2800V/2850V/2900V/2950V/3000V/3050V/3100V/3150V/3200V/3250V/3300V/3350V/3400V/3450V/3500V/3550V/3600V/3650V/3700V/3750V/3800V/3850V/3900V/3950V/4000V/4050V/4100V/4150V/4200V/4250V/4300V/4350V/4400V/4450V/4500V/4550V/4600V/4650V/4700V/4750V/4800V/4850V/4900V/4950V/5000V/5050V/5100V/5150V/5200V/5250V/5300V/5350V/5400V/5450V/5500V/5550V/5600V/5650V/5700V/5750V/5800V/5850V/5900V/5950V/6000V/6050V/6100V/6150V/6200V/6250V/6300V/6350V/6400V/6450V/6500V/6550V/6600V/6650V/6700V/6750V/6800V/6850V/6900V/6950V/7000V/7050V/7100V/7150V/7200V/7250V/7300V/7350V/7400V/7450V/7500V/7550V/7600V/7650V/7700V/7750V/7800V/7850V/7900V/7950V/8000V/8050V/8100V/8150V/8200V/8250V/8300V/8350V/8400V/8450V/8500V/8550V/8600V/8650V/8700V/8750V/8800V/8850V/8900V/8950V/9000V/9050V/9100V/9150V/9200V/9250V/9300V/9350V/9400V/9450V/9500V/9550V/9600V/9650V/9700V/9750V/9800V/9850V/9900V/9950V/10000V/10050V/10100V/10150V/10200V/10250V/10300V/10350V/10400V/10450V/10500V/10550V/10600V/10650V/10700V/10750V/10800V/10850V/10900V/10950V/11000V/11050V/11100V/11150V/11200V/11250V/11300V/11350V/11400V/11450V/11500V/11550V/11600V/11650V/11700V/11750V/11800V/11850V/11900V/11950V/12000V/12050V/12100V/12150V/12200V/12250V/12300V/12350V/12400V/12450V/12500V/12550V/12600V/12650V/12700V/12750V/12800V/12850V/12900V/12950V/13000V/13050V/13100V/13150V/13200V/13250V/13300V/13350V/13400V/13450V/13500V/13550V/13600V/13650V/13700V/13750V/13800V/13850V/13900V/13950V/14000V/14050V/14100V/14150V/14200V/14250V/14300V/14350V/14400V/14450V/14500V/14550V/14600V/14650V/14700V/14750V/14800V/14850V/14900V/14950V/15000V/15050V/15100V/15150V/15200V/15250V/15300V/15350V/15400V/15450V/15500V/15550V/15600V/15650V/15700V/15750V/15800V/15850V/15900V/15950V/16000V/16050V/16100V/16150V/16200V/16250V/16300V/16350V/16400V/16450V/16500V/16550V/16600V/16650V/16700V/16750V/16800V/16850V/16900V/16950V/17000V/17050V/17100V/17150V/17200V/17250V/17300V/17350V/17400V/17450V/17500V/17550V/17600V/17650V/17700V/17750V/17800V/17850V/17900V/17950V/18000V/18050V/18100V/18150V/18200V/18250V/18300V/18350V/18400V/18450V/18500V/18550V/18600V/18650V/18700V/18750V/18800V/18850V/18900V/18950V/19000V/19050V/19100V/19150V/19200V/19250V/19300V/19350V/19400V/19450V/19500V/19550V/19600V/19650V/19700V/19750V/19800V/19850V/19900V/19950V/20000V/20050V/20100V/20150V/20200V/20250V/20300V/20350V/20400V/20450V/20500V/20550V/20600V/20650V/20700V/20750V/20800V/20850V/20900V/20950V/21000V/21050V/21100V/21150V/21200V/21250V/21300V/21350V/21400V/21450V/21500V/21550V/21600V/21650V/21700V/21750V/21800V/21850V/21900V/21950V/22000V/22050V/22100V/22150V/22200V/22250V/22300V/22350V/22400V/22450V/22500V/22550V/22600V/22650V/22700V/22750V/22800V/22850V/22900V/22950V/23000V/23050V/23100V/23150V/23200V/23250V/23300V/23350V/23400V/23450V/23500V/23550V/23600V/23650V/23700V/23750V/23800V/23850V/23900V/23950V/24000V/24050V/24100V/24150V/24200V/24250V/24300V/24350V/24400V/24450V/24500V/24550V/24600V/24650V/24700V/24750V/24800V/24850V/24900V/24950V/25000V/25050V/25100V/25150V/25200V/25250V/25300V/25350V/25400V/25450V/25500V/25550V/25600V/25650V/25700V/25750V/25800V/25850V/25900V/25950V/26000V/26050V/26100V/26150V/26200V/26250V/26300V/26350V/26400V/26450V/26500V/26550V/26600V/26650V/26700V/26750V/26800V/26850V/26900V/26950V/27000V/27050V/27100V/27150V/27200V/27250V/27300V/27350V/27400V/27450V/27500V/27550V/27600V/27650V/27700V/27750V/27800V/27850V/27900V/27950V/28000V/28050V/28100V/28150V/28200V/28250V/28300V/28350V/28400V/28450V/28500V/28550V/28600V/28650V/28700V/28750V/28800V/28850V/28900V/28950V/29000V/29050V/29100V/29150V/29200V/29250V/29300V/29350V/29400V/29450V/29500V/29550V/29600V/29650V/29700V/29750V/29800V/29850V/29900V/29950V/30000V/30050V/30100V/30150V/30200V/30250V/30300V/30350V/30400V/30450V/30500V/30550V/30600V/30650V/30700V/30750V/30800V/30850V/30900V/30950V/31000V/31050V/31100V/31150V/31200V/31250V/31300V/31350V/31400V/31450V/31500V/31550V/31600V/31650V/31700V/31750V/31800V/31850V/31900V/31950V/32000V/32050V/32100V/32150V/32200V/32250V/32300V/32350V/32400V/32450V/32500V/32550V/32600V/32650V/32700V/32750V/32800V/32850V/32900V/32950V/33000V/33050V/33100V/33150V/33200V/33250V/33300V/33350V/33400V/33450V/33500V/33550V/33600V/33650V/33700V/33750V/33800V/33850V/33900V/33950V/34000V/34050V/34100V/34150V/34200V/34250V/34300V/34350V/34400V/34450V/34500V/34550V/34600V/34650V/34700V/34750V/34800V/34850V/34900V/34950V/35000V/35050V/35100V/35150V/35200V/35250V/35300V/35350V/35400V/35450V/35500V/35550V/35600V/35650V/35700V/35750V/35800V/35850V/35900V/35950V/36000V/36050V/36100V/36150V/36200V/36250V/36300V/36350V/36400V/36450V/36500V/36550V/36600V/36650V/36700V/36750V/36800V/36850V/36900V/36950V/37000V/37050V/37100V/37150V/37200V/37250V/37300V/37350V/3740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**EXHIBIT C**  
**CONDITIONS OF APPROVAL**  
**WILDFLOWER STATION TOWNHOMES 2 - TM-02 | AR-23-05**

**GENERAL CONDITIONS**

1. **Project Approval.** This Vesting Tentative Subdivision Map and Design Review approval is for the Wildflower Station 2 Townhome Subdivision 9601 Project located south of Hillcrest Crossroads and directly east of Hillcrest Avenue (APN: 052-140-013, 052-140-014, 052-140-015, 052-140-016), as substantially shown and described on the project plans, except as required to be modified by conditions. Plans date received February 5, 2024, and Civil plans dated received June 20, 2025 as presented to the Planning Commission on July 16, 2025 ("Approval Date."). For any condition herein that requires preparation of a Final Plan where the project applicant has submitted a conceptual plan, the project applicant shall submit final plan(s) in substantial conformance with the conceptual plan, but incorporate the modifications required by the conditions herein for approval by the City.
2. **Project Approval Expiration.** This approval expires on July 16, 2027 (two years from the date on which this approval becomes effective) or at an alternate time specified as a condition of approval unless a building permit has been issued and construction diligently pursued. All approval extensions shall be processed as stated in the Antioch Municipal Code.
3. **Appeals.** Pursuant to Antioch Municipal Code Section 9-5.2509, any decision made by the Planning Commission which would otherwise constitute final approval or denial may be appealed to the City Council. Such appeal shall be in writing and shall be filed with the City Clerk within five (5) working days after the decision. All appeals to the City Council from the Planning Commission shall be accompanied by a filing fee established by a resolution of the City Clerk.
4. **Requirement for Building Permit.** Approval granted by the Planning Commission does not constitute a building permit or authorization to begin any construction or demolition of an existing structure. An appropriate permit issued by the Community Development Department must be obtained before constructing, enlarging, moving, converting, or demolishing any building or structure within the city.
5. **Modification of Approved Plans.** The project shall be constructed as approved and with any additional changes required pursuant to the Zoning Administrator, Planning Commission, or City Council's Conditions of Approval. Planning staff may approve minor modifications in the project design as outlined in Antioch Municipal Code Section 9-5.2708.
6. **Hold Harmless Agreement/Indemnification.** The applicant (including any agent thereof) shall defend, indemnify, and hold harmless, the City of Antioch and its agents, and employees, from any claim, action, or proceeding against the City or its agents, officers or employees to attack, set aside, void, or annul



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the City's approval concerning this application. The city will promptly notify the applicant of any such claim action or proceeding and cooperate fully in the defense.

7. **Final Approval.** A final and unchallenged approval of this project supersedes previous approvals that have been granted for this site.
8. **Compliance Matrix.** With the submittal of all grading plans, improvement plans, and building permit plans, the applicant shall submit to the Community Development Department a Conditions and Mitigation Measures Compliance Matrix that lists: each Condition of Approval and Mitigation Measure, the City division responsible for review, and how the applicant meets the Condition of Approval or Mitigation Measure. The applicant shall update the compliance matrix and provide it with each submittal.

## **FEES**

9. **City Fees.** The applicant shall pay all City and other related fees applicable to the property, as may be modified by the conditions herein. Fees shall be limited to those applicable when the related SB 330 application was submitted and shall be based on the current fee structure in effect at the time provided that any increase since submittal of the project's SB 330 application reflects only the annual adjustments based on a published cost index. Fees shall be paid before issuance of said permit. Notice shall be taken specifically of Plan Check, Engineering, Fire, and Inspection Fees. The project applicant shall also reimburse the City for direct costs of planning; building and engineering plan check and inspection, as mutually agreed between the City and applicant.

*Neither discretionary nor ministerial permits/approvals will be considered if the developer is not current on fees, balances, and reimbursement that are outstanding and owed to the City.*

10. **Pass-Through Fees.** The developer shall pay all pass-through fees. Fees include but are not limited to:
  - a. East Contra Costa Regional Fee and Financing Authority (ECCRFFA) Fee in effect at the time of building permit issuance.
  - b. Contra Costa County Fire Protection District Fire Development Fee in place at the time of building permit issuance.
  - c. Contra Costa County Map Maintenance Fee in affect at the time of recordation of the final map(s).
  - d. Contra Costa County Flood Control District Drainage Area fee.
  - e. School Impact Fees.
  - f. Delta Diablo Sanitation Sewer Fees.
  - g. Contra Costa Water District Fees.



- 11. Proof of CFD Annexation.** Concurrent with, or prior to, submittal of the Final Map, the developer shall submit evidence of annexation into all required districts, including:
- a. The applicant shall annex into the existing Community Facilities District (CFD) 2018-02 (Police Protection).
  - b. Public Services District (Public Services) CFD 2018-01 and accept a level of annual assessments sufficient to maintain public facilities in the vicinity of the project area at no cost to the City. The annual assessment shall cover the actual annual cost of public services as described in the Engineer's Report.
  - c. Fire Services CFD. The applicant shall annex into CFD 2022-1 (Antioch Fire Protection and Emergency Response Services). This CFD is administered by the Contra Costa County Fire Protection District. To comply with this condition, the applicant must provide the City proof of annexation by furnishing a copy of the resolution passed by the County Board of Supervisors.

### **VESTING TENTATIVE MAP**

- 12. Subdivision Map Act Compliance.** The Vesting Tentative Map approval is subject to the timelines established in the State of California Subdivision Map Act or as extended by a Development Agreement.
- 13. Map Approval.** Map approval is granted based on substantial conformance with the Vesting Tentative Map prepared on February 2, 2024 and June 27, 2025, and received by the Community Development Department on June 30, 2025

Approval of the Vesting Tentative Map shall not constitute the approval of any improvements on the Vesting Tentative Map and shall not be construed as a guarantee of future extension or reapprovals of this or similar maps.

### **PUBLIC WORKS' STANDARD CONDITIONS**

- 14. City Standards.** All proposed public improvements shall be designed and constructed to City standards or as otherwise approved by the City Engineer in writing or on approved plans. The developer shall file for a City Encroachment permit for all improvements within the public right of way, a grading permit for grading of the site, and a building permit for all buildings and utilities to be installed on the site.
- 15. Required Easements and Rights-of-Way.** If necessary, all required easements or rights-of-way for improvements shall be obtained by the



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- developer at no cost to the City of Antioch prior to or concurrently with the recordation of the final map or subsequent final maps or separate recorded documents as approved by the City Engineer. Advance permission shall be obtained from any property or, if required from easement holders, for any work done within such property or easements.
- 16. Removal of Vacated Easements.** If necessary, all existing easements of record that are no longer required and will affect parcels within this project shall be removed prior to or concurrently with the recordation of the final map or subsequent final maps or separate recorded documents as approved by the City Engineer.
- 17. Utility Construction.** Relocation of Public utilities and construction of new private utilities shall be constructed to their ultimate size and configuration, as shown on the preliminary entitlement plan documents submitted to the City and improvement plans approved by the City Engineer and constructed prior to occupancy of the first building;
- 18. Utility Undergrounding.** To the extent possible all existing and proposed utilities shall be undergrounded (e.g., transformers and PMH boxes) and subsurface pursuant to Section 7-3 "Underground Utility Districts" of the Antioch Municipal Code prior to the final occupancy permit. Where utilities cannot be installed underground, they should be screened with landscaping, per City standards.
- 19. Utility Mapping.** Prior to acceptance of public utilities, the developer shall provide GPS coordinates of all below ground and above ground utilities. This includes all Water Distribution Utility features, sewer collection Utility features, Storm Water Utility features, and inverts, locations of pipes, manholes, cleanouts and utility meters associated with these features. Developer shall include GPS coordinates of water meters, irrigation meters, sewer cleanouts, sewer manholes, subdivision entryway signs, street signs, light poles, storm drain manholes, drainage inlets and transformers and gas meters needed for recording the location of all proposed utilities in the project as defined by the City Engineer. These GPS coordinates must be taken on a survey-grade GPS data receiver/collector and provided in GIS shapefile format using the NAVD 88 (with conversion information). Submittal of as-built drawings in AutoCAD drawing format in NAVD 88 coordinates shall satisfy this condition prior to the final occupancy permit.
- 20. Sewer.** All sewage shall flow by gravity to the sewer main located in the closest public street. The sewer connection shall comply with City standard plans and specifications. All sewer lines and utility connections to the City sewer system shall be in accordance with city and local sewer district



specifications. Proper backfill, compaction and road repair shall also be in accordance with City specifications and standard plans. The Developer shall install all sewage laterals from the lot to flow by gravity to the public sewer main which will drain by gravity to existing City sewer trunk mains located close to the project per City standards.

21. **Requirement for Looped System.** Water systems shall be designed as a looped distribution system. The developer shall be responsible for installing any off-site water mains to create a looped system in accordance with City and Fire District requirements at no cost to the City.
22. **Water Pressure.** The developer shall provide adequate water pressure and volume for fire flow and domestic use to serve this development. This will include a minimum residual pressure of 20 psi with all losses included at the highest sprinkler unit point in the building and at water service and a minimum static pressure of 20 psi at the water service or as approved by the City Engineer. See Fire Requirements for additional water flow conditions.
23. **Retaining Walls**
  - a. **Public Right of Way.** Retaining walls shall not be constructed in City right-of-way or other City maintained parcels, unless otherwise approved by the City Engineer.
  - b. **Materials.** All retaining walls shall be of concrete or masonry unit construction.
  - c. **Height.** All retaining walls shall be reduced in height to the maximum extent practicable and the walls shall meet the height requirements in the frontage setback and sight distance triangles as required by the City Engineer.
  - d. **Slope.** The 2:1 maximum slope above the retaining wall shall be landscaped with trees, ground cover, grass or erosion control vegetation as shown on the preliminary landscape plans to control erosion.
24. **Fences.** All perimeter fences shall be located in substantial conformance with the preliminary landscape plan approved by the Planning Commission / City Council as shown on the landscape improvement plans approved by the City Engineer.
  - a. In cases where a fence is to be built in conjunction with a retaining wall, and the wall face is exposed to a side street, the fence shall be setback a minimum of three feet (3') behind the retaining wall per Antioch Municipal Code Section 9-5.1603, or as otherwise approved by the City Engineer in writing.
  - b. All fencing shall be in substantial conformance with the fencing shown in the preliminary landscape plan approved by the Planning Commission/City Council or as shown on the landscape improvement



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plans approved by the City Engineer or as otherwise proposed by Applicant and approved by the City Engineer in writing at the time of improvement plan approval.

- 25. Storm Drain Design/Construction.** The developer shall design and construct all needed storm drain facilities to adequately collect and convey stormwater entering or originating within the development to the nearest adequate man-made drainage facility or natural watercourse, without diversion of the watershed.
- a. All public utilities, including storm drainpipes and ditches, shall be installed in all streets avoiding one lot draining over or between other lots. All proposed drainage facilities, including open ditches and detention basins shall be constructed of Portland Concrete Cement or as approved by the City Engineer. These public utilities shall be designed prior to building permit and constructed prior to occupancy.
  - b. Storm drains system shall flow to the Detention Basins shown within the project drainage study and as shown on the project grading and improvement plans with no diversion out of existing watershed(s).
  - c. The detention basin(s) and associated improvements shall be constructed and operational prior to occupancy of the first residential building permit.
  - d. Detention basins shall be designed in substantial conformance with the Planning Commission/City Council-approved Preliminary Stormwater Control Plan within the Vesting Tentative Map for Condominium Purposes. All storm drain and storm water improvements are subject to final review and approval. An Operations and Maintenance Manual shall be submitted for basins prior to the issuance of the first building permit.
- 26. Hydrology Analysis.** The developer shall submit hydrology and hydraulic analyses as part of the storm water control plan. The analysis shall demonstrate adequacy of the in-tract drainage system and downstream drainage system. The analysis shall be reviewed and approved by Contra Costa County Flood Control.

### **CONSERVATION / NPDES**

- 27. C.3 Compliance.** Per State Regulations, all onsite and offsite impervious surfaces including off-site roadways to be designed and constructed as part of the project are subject to State C.3 requirements. The design shall be approved prior to the first building permit and construction of the various facilities completed prior to occupancy of the first residential unit.
- 28. NPDES.** The project shall comply with all Federal, State, and City regulations for the National Pollution Discharge Elimination System (NPDES) (AMC§6-9). (Note: Per State Regulations, NPDES Requirements are those in affect



at the time of the Final Discretionary Approval.) Under NPDES regulations, the project is subject to provision C.3: "New development and redevelopment regulations for storm water treatment."

- a. **Requirements.** Provision C.3 requires that the project include storm water treatment and source control measures, as well as run-off flow controls so that post-project runoff does not exceed estimated pre-project runoff.
- b. **Storm Water Control Plan.** C.3 regulations require the submittal of a Storm Water Control Plan (SWCP) that demonstrates plan compliance. The SWCP shall be submitted to the Building and City Engineering Department concurrently with site improvement plans.
- c. **Operation and Maintenance Plan.** For the treatment and flow-controls identified in the approved SWCP, a separate Operation and Maintenance Plan (O&M) shall be submitted to the Building Department at the time of permit submittal and approved by the City Engineer.
- d. **CC&Rs.** Both the approved SWCP and O&M plans shall be included in the project CC&Rs. The design shall be approved prior to the first building permit and construction of the various facilities completed prior to occupancy of the first residential unit. The developer shall execute any agreements identified in the Storm Water Control Plan that pertain to the transfer of ownership and/or long-term maintenance of storm water treatment or hydrograph modification BMPs.

29. **NPDES Plan Submittal Requirements.** The following requirements of the federally mandated NPDES program (National Pollutant Discharge Elimination System) shall be complied with as appropriate, or as required by the City Engineer:

- a. **Application.** Prior to issuance of permits for building, site improvements, and/or landscaping, the developer shall submit a permit application consistent with the developer's approved C3 Storm Water Control Plan, and include drawings and specifications necessary for construction of site design features, measures to limit directly connected impervious areas, pervious pavements, self-retaining areas, treatment BMPs, permanent source control BMPs, and other features that control storm water flow and potential storm water pollutants.
- b. **Certified Professional.** The Storm Water Control Plan shall be stamped and signed by a registered civil engineer, or by a registered architect or landscape architect as applicable. Professionals certifying the Storm Water Control Plan shall be registered in the State of California on design of treatment measures for water quality, not more than three years prior to the signature date by an organization with storm water treatment measure design expertise (e.g., a university, American Society of Civil Engineers, American Society of Landscape Architects, American Public Works Association, or the California Water



- Environment Association), and verify understanding of groundwater protection principles applicable to the project site (see Provision C.3.i of Regional Water Quality Control Board Order R2 2003 0022).
- c. **Final Operation & Maintenance Plan.** Prior to building permit final and issuance of a Certificate of Occupancy, the Developer shall submit, for review and approval by the City, a final Storm Water BMP Operation and Maintenance Plan in accordance with City of Antioch guidelines. This O&M plan shall incorporate City comments on the draft O&M plan and any revisions resulting from changes made during construction. The O&M plan shall be incorporated into the CC&Rs for the Project if the project has CC&Rs.
  - d. **Long Term Management.** Prior to building permit final and issuance of a Certificate of Occupancy, the Developer shall execute and record any agreements identified in the Storm Water Control Plan which pertain to the transfer of ownership and/or long-term maintenance of all storm water treatment facility and maintenance of the underground detention facility to the satisfaction of the City Engineer.
    - i. The project shall prevent site drainage from draining across public sidewalks and driveways in a concentrated manner by installing drainpipes within or under the sidewalks per city details.
    - ii. Install on all catch basins “No Dumping, Drains to River” decal buttons in curb at all new site curb inlets and drainage inlets.
  - e. **C3 Hydrology Calculations.** The developer shall prepare a C3 storm water report with calculations of anticipated conveyance all C3 storm water entering and originating from the site to an adequate downstream drainage facility without diversion of the watershed prior to building permit. The developer shall submit C3 hydrologic and hydraulic calculations with the Improvement Plans to the City Engineering Department for review and approval by the City Engineer and to Contra Costa County Flood Control District.
  - f. **Regional Water Quality Control.** Prior to issuance of the grading permit, the developer shall submit proof of filing of a Notice of Intent (NOI) by providing the unique Waste Discharge Identification Number (WDID#) issued from the Regional Water Quality Control Board.
  - g. **SWPPP.** The developer shall submit a copy of the Storm Water Pollution Prevention Plan (SWPPP) for review to the Engineering Department prior to issuance of a building and/or grading permit. The general contractor and all subcontractors and suppliers of materials and equipment shall implement these BMP’s. Construction site cleanup and control of construction debris shall also be addressed in this program. Failure to comply with the approved construction BMP may result in the issuance of correction notices, citations, or a project stop work order.
  - h. **BMP.** The developer shall install all appropriate clean water devices at all storm drain locations immediately prior to entering the public storm



- drain system and Implement Best Management Practices (BMP's) at all times to the project before, during and after construction.
- i. **Erosion Control.** Include erosion control/storm water quality measures in the grading plan that specifically address measures to prevent soil, dirt, and debris from entering the storm drain system. Such measures may include, but are not limited to, hydro seeding, gravel bags and siltation fences or other measures that are subject to review and approval of the City Engineer. A grading plan will be required with the necessary erosion control/storm water quality measures shall be shown on the site plan submitted for an on-site permit, subject to review and approval of the City Engineer. The developer shall be responsible for ensuring that all contractors and subcontractors are aware of and implement such measures.
- i. The Developer or his assignee shall sweep and/or vacuum the paved parking lot(s) a minimum of once a month and prevent the accumulation of silt, litter, and debris on the site. Corners and hard to reach areas shall be swept manually.
- ii. If any sidewalks are to be pressure washed, debris shall be trapped and collected to prevent entry into the storm drain system. No cleaning agent may be discharged into the storm drain. If any cleaning agent or degreaser is used, wash water shall be collected and discharged to the sanitary sewer, subject to the approval of the sanitary sewer District.
- iii. Ensure that the area surrounding the project such as the streets stay free and clear of construction debris such as silt, dirt, dust, and tracked mud coming in from the project construction. Areas that are exposed for extended periods shall be watered regularly to reduce wind erosion. Paved areas and access roads shall be swept on a regular basis. All loads in dump trucks shall be covered per City requirements.
- iv. Clean all on-site storm drain facilities a minimum of twice a year, once immediately prior to October 15 and once in January. Additional cleaning may be required if found necessary by City Inspectors and/or City Engineer.

### **OUTSIDE AGENCIES**

30. **Contra Costa County Fire Protection District.** The applicant shall comply with the conditions provided by the Contra Costa County Fire Protection District in the letter dated March 20, 2024, and attached in the staff report.



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## **GRADING**

31. **Requirement for Grading Permit.** Grading plans shall be submitted, processed, and issued prior to commencement of any grading operations within the project. The permit shall be obtained through the City's Engineering and Building Divisions, subject to review and approval by the City Engineer. The submitted plans shall incorporate any modifications required by the Conditions of Approval, and applicable building codes.
32. **Grading Plans.** Locations of building exterior walls, fences and retaining walls, drainage swales, side slopes, top and bottom of slopes, parking lot drainage to catch basins with underground pipe drainage systems and pipe out falls shall be shown on the grading plans for review and approval. All the above features shall have proposed elevations shown on the grading plan and the grading of project will adequately drain to an above and underground drainage system in substantial conformance with the approved entitlement plans and improvement plans.
33. **Elevations on Grading Plans.** All elevations shown on the grading plans and plot plans shall be based on actual surveyed NAVD 88 survey control vertical datum, and, if needed, with conversion information, as approved by the City Engineer.
34. **Soils.** Prior to the approval of the grading plan(s), the City Engineer requires a registered soils engineer to review the grading plans, improvement plans, building permit plans, project specifications submitted for this project. The soil engineers field inspections will be required to verify compliance with the approved plans and soils reports prior to final occupancy permit. Costs for these consulting services shall be incurred by the developer.
35. **Off-Site Grading.** All off-site grading is subject to the coordination and approval of the affected property owners and the City Engineer. The developer shall submit written authorization to "access, enter, and/or grade" adjacent properties prior to issuance of permit and shall have permission from the property owner to performing any and all work prior to issuance of the first building permit of each phase of the subdivision.
36. **Grading Easements.** Any sale of a portion (or portions) of this project to another developer shall include the necessary CCR's, and/or grading and drainage easements, to assure that project-wide grading conforms to the approved development conditions of approval.



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## **AT BUILDING PERMIT SUBMITTAL**

37. **Requirement for Phasing Plan.** The Developer shall continuously build all improvements of this project in one phase. If the project becomes a phased project, then the developer shall provide a phasing plan to the Community Development Department and Engineering Department for review and approval..
38. **Final Landscape Plans.** The applicant shall submit final landscape plans that identify specific plant materials to be constructed, including all trees, shrubs and groundcover, and landscape features providing both common and botanical names, sizes, and quantities at the time of building permit submittal that are in substantial conformance with the Preliminary Landscape Plan approved by the Planning Commission/City Council and the approved improvement plans. See City approved plant list for installing approved plants in the public right of way.
39. **Water Efficient Landscape Ordinance.** Landscaping for the project shall be designed to comply with the applicable requirements of City of Antioch Ordinance No. 2162-C-S The State Model Water Efficient Landscape Ordinance (MWELo). The applicant shall demonstrate compliance with the applicable requirements of the MWELo in the landscape and irrigation plans submitted to the City.
40. **Site Landscaping** All site landscaping within the project site, including on all slopes, medians, C.3 drainage basins, retaining walls, bioretention basins, common areas, open space and park landscape areas, and any other areas that are to be landscaped shall be installed prior to issuance of final certificate of occupancy.
41. **Site Drainage.** All buildings in the site shall contain rain gutters and downspouts that catch rainwater from the roof and direct water into the underground storm drainpipe system and away from the foundation towards the closest drainage inlet structure or detention facility of the site in accordance with the applicable codes and as approved by the City Engineer. A detailed site grading drainage plan shall be prepared to drain water away from all proposed buildings into an acceptable drainage system. Temporary and permanent soil erosion control measures shall be designed and installed to prevent control soil erosion on the site
42. **Utility Location on Private Property.** All existing improvements (water meters, sewer cleanouts, etc.) that are disturbed shall be relocated as necessary within the immediate area of site as defined by the Planning Commission/City Council and approved by the City Engineer.



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43. **Construction Traffic Control Plan.** A Construction Traffic Control Plan shall be submitted for review and approval with the improvement plans for approval by the City Engineer.
  44. **Noise Mitigation Plan.** Prior to the issuance of any grading permits, the applicant shall submit a noise mitigation plan in compliance with General Plan Policy 11.8.2(o).
  45. **Residential Construction Controls for Diesel Particulate Matter.** Pursuant to Housing Element EIR Mitigation Measure AIR-3a, prior to the issuance of building permits, the applicant shall submit for review and approval by the Community Development Director documentation detailing how the mitigation measure will be met.

#### **AT BUILDING PERMIT ISSUANCE**

46. **Encroachment Permit.** The applicant shall obtain an encroachment permit from the Engineering Division before commencing any construction activities within any existing or proposed public right- of-ways or easements.

#### **MODEL HOMES AND SALES TRAILERS**

47. **Model Homes.** If the developer requests model homes or sales trailers be installed, prior to the placement of any sales trailers, the building and improvement plans shall be submitted to the Engineering Department for review and approval. All trailers shall be placed out of the public right-of-way. and shall have its own parking lot.
48. **Parking Lot** The model home complex parking lot location and design shall be subject to the City Engineer's approval.

#### **DURING CONSTRUCTION**

49. **Collection of Construction Debris.** During construction, the Developer shall place onsite dumpsters or other containers to contain all construction debris. The dumpster or other container shall be emptied on a regular basis consistent with the Construction and Demolition Debris Ordinance. When appropriate, use tarps on the ground to collect fallen debris or splatters that could contribute to stormwater pollution.
50. **Construction Hours.** Construction activity shall be as outlined in the Antioch Municipal Code Section 5-17.04 and .05(B). Construction activity is limited to 7:00 AM to 6:00 PM Monday-Friday except that activity within 300 feet of occupied dwelling space is limited to the hours of 8:00 AM to 5:00 PM on



weekdays. On weekends and holidays, construction activity is allowed 9:00 AM to 5:00 PM, irrespective of the distance from an occupied dwelling. Extended hours may be approved in writing by the City Manager or designee. These hours also ensure compliance with General Plan policy 11.8.2(o).

- 51. Demolition, Debris, Recycling.** All debris, garbage spoils, unwanted materials and vegetation shall be removed from the project site in accordance with City requirements. All materials that can be recycled shall be taken to an approved recycling facility. The project shall be kept clean and in compliance with and supply all the necessary documentation for Antioch Municipal Code Section 6-3.2: Construction and Demolition Debris Recycling.
- 52. Filter Materials at Storm Drain Inlet.** The developer shall Install filter materials (such as sandbags, filter fabric, etc.) at each storm drain inlet nearest the downstream side of the project site prior to:
- a. start of the rainy season (October 1).
  - b. site dewatering activities.
  - c. street washing activities.
  - d. saw cutting asphalt or concrete; and
  - e. order to retain any debris or dirt flowing into the city stormdrain system.

Filter materials shall be maintained and/or replaced as necessary to ensure effectiveness, prevent street flooding and further erosion of soil on City streets and draining into storm drain system. Dispose of used filter particles in the trash or at local approved landfill facility.

- 53. Archeological Remains.** In the event subsurface archeological remains are discovered during any construction or preconstruction activities on the site, all construction work within 100 feet of the find shall be halted, the Community Development Department notified, and a professional archeologist, certified by the Society of California Archeology and/or the Society of Professional Archeology, shall be notified. Site work in this area shall not occur until the archeologist has had an opportunity to evaluate the significance of the find and to outline appropriate mitigation measures, if deemed necessary. If prehistoric archeological deposits are discovered during development of the site, local Native American organizations shall be consulted and involved in making resource management decisions.
- 54. Erosion Control Measures.** The grading operation of the development shall take place at one time, and in a manner, to prevent soil erosion and sedimentation. The slopes shall be landscaped and reseeded as soon as possible after the grading operation ceases. Erosion measures shall be implemented during all phases of construction in accordance with an approved erosion and sedimentation control plan.



- 55. Dust Control.** Standard dust control methods and designs shall be used to stabilize the dust generated by construction activities. The developer shall post dust control signage with a contact number of the developer, City staff, and the air quality control board.
- 56. Debris Removal During Construction.** The site shall be kept clean of all debris (boxes, junk, garbage, etc.) at all times.

### **FINAL MAP**

- 57. Requirements for Final Map.** The Final Subdivision Map submittal shall include all of the following required information described in Title 9, Chapter 4, Article 5: Final Maps, of the Antioch Municipal Code, including, but not limited to:
- a.** Improvement security in one of the following forms:
    - i.** Bond or bonds issued by one or more duly authorized corporate securities in an amount equal to 100% of the total estimated costs of the improvements for faithful performance, and in an amount equal to 100% of the total estimated costs of the improvements for labor and materials.
    - ii.** A deposit, in an amount equal to 100% of the total estimated costs of the improvements, either with the city or a responsible escrow agent or trust company, at the option of the City Engineer, of money or negotiable bonds of the kind approved for securing deposits of public moneys, in the amounts and for security as specified above, to be released in the same manner as described above for bonds.
  - b.** An original, signed subdivision agreement, to be executed by the subdivider or his agent, guaranteeing the construction costs, completion of the construction of the improvements required by the governing body within a specified time and payment, satisfactory to the City Attorney and the City Engineer.
  - c.** A letter from the Tax Collector showing that all payable taxes have been paid and a bond for the payment of taxes, a lien on the property but not yet payable, as required by the Subdivision Map Act.
  - d.** A cash payment, or receipt therefore, of all the fees required for the checking and filing of the maps and the inspections of the construction; payment for the street signs to be furnished and installed by the city, if required by the subdivider; a cash deposit for the payment of such fire hydrant rental fees as may be established by the fire districts or water company or district having jurisdiction; and any other applicable fees or deposits.



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- e. Deeds for all right of way dedications, easements for access and utility purposes as shown on the tentative and final maps.
  - f. Written evidence acceptable to the city, in the form of rights of entry or permanent easements across private property outside the subdivision, permitting or granting access to perform the necessary construction work and permitting the maintenance of the facility.
  - g. Agreements acceptable to the city, executed by the owners of existing utility easements within the proposed roads rights-of-way, consenting to the dedication of roads or consenting to the joint use of the rights-of-way as may be required by the city for the purpose use and convenience of the roads.
  - h. A surety bond acceptable to the city, guaranteeing the payment of the taxes and assessments which will be a lien on the property, as set forth in the Subdivision Map Act, when applicable.
  - i. Payment of map maintenance fee.
  - j. Payment of the assessment district apportionment fee, if applicable.
  - k. Evidence of annexation into Police Services Fee CFD
  - l. Evidence of payment of Contra Costa County Flood Control District fees.
  - m. A final soil report, prepared by a civil engineer who is registered by the state, based upon adequate test borings or excavations of every subdivision, as defined in Cal. Gov't Code §§ 66490 and 66491. The final soil report may be waived if the City Engineer shall determine that, due to the knowledge of such department as to the soil qualities of the subdivision, no preliminary analysis is necessary.

**58. Postal Service.** Provisions for mail delivery and locations of mailbox facilities shall be reviewed by the USPS and approved by the City Engineer prior to the first occupancy..

#### **PRIOR TO ISSUANCE OF OCCUPANCY PERMIT**

**59. Planning Inspection.** Planning staff shall conduct a site visit to review exterior building elevations for architectural consistency with the approved plans and landscape installation (if required). All exterior finishing details including window trim, paint, gutters, downspouts, decking, guardrails, and driveway installation shall be in place prior to scheduling the final inspection.

**60. Debris Removal.** All mud, dirt, and construction debris shall be removed from the construction site prior to scheduling the final Planning inspection. No materials shall be discharged onto a sidewalk, street, gutter, storm drain or creek.

**61. Damage to Street Improvements.** Any damage occurring during construction to existing streets and site improvements or adjacent property improvements in the



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immediate area of the project, shall be repaired and/or rebuilt to the satisfaction of the City Engineer at the full expense of the developer. This shall include sidewalks, asphalt and concrete pavement, slurry seal existing AC pavements, parking lot curb and gutter, landscaping, street reconstruction along the project frontage, as may be required by the City Engineer to make the developed area to be looking like it is new.

- 62. Right-of-Way Construction Standards.** All improvements within the public right-of-way, including curb, gutter, sidewalks, driveways, paving and utilities, shall be constructed in accordance with the City approved improvement plans, standard plans and/or city specifications as directed by the City Engineer.
- 63. Double Detector Check Valve Assembly.** The developer shall install the required sprinkler Double Detector Check Valve assemblies, and fire department connections in an enclosed area that is screened by landscaping or small 3.5' high masonry walls or placed within the building or in an underground vault so it is not visible from public view as approved by City Engineer and Fire Marshal prior to building permit and installed prior occupancy.
- 64. Trash Receptacles.** Trash receptacles located in common area trash enclosures shall use City Park type three-sort trash receptables. All Trash receptacles shall be in place prior to issuance of the first building certificate of occupancy.

#### **SPECIAL CONDITIONS OF APPROVAL**

- 65.** All bioretention basins for the site and associated storm drain improvements shall be designed per details shown on the preliminary plans and approved prior to building permit and constructed and operational prior to issuance of the first occupancy permit of the residential complex.
- 66. Open Space Construction.** The Central Common Area (Parcel K) shall be constructed prior to occupancy of the fourth (4<sup>th</sup>) residential building.
- 67. Open Space Construction.** Prior to the issuance of the building permit for the construction of the first multifamily unit show, the preliminary site plans, a Northern Common Area (Parcel J) shall be constructed prior to occupancy of the fourth (4<sup>th</sup>) residential building.
- 68. Private Park Construction.** The private park shall meet all the City's Park design standards current at the time of park construction and shall include a color scheme soothing for children with visual sensitivities.
- 69. Park In-Lieu Fee.** As recommended by the Parks and Recreation Commission



on October 24, 2024, the following condition shall apply:

Prior to the issuance of the first building permit, the project shall pay the required in-lieu fee of \$151,050 as required in Antioch Municipal Code Section Title 9, Chapter 4, Article 10: Regulations for the Dedication of Land, the Payment of Fees, or Both, for Park and Recreation Lands.

70. The maximum backslopes, side slopes, natural grade transitions shall be a maximum grade slope of 3:1, and shall have proper drainage swales, benching and drainage ditches to adequately drain the residential site so no ponding occurs. All slopes shall be graded with proper erosion control measures in place so not cause slope failures or erosion of the soil as approved by the City Engineer and soil engineer. A 2:1 slope may be used for slopes less than 10' high if approved by the project soil engineer for small grading transitions, and the slope is vegetated with erosion control measures.
71. The minimum longitudinal slope of concrete gutters and pavement slopes shall be 0.35% unless approved by the City Engineer. The minimum slope of asphalt pavement is 1.5% and the minimum slope of concrete surface is 0.5% unless approved by the City Engineer.
72. The Developer shall implement project-specific geotechnical recommendations Prior to issuance of any grading permits, all recommendations and specifications set forth in the project specific Geotechnical Exploration Report prepared for the proposed project by the project Soils Engineers, shall be reflected on the project grading and foundation plans (inclusive of seismic design parameters), subject to review and approval by the City Engineer.
73. Grading and Foundation Plan Review and Construction inspection and Monitoring shall be provided by the Developer The Developer shall retain a geotechnical engineering firm to review the final grading and foundation plans and specifications to evaluate whether recommendations have been implemented from the project-specific Geotechnical Exploration Report, and to provide additional or modified recommendations, as needed. Construction monitoring shall be performed by a California Registered Geologist and/or Engineer to check the validity of the assumptions made in the geotechnical investigation. Earthwork operations shall be performed under the observation of a California Registered Geologist and/or Soils Engineer to check that the site is properly prepared, the selected fill materials are satisfactory, and that placement and compaction of the fills has been performed in accordance with recommendations and the project specifications.
74. The Developer shall dedicate the following easements on the Final Map in substantial conformance with the approved Vesting Tentative Map Sheets 1 & 2:



- a) Public utility easements (PUE) shown on all private streets and accessways as shown on the vesting tentative map and preliminary site plan 13 and these conditions including all the locations of water meters, water services sanitary sewer cleanout to all buildings to the satisfaction of the City Engineer.
  - b) Emergency vehicle access easements (EVAE) shall be over all private streets, alleys and walkways as shown on the preliminary site plan 3 and 4.
  - c) All new bioretention basins and underground detention basins shall be in a PUE or a separate storm drain easement.
  - d) Public utility easements all be over all roadways, alleys, and access for underground utilities.
  - e) All easements shall be dedicated on the first final map of the proposed development prior to the first building permit for the first unit.
75. In substantial conformance with the City-approved Traffic Impact Analysis (Abrams, Dec 4, 2024) and the approved Preliminary Signing and Striping Plan (Sheet 14) the Developer shall install stop legends and stop bars at all intersections with street A, and at existing Wildflower Station Place submitted prior to occupancy of the first building unit.
76. As part of the project acceptance and prior to release of warranty bonds of the sewer and storm drain lines one year after installation of the project, the Developer shall video all sewer and storm drain lines installed to document the condition of pipe one year after construction to see if there has been any settlement of the pipe or if there anything that will restrict the flow or capacity of the pipes. Any settlement in the pipes shall be repaired and any restriction in the flow or pipe capacity shall be removed prior to acceptance to the satisfaction of the City Engineer.
77. **Street Names.** The street names in the development shall be as follows. Changes to the street names shall require Planning Commission review and approval:
- Parcels A, N-Q – Meadowfoam Lane
  - Parcels B – Clarkia Place
  - Parcels C – Ceanothus Lane
  - Parcels D – Lewisia Lane
  - Parcels E – Fuchsia Place
  - Parcels F – Yarrow Place
  - Parcels G – Snowberry Place
  - Parcels H – Artemesia Place
  - Between Lots 7 and K – Salvia Lane





**PARKS & RECREATION COMMISSION SPECIAL MEETING**

**Thursday  
October 24, 2024  
7:00 p.m.**

**ANNOTTATED AGENDA/ MINUTES**

**I. CALL TO ORDER**

*Call to Order by Chair King at 7:02 pm*

**II. ROLL CALL**

*Commissioners Present: Ellis, Eubanks, Hunt, King  
Commissioners Absent: Del Castillo (arrived 7:05pm), Williams (arrived 7:09pm)  
Staff Present: Brad Helfenberger, Parks and Recreation Director  
Scott Buenting, Acting Public Works Director/ City Engineer  
Carlos Zepeda, Deputy Public Works Director*

**III. PLEDGE OF ALLEGIANCE**

*Chair King led the Pledge of Allegiance*

**VI. BUSINESS**

1. Bicycle and Pedestrian Advisory Committee Review of the Transportation Development Act Grant for School Zone Improvements at Marsh Elementary School and John Muir Elementary School; P.W. 124-3

*Commissioner Del Castillo arrived at 7:05 pm  
Commissioner Williams arrived at 7:09 pm*

*Acting Director Buenting introduced the project and presented the improvements and how grant funds would be used. Several clarifying questions were asked and answered. Commissioner Del Castillo asked about asking for increased funding for future projects. Director Buenting responded that the projects are more competitive at the current size. Commissioner Del Castillo requested a more thoughtful process for future projects, as he feels that this project is a band-aid fix.*

*Individuals may view the agenda and related writings on the City of Antioch website: [www.antiochca.gov](http://www.antiochca.gov). In accordance with the Americans with Disabilities Act and California law, it is the policy of the City of Antioch to offer its public programs, services and meetings in a manner that is readily accessible to everyone, including individuals with disabilities. If you are a person with a disability and require information or materials in an appropriate alternative format; or if you require any other accommodation, please contact the ADA Coordinator at the number or address below at least 72 hours prior to the meeting or when you desire to receive services. Advance notification within this guideline will enable the City to make reasonable arrangements to ensure accessibility. The City's ADA Coordinator can be reached @ Phone: (925) 779-6950, and e-mail: [publicworks@ci.antioch.ca.us](mailto:publicworks@ci.antioch.ca.us).*



*Motion to Recommend Approval of the Transportation Development Act Grant for School Zone Improvements at Marsh Elementary School and John Muir Elementary School.*

*Motion: Ellis  
Second: Hunt  
4 Yes  
1 No (Del Castillo)  
1 Abstain (King)  
0 Absent*

**2. Albers Property Subdivision 9515 Residential Project Determination of Dedication of Land or Payment of Park-In-Lieu Fees**

*Acting Director Buenting introduced the project. The development will result \$441,000 in Park in Lieu Fees. Commissioner Del Castillo asked about the role of the Commission in this process. Commissioner Ellis asked what happens to the funds. Director Helfenberger responded that they go into a fund for new parks or to renovate existing parks. A discussion was held on how the fees are determined and how the park-in-lieu program works.*

*Motion to recommend a determination that Albers Property Subdivision 9515 single family residential project is subject to a payment of \$441,000 of park land dedication park-in-lieu fees.*

*Motion: Del Castillo  
Second: Ellis  
6 Yes/ 0 No  
0 Absent*

**3. Slatten Ranch Subdivision 9645 Condominium Multi-family Project Determination of Dedication of Land or Payment of Park-In-Lieu Fees**

*Acting Director Buenting introduced the project and the developer. The development will result \$122,550 in Park in Lieu Fees. The developer, Trent Sanson with DeNova Homes provided some information on the project as well as further information about the Quimby Act, which governs how park-in-lieu fees are determined and how funds can be used. The Commission thanked Mr. Sanson for the explanation.*

*Motion to recommend a determination that Slatten Ranch Subdivision 9645 single family residential project is subject to a payment of \$122,550 of park land dedication park-in-lieu fees.*

*Motion: Del Castillo  
Second: Hunt  
6 Yes/ 0 No  
0 Absent*

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4. Wildflower Station Townhomes 2 Subdivision 9601 Determination of Dedication of Land or Payment of Park-In-Lieu Fees

*Acting Director Buenting introduced the project and the developer, Trent Sanson with DeNova Homes. The development will result \$151,050 in Park in Lieu Fees.*

*Motion to recommend a determination that Wildflower Station Townhomes 2 Subdivision 9601 single family residential project is subject to a payment of \$151,050 of park land dedication park-in-lieu fees.*

*Motion: Del Castillo  
Second: Ellis  
6 Yes/ 0 No  
0 Absent*

5. Formation of Ad-Hoc Committee for Purposes of Reviewing 2024-25 Civic Enhancement Grant Applications and Award Recommendation

*Director Helfenberger gave an overview of the program and reviewed how it was implemented for 2024. A new ad-hoc committee is needed to determine recommendations for the 2024-25 program. The committee will return to the Commission with a report and final recommendation, which will then be forwarded to the City Council for approval.*

*Motion to appoint Commissioners Del Castillo, Hunt, and Williams to the 2024-25 Civic Enhancement Grant Selection Committee*

*Motion: Ellis  
Second: Eubanks  
6 Yes/ 0 No  
0 Absent*

**VIII. ADJOURNMENT**

*Motion to adjourn the meeting.*

*Motion: Hunt  
Second: Del Castillo*

*6 Yes /0 No  
0 Absent*

*Meeting Adjourned at 8:06pm*

*Individuals may view the agenda and related writings on the City of Antioch website: [www.antiochca.gov](http://www.antiochca.gov). In accordance with the Americans with Disabilities Act and California law, it is the policy of the City of Antioch to offer its public programs, services and meetings in a manner that is readily accessible to everyone, including individuals with disabilities. If you are a person with a disability and require information or materials in an appropriate alternative format; or if you require any other accommodation, please contact the ADA Coordinator at the number or address below at least 72 hours prior to the meeting or when you desire to receive services. Advance notification within this guideline will enable the City to make reasonable arrangements to ensure accessibility. The City's ADA Coordinator can be reached @ Phone: (925) 779-6950, and e-mail: [publicworks@ci.antioch.ca.us](mailto:publicworks@ci.antioch.ca.us).*



## ATTACHMENT "C"

**From:** [Ayon, Llisel@DOT](mailto:Ayon, Llisel@DOT)  
**To:** [Merideth, Zoe](#)  
**Cc:** [Valente, Kevin](#); [Kerri Watt](#)  
**Subject:** RE: Wildflower Town Homes (TM-02, AR-23-05)  
**Date:** Wednesday, March 13, 2024 1:48:31 PM  
**Attachments:** [image001.png](#)  
[image002.png](#)

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CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Thanks!

**Llisel Ayon**

Associate Transportation Planner  
Caltrans, District 4 | Local Development Review  
Cell: (510) 506-6184



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**From:** Merideth, Zoe <[zmerideth@antiochca.gov](mailto:zmerideth@antiochca.gov)>  
**Sent:** Thursday, March 7, 2024 11:09 AM  
**To:** Ayon, Llisel@DOT <[Llisel.Ayon@dot.ca.gov](mailto:Llisel.Ayon@dot.ca.gov)>  
**Cc:** Valente, Kevin <[kvalente@raneymanagement.com](mailto:kvalente@raneymanagement.com)>; Kerri Watt <[kwatt@denovahomes.com](mailto:kwatt@denovahomes.com)>  
**Subject:** RE: Wildflower Town Homes (TM-02, AR-23-05)

**EXTERNAL EMAIL.** Links/attachments may not be safe.

Hello,

Thanks for your review. There was a TIA for this project, which found no significant impacts or off-site improvements will be required. It's attached. Thank you.

**Zoe Merideth**  
Planning Manager

Direct: (925) 779-6122  
Main: (925) 779-6159

City of Antioch | P.O. Box 5007, Antioch, CA 94531-5007



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**From:** Ayon, Llisel@DOT <[Llisel.Ayon@dot.ca.gov](mailto:Llisel.Ayon@dot.ca.gov)>  
**Sent:** Thursday, March 7, 2024 10:51 AM



**To:** Merideth, Zoe <[zmerideth@antiochca.gov](mailto:zmerideth@antiochca.gov)>

**Subject:** Wildflower Town Homes (TM-02, AR-23-05)

**CAUTION:** This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Hi Zoe,

Thank you for including Caltrans in the review process for the above referenced project. We have reviewed the provided documents and do not have any concerns regarding the materials provided. We were wondering if there was a traffic impact study for this project, given the size?

Thank you,

**Llisel Ayon**

Associate Transportation Planner

Caltrans, District 4 | Local Development Review

Cell: (510) 506-6184





## Villagrana, Monique

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**From:** Kelsey Gunter <KGunter@pittsburgca.gov>  
**Sent:** Tuesday, March 5, 2024 11:11 AM  
**To:** Villagrana, Monique  
**Subject:** RE: City of Antioch Project Routing for Wildflower Town Homes (TM-02, AR-23-05)

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello,

The City of Pittsburg does not have any comment on the project proposed. Thank you for including us on your referral.

Thank you,

**Kelsey Gunter**  
Associate Planner



City of Pittsburg  
Community & Economic Development Department - Planning Division  
65 Civic Avenue, Pittsburg, CA 94565  
Tel: 925.252.4824 | Fax: 925.252.4814  
[kgunter@pittsburgca.gov](mailto:kgunter@pittsburgca.gov)

<https://www.pittsburgca.gov/services/community-development/planning>



**Please click [here](#) to access the City's Online Permitting Portal, Accela. Visit [pittsburg.opencounter.com](http://pittsburg.opencounter.com) to find zoning and permit information.**

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**From:** Villagrana, Monique <mvillagrana@antiochca.gov>  
**Sent:** Wednesday, February 28, 2024 2:17 PM  
**Subject:** City of Antioch Project Routing for Wildflower Town Homes (TM-02, AR-23-05)

**\*\*External Sender: Use caution before opening links or attachments\*\***

Good Afternoon,

The City of Antioch Planning Division is requesting your review of the project information and plans contained in the attached document.

Please provide feedback on availability of services, potential design or code conflicts, requirements for additional permits and draft conditions of project approval.

Responses are requested at your earliest availability, by or before March 20, 2024, to Zoe Merideth at [zmerideth@antiochca.gov](mailto:zmerideth@antiochca.gov).



Thank you in advance for your time and collaboration.

## *Monique Villagrana*

Community Development Technician | Planning Division

☎: (925) 779-6163 (Direct)

☎: (925) 779-6159 (Main)

☎: (925) 779-7034 (Fax)

✉: [mvillagrana@antiochca.gov](mailto:mvillagrana@antiochca.gov)

🌐: [www.antiochca.gov](http://www.antiochca.gov)

Community Development Department | 200 'H' Street, Antioch, CA 94509-1005







March 19, 2024

Monique Villagrana  
City of Antioch  
200 H Street  
Antioch, CA 94509

Re: TM-02 AR-23-05 Wildflower Townhomes II  
2500 Wildflower Station Place, Antioch, CA 94531  
APN: 052-140-013, 052-140-014, 052-140-015, 052-140-016

Dear Monique Villagrana,

Thank you for giving us the opportunity to review the proposed TM-02 AR-23-05. The installation of new gas and electric facilities and/or relocation of existing PG&E facilities will be performed in accordance with common law or Rules and Tariffs as authorized by the California Public Utilities Commission.

Following our review, PG&E recommends the following language be expressly stated for the offer to dedicate Public Utility Easements (PUE):

I/We the undersigned, as Owner(s) of the land shown hereon, do hereby state that I/we am/are the only person(s) whose consent is necessary to pass clear title to said land and do hereby consent to the preparation and recordation of this map and offer for dedication and do hereby dedicate for public uses the Public Utility Easements (PUEs) shown on this map for public utility purposes including electric, gas, communication facilities and all other public utility purposes; together with any and all appurtenances thereto, including the right from time to time to trim and to cut down and clear away or otherwise control any trees or brush. The PUEs hereby offered for dedication are to be kept open and free of buildings, structures and wells of any kind.

The final map must contain a statement setting forth dedications and offers to dedicate interests in real property for public utility purposes. If the offer of dedication has terminated, or the local agency declines to accept it, the applicant maybe required to provide an easement in gross satisfactory to PG&E. Please note that this is our preliminary review and PG&E reserves the right for future review as needed.

Please work with PG&E's Service Planning department at [www.pge.com/cco](http://www.pge.com/cco) for additional services you may require, or for any modification and/or relocation requests.

Sincerely,

Brian Callaghan  
Land Management  
925-204-4074





March 4, 2024

Monique Villagrana  
City of Antioch  
200 H Street  
Antioch, CA 94509

Ref: Gas and Electric Transmission and Distribution

Dear Monique Villagrana,

Thank you for submitting the TM-02 and AR-23-05 plans for our review. PG&E will review the submitted plans in relationship to any existing Gas and Electric facilities within the project area. If the proposed project is adjacent/or within PG&E owned property and/or easements, we will be working with you to ensure compatible uses and activities near our facilities.

Attached you will find information and requirements as it relates to Gas facilities (Attachment 1) and Electric facilities (Attachment 2). Please review these in detail, as it is critical to ensure your safety and to protect PG&E's facilities and its existing rights.

Below is additional information for your review:

1. This plan review process does not replace the application process for PG&E gas or electric service your project may require. For these requests, please continue to work with PG&E Service Planning: [https://www.pge.com/en\\_US/business/services/building-and-renovation/overview/overview.page](https://www.pge.com/en_US/business/services/building-and-renovation/overview/overview.page).
2. If the project being submitted is part of a larger project, please include the entire scope of your project, and not just a portion of it. PG&E's facilities are to be incorporated within any CEQA document. PG&E needs to verify that the CEQA document will identify any required future PG&E services.
3. An engineering deposit may be required to review plans for a project depending on the size, scope, and location of the project and as it relates to any rearrangement or new installation of PG&E facilities.

Any proposed uses within the PG&E fee strip and/or easement, may include a California Public Utility Commission (CPUC) Section 851 filing. This requires the CPUC to render approval for a conveyance of rights for specific uses on PG&E's fee strip or easement. PG&E will advise if the necessity to incorporate a CPUC Section 851 filing is required.

This letter does not constitute PG&E's consent to use any portion of its easement for any purpose not previously conveyed. PG&E will provide a project specific response as required.

Sincerely,

Plan Review Team  
Land Management





## **Attachment 1 – Gas Facilities**

There could be gas transmission pipelines in this area which would be considered critical facilities for PG&E and a high priority subsurface installation under California law. Care must be taken to ensure safety and accessibility. So, please ensure that if PG&E approves work near gas transmission pipelines it is done in adherence with the below stipulations. Additionally, the following link provides additional information regarding legal requirements under California excavation laws: <https://www.usanorth811.org/images/pdfs/CA-LAW-2018.pdf>

1.     Standby Inspection: A PG&E Gas Transmission Standby Inspector must be present during any demolition or construction activity that comes within 10 feet of the gas pipeline. This includes all grading, trenching, substructure depth verifications (potholes), asphalt or concrete demolition/removal, removal of trees, signs, light poles, etc. This inspection can be coordinated through the Underground Service Alert (USA) service at 811. A minimum notice of 48 hours is required. Ensure the USA markings and notifications are maintained throughout the duration of your work.
2.     Access: At any time, PG&E may need to access, excavate, and perform work on the gas pipeline. Any construction equipment, materials, or spoils may need to be removed upon notice. Any temporary construction fencing installed within PG&E's easement would also need to be capable of being removed at any time upon notice. Any plans to cut temporary slopes exceeding a 1:4 grade within 10 feet of a gas transmission pipeline need to be approved by PG&E Pipeline Services in writing PRIOR to performing the work.
3.     Wheel Loads: To prevent damage to the buried gas pipeline, there are weight limits that must be enforced whenever any equipment gets within 10 feet of traversing the pipe.

Ensure a list of the axle weights of all equipment being used is available for PG&E's Standby Inspector. To confirm the depth of cover, the pipeline may need to be potholed by hand in a few areas.

Due to the complex variability of tracked equipment, vibratory compaction equipment, and cranes, PG&E must evaluate those items on a case-by-case basis prior to use over the gas pipeline (provide a list of any proposed equipment of this type noting model numbers and specific attachments).

No equipment may be set up over the gas pipeline while operating. Ensure crane outriggers are at least 10 feet from the centerline of the gas pipeline. Transport trucks must not be parked over the gas pipeline while being loaded or unloaded.

4.     Grading: PG&E requires a minimum of 36 inches of cover over gas pipelines (or existing grade if less) and a maximum of 7 feet of cover at all locations. The graded surface cannot exceed a cross slope of 1:4.
5.     Excavating: Any digging within 2 feet of a gas pipeline must be dug by hand. Note that while the minimum clearance is only 24 inches, any excavation work within 24 inches of the edge of a pipeline must be done with hand tools. So to avoid having to dig a trench entirely with hand tools, the edge of the trench must be over 24 inches away. (Doing the math for a 24 inch





wide trench being dug along a 36 inch pipeline, the centerline of the trench would need to be at least 54 inches [ $24/2 + 24 + 36/2 = 54$ ] away, or be entirely dug by hand.)

Water jetting to assist vacuum excavating must be limited to 1000 psig and directed at a 40° angle to the pipe. All pile driving must be kept a minimum of 3 feet away.

Any plans to expose and support a PG&E gas transmission pipeline across an open excavation need to be approved by PG&E Pipeline Services in writing PRIOR to performing the work.

6. Boring/Trenchless Installations: PG&E Pipeline Services must review and approve all plans to bore across or parallel to (within 10 feet) a gas transmission pipeline. There are stringent criteria to pothole the gas transmission facility at regular intervals for all parallel bore installations.

For bore paths that cross gas transmission pipelines perpendicularly, the pipeline must be potholed a minimum of 2 feet in the horizontal direction of the bore path and a minimum of 24 inches in the vertical direction from the bottom of the pipe with minimum clearances measured from the edge of the pipe in both directions. Standby personnel must watch the locator trace (and every ream pass) the path of the bore as it approaches the pipeline and visually monitor the pothole (with the exposed transmission pipe) as the bore traverses the pipeline to ensure adequate clearance with the pipeline. The pothole width must account for the inaccuracy of the locating equipment.

7. Substructures: All utility crossings of a gas pipeline should be made as close to perpendicular as feasible ( $90^\circ \pm 15^\circ$ ). All utility lines crossing the gas pipeline must have a minimum of 24 inches of separation from the gas pipeline. Parallel utilities, pole bases, water line 'kicker blocks', storm drain inlets, water meters, valves, back pressure devices or other utility substructures are not allowed in the PG&E gas pipeline easement.

If previously retired PG&E facilities are in conflict with proposed substructures, PG&E must verify they are safe prior to removal. This includes verification testing of the contents of the facilities, as well as environmental testing of the coating and internal surfaces. Timelines for PG&E completion of this verification will vary depending on the type and location of facilities in conflict.

8. Structures: No structures are to be built within the PG&E gas pipeline easement. This includes buildings, retaining walls, fences, decks, patios, carports, septic tanks, storage sheds, tanks, loading ramps, or any structure that could limit PG&E's ability to access its facilities.

9. Fencing: Permanent fencing is not allowed within PG&E easements except for perpendicular crossings which must include a 16 foot wide gate for vehicular access. Gates will be secured with PG&E corporation locks.

10. Landscaping: Landscaping must be designed to allow PG&E to access the pipeline for maintenance and not interfere with pipeline coatings or other cathodic protection systems. No trees, shrubs, brush, vines, and other vegetation may be planted within the easement area. Only those plants, ground covers, grasses, flowers, and low-growing plants that grow unsupported to a maximum of four feet (4') in height at maturity may be planted within the easement area.





11. Cathodic Protection: PG&E pipelines are protected from corrosion with an “Impressed Current” cathodic protection system. Any proposed facilities, such as metal conduit, pipes, service lines, ground rods, anodes, wires, etc. that might affect the pipeline cathodic protection system must be reviewed and approved by PG&E Corrosion Engineering.

12. Pipeline Marker Signs: PG&E needs to maintain pipeline marker signs for gas transmission pipelines in order to ensure public awareness of the presence of the pipelines. With prior written approval from PG&E Pipeline Services, an existing PG&E pipeline marker sign that is in direct conflict with proposed developments may be temporarily relocated to accommodate construction work. The pipeline marker must be moved back once construction is complete.

13. PG&E is also the provider of distribution facilities throughout many of the areas within the state of California. Therefore, any plans that impact PG&E’s facilities must be reviewed and approved by PG&E to ensure that no impact occurs which may endanger the safe operation of its facilities.





## **Attachment 2 – Electric Facilities**

It is PG&E's policy to permit certain uses on a case by case basis within its electric transmission fee strip(s) and/or easement(s) provided such uses and manner in which they are exercised, will not interfere with PG&E's rights or endanger its facilities. Some examples/restrictions are as follows:

1. **Buildings and Other Structures:** No buildings or other structures including the foot print and eave of any buildings, swimming pools, wells or similar structures will be permitted within fee strip(s) and/or easement(s) areas. PG&E's transmission easement shall be designated on subdivision/parcel maps as **"RESTRICTED USE AREA – NO BUILDING."**
2. **Grading:** Cuts, trenches or excavations may not be made within 25 feet of our towers. Developers must submit grading plans and site development plans (including geotechnical reports if applicable), signed and dated, for PG&E's review. PG&E engineers must review grade changes in the vicinity of our towers. No fills will be allowed which would impair ground-to-conductor clearances. Towers shall not be left on mounds without adequate road access to base of tower or structure.
3. **Fences:** Walls, fences, and other structures must be installed at locations that do not affect the safe operation of PG&E's facilities. Heavy equipment access to our facilities must be maintained at all times. Metal fences are to be grounded to PG&E specifications. No wall, fence or other like structure is to be installed within 10 feet of tower footings and unrestricted access must be maintained from a tower structure to the nearest street. Walls, fences and other structures proposed along or within the fee strip(s) and/or easement(s) will require PG&E review; submit plans to PG&E Centralized Review Team for review and comment.
4. **Landscaping:** Vegetation may be allowed; subject to review of plans. On overhead electric transmission fee strip(s) and/or easement(s), trees and shrubs are limited to those varieties that do not exceed 10 feet in height at maturity. PG&E must have access to its facilities at all times, including access by heavy equipment. No planting is to occur within the footprint of the tower legs. Greenbelts are encouraged.
5. **Reservoirs, Sumps, Drainage Basins, and Ponds:** Prohibited within PG&E's fee strip(s) and/or easement(s) for electric transmission lines.
6. **Automobile Parking:** Short term parking of movable passenger vehicles and light trucks (pickups, vans, etc.) is allowed. The lighting within these parking areas will need to be reviewed by PG&E; approval will be on a case by case basis. Heavy equipment access to PG&E facilities is to be maintained at all times. Parking is to clear PG&E structures by at least 10 feet. Protection of PG&E facilities from vehicular traffic is to be provided at developer's expense AND to PG&E specifications. Blocked-up vehicles are not allowed. Carports, canopies, or awnings are not allowed.
7. **Storage of Flammable, Explosive or Corrosive Materials:** There shall be no storage of fuel or combustibles and no fueling of vehicles within PG&E's easement. No trash bins or incinerators are allowed.





8. Streets and Roads: Access to facilities must be maintained at all times. Street lights may be allowed in the fee strip(s) and/or easement(s) but in all cases must be reviewed by PG&E for proper clearance. Roads and utilities should cross the transmission easement as nearly at right angles as possible. Road intersections will not be allowed within the transmission easement.

9. Pipelines: Pipelines may be allowed provided crossings are held to a minimum and to be as nearly perpendicular as possible. Pipelines within 25 feet of PG&E structures require review by PG&E. Sprinklers systems may be allowed; subject to review. Leach fields and septic tanks are not allowed. Construction plans must be submitted to PG&E for review and approval prior to the commencement of any construction.

10. Signs: Signs are not allowed except in rare cases subject to individual review by PG&E.

11. Recreation Areas: Playgrounds, parks, tennis courts, basketball courts, barbecue and light trucks (pickups, vans, etc.) may be allowed; subject to review of plans. Heavy equipment access to PG&E facilities is to be maintained at all times. Parking is to clear PG&E structures by at least 10 feet. Protection of PG&E facilities from vehicular traffic is to be provided at developer's expense AND to PG&E specifications.

12. Construction Activity: Since construction activity will take place near PG&E's overhead electric lines, please be advised it is the contractor's responsibility to be aware of, and observe the minimum clearances for both workers and equipment operating near high voltage electric lines set out in the High-Voltage Electrical Safety Orders of the California Division of Industrial Safety (<https://www.dir.ca.gov/Title8/sb5g2.html>), as well as any other safety regulations. Contractors shall comply with California Public Utilities Commission General Order 95 ([http://www.cpuc.ca.gov/gos/GO95/go\\_95\\_startup\\_page.html](http://www.cpuc.ca.gov/gos/GO95/go_95_startup_page.html)) and all other safety rules. No construction may occur within 25 feet of PG&E's towers. All excavation activities may only commence after 811 protocols has been followed.

Contractor shall ensure the protection of PG&E's towers and poles from vehicular damage by (installing protective barriers) Plans for protection barriers must be approved by PG&E prior to construction.

13. PG&E is also the owner of distribution facilities throughout many of the areas within the state of California. Therefore, any plans that impact PG&E's facilities must be reviewed and approved by PG&E to ensure that no impact occurs that may endanger the safe and reliable operation of its facilities.





# CONTRA COSTA COUNTY FIRE PROTECTION DISTRICT

4005 PORT CHICAGO HWY, STE 250, CONCORD, CA 94520 • (925) 941-3300 • CCCFPD.ORG

March 20, 2024

Ms. Zoe Merideth  
City of Antioch  
Planning Division

**Subject:** Wildflower Townhomes II  
2500 Wildflower Station Place  
Project # TM-02, AR-23-05  
**CCCFPD Project No.: P-2024-000721**

Dear Ms. Merideth:

We have reviewed the design review application to establish 19 three-story buildings containing 159 new condominium units at the subject location. The following is required for Fire District approval in accordance with the 2022 California Fire Code (CFC), the 2022 California Building Code (CBC), the 2022 California Residential Code (CRC), and Local and County Ordinances and adopted standards:

1. Access as shown on plans complies with Fire District requirements.

Provide emergency apparatus access roadways with all-weather (paved) driving surfaces of not less than 20-feet unobstructed width, and not less than 13 feet 6 inches of vertical clearance, to within 150 feet of travel distance to all portions of the exterior walls of every building. Access shall have a minimum outside turning radius of 45 feet, and must be capable of supporting the imposed fire apparatus loading of 37 tons. (503) CFC

Aerial Fire Apparatus Access is required where the vertical distance between grade plane and the highest roof surface exceeds 30 feet as measured in accordance with Appendix D, Section 105 of the 2022 CFC. Aerial access roads shall have a minimum unobstructed width of 26 feet, exclusive of shoulders, in the immediate vicinity of the building or portion thereof. At least one of the required routes shall be located within a minimum of 15 feet and a maximum of 30 feet from the building, and shall be positioned parallel to one entire side of the building. Overhead utility and power lines shall not be located over the aerial fire apparatus access road or between the aerial fire apparatus road and building.

2. Access roadways of **less than 28-feet** unobstructed width shall have signs posted or curbs painted red with the words: **NO PARKING – FIRE LANE** clearly marked. (22500.1) CVC, (503.3) CFC

Access roadways of **28 feet or greater, but less than 36-feet** unobstructed width shall have **NO PARKING – FIRE LANE** signs posted, allowing for parking on one side only or curbs painted red with the words **NO PARKING – FIRE LANE** clearly marked. (22500.1) CVC, (503.3) CFC

3. The developer shall provide an adequate and reliable water supply for fire protection with a minimum fire flow of 2125 GPM. Required flow must be delivered from not more than 2 hydrants



flowing simultaneously for a duration of 120 minutes while maintaining 20-pounds residual pressure in the main. (507.1), (B105) CFC

4. The developer shall provide hydrants of the East Bay type in compliance with Chapter 5 and Appendix B and C of the California Fire Code. The proposed locations are acceptable to the Fire District. (C103.1) CFC
5. **Provide emergency escape and rescue openings in Group R occupancies of type V construction.** Basements and sleeping rooms below the fourth story above grade plane shall have at least one exterior emergency escape and rescue opening. Such openings shall open directly into a public way or to a yard or court that opens to a public way.

Landscaping, signage and other obstructions must not hinder the positioning of firefighting ground ladders from apparatus access to the rescue windows.

**A ground ladder access exhibit will be required with Land Development submittal.**

6. A land development permit is required for access and water supply review and approval prior to submitting building construction plans.

The developer shall submit scaled site improvement plans indicating:

All existing or proposed hydrant locations,  
Fire apparatus access to include slope and road surface  
Aerial fire apparatus access,  
Elevations of building,  
Size of building and type of construction,  
Gates, fences, retaining walls, bio-retention basins, any obstructions to access.  
Detail showing the lowest level of fire department vehicle access and the floor level of the highest occupied floor,  
Striping and signage plan to include "NO PARKING-FIRE LANE" markings  
Provide drawings for paths from the public way to under emergency escape and rescue openings showing a proposed clear path and clear space under these openings that allow for the placement of ground ladders at a climbing angle of 70 to 75 degrees and a minimum of 18" clearance from the base of the ladder to any obstruction (see attached ground ladder access standard) for review and approval prior to obtaining a building permit.

**This is a separate submittal from the building construction plans. These plans shall be approved prior to submitting building plans for review.** (501.3) CFC

7. **Emergency apparatus access roadways and hydrants shall be installed, in service, and inspected by the Fire District prior to construction or combustible storage on site.** (501.4) CFC

**Note:** A temporary aggregate base or asphalt grindings roadway is not considered an all-weather surface for emergency apparatus access. The first lift of asphalt concrete paving shall be installed as the minimum roadway material and must be engineered to support the designated gross vehicle weight of 37 tons.

8. The buildings as proposed shall be protected with an approved automatic fire sprinkler system complying with the 2022 edition of NFPA 13. Submit to this office for review and approval prior to installation. (903.2) CFC, Contra Costa County General Plan / Contra Costa County Ordinance 2022-34.



9. New buildings shall have approved radio coverage for emergency responders. An emergency responder radio coverage system shall be installed when the conditions of CFC 510.4.1 are not met. Testing shall be conducted and the results submitted to the Fire District prior to the building final. (510.1) CFC
10. The developer shall provide traffic signal pre-emption systems (Opticom) on any new or modified traffic signals installed with this development. (21351) CVC
11. Flammable or combustible liquid storage tanks shall **not** be located on the site without obtaining approval and necessary permits from the Fire District. (3401.4) CFC
12. The owner shall cut down and remove all weeds, grass, vines, or other growth that is capable of being ignited and endangering property. (304.1.2) CFC
13. The owner or the owner's authorized agent shall be responsible for the development, implementation and maintenance of a written plan in compliance with NFPA 241, establishing a fire prevention program at the project site applicable throughout all phases of the construction. The plan shall be made available for review by the fire code official upon request. (Ch.33) CFC

The fire prevention program superintendent shall develop and maintain an approved prefire plan in cooperation with the fire chief. The fire chief and fire code official shall be notified of changes affecting the utilization of information contained in such prefire plans. (Ch.33) CFC

14. The developer shall submit building construction plans and specifications for the subject project to the through the Fire District public portal (<https://confire.vision33cloud.com/citizenportal/app/landing>). After the new construction / tenant improvement plans are approved, plans and specifications for all deferred submittals shall be submitted, including, but not limited to the following.

- Private underground fire service water mains
- Fire sprinklers
- Standpipe
- Fire alarm
- Fire pump if required
- Energy systems
- Emergency generator if required
- Photovoltaic
- Emergency Responder Radio Coverage System (ERRCS)

**All plan submittals shall be submitted to the through the Fire District public portal** (<https://confire.vision33cloud.com/citizenportal/app/landing>) for review and approval **prior** to construction of the building or installation of the systems to ensure compliance with minimum requirements related to fire and life safety. Plan review and inspection fees shall be submitted at the time of plan review submittal. (105.4.1) CFC, (901.2) CFC, (107) CBC

**TO SCHEDULE A FIRE DISTRICT INSPECTION OF THE ACCESS AND HYDRANT INSTALLATION PRIOR TO CONSTRUCTION OR THE STORAGE OF COMBUSTIBLE MATERIAL ON THE JOB SITE, CONTACT THE FIRE DISTRICT (MINIMUM 2 WORKING DAYS IN ADVANCE) AT 925-941-3300 EXT 3902 OR SCHEDULE THROUGH THE FIRE DISTRICT PUBLIC PORTAL UNDER THE CORRECT PERMIT NUMBER.**

**<https://confire.vision33cloud.com/citizenportal/app/landing>**

Our preliminary review comments shall not be construed to encompass the complete project. Additional plans and specifications may be required after further review.



If you have any questions regarding this matter, please contact this office at (925) 941-3300.

Sincerely,



Michael Cameron  
Fire Inspector

File: 2500 WILDFLOWER STATION PL-PLN-P-2024-000721



## ATTACHMENT "D"

**Villagrana, Monique**

---

**From:** Achal Maharaj <[REDACTED]>  
**Sent:** Friday, February 2, 2024 8:17 PM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

**This is a public comment for the project:**

**Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016**

Hello Commissioners,

I'm a resident owner at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

As a current owner and resident, I am very concerned not only for my safety but safety of all the residents/tenants that live here. There's been a lot of car break-ins, along with vandalism, as well as strange people just walking up to people's homes or sleeping in their front patio. We also have people that do side shows use our driveway as a means of getting ready to go start these illegal car shows. If gates were available, it will become a barrier for them and prevent them from using our community's driveways and perhaps it will help slow down these illegal car shows. I ask if you can please understand our concerns and the safety of everyone that lives here.

Kindly,

Achal Singh

Sent from my iPhone



## Villagrana, Monique

---

**From:** Aurel Draï <[REDACTED]>  
**Sent:** Friday, February 2, 2024 7:39 AM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

This is a public comment for the project:

Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016

Aurelien Draï  
[REDACTED]

Antioch, CA 94531

Hello Commissioners,

I'm a resident owner at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

It is extremely important for the entire community (the current one and the new project) to be gated due to the lack of safety and increased crime in the area. In the past few years since moving into the Wildflower Condos, we've had countless car break-ins, trespassing, theft, high-speed vehicles using the community as a shortcut, vandalism, to name a few. These incidents compromise our security and our quality of life as Antioch residents. Therefore, having a gated community would extremely benefit us.

Kindly,

Aurelien Draï  
[REDACTED]



## Villagrana, Monique

---

**From:** Brenda Gonzalez <[REDACTED]>  
**Sent:** Saturday, February 3, 2024 7:25 AM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

**This is a public comment for the project:**

**Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016**

**Brenda Gonzalez**  
[REDACTED]

**Antioch, CA 94531**

Hello Commissioners,

I'm a resident owner at the original Wildflower Condos. I am writing to you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

There have been several incidents that have led me to not feel safe in my own home such as my car being stolen from my parking spot. I strongly advocate for action to be taken to help secure and improve our living space.

In Community Spirit,

Brenda Gonzalez

--

**Brenda Gonzalez, MS**

Puente Co-Coordinator/General Counselor

Diablo Valley College

Pronouns: She/Her/Ella  
[REDACTED]



## Villagrana, Monique

---

**From:** Calvin Douglas <[REDACTED]>  
**Sent:** Monday, February 5, 2024 9:30 PM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

This is a public comment for the project:

Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016

Calvin Douglas  
[REDACTED]  
Antioch, CA 94531

Hello Commissioners,

I'm a resident owner at the original Wildflower Station Condos. I am writing to you today to further reinforce the public comment submitted by our HOA Board (The Landing at Wildflower Station) requesting the fence and gate be a required part of the new project approval.

I've been a proud resident of Antioch since 1999. I have been a resident of this development since April of 2021, with this being my first home purchase. Since living in Antioch, I have seen the crime rate increase dramatically. Being employed as a Police Officer in San Francisco, I understand the staffing crisis for law enforcement. While during this staffing crisis, I also understand criminals are taking advantage of the low staffing to commit a number of crimes, mainly property crimes. Understanding that it will take time for law enforcement staffing levels to rise, I feel it would be important to protect this development with security fencing and gates. Since living in this development, I have been a victim of vehicle theft twice (APD Case #23-002438 and Case #23-006004). Most recently on 01/20/2024, I observed subjects attempting to steal my car for the third time. As a result of this observation, I was able to run the subjects away, without taking my vehicle, but the fleeing subjects fired numerous gunshots in the air prior to exiting the complex (APD Case #24-000519).

I have witnessed the development (residential communities, population, restaurants, etc) of Antioch and still believe it has the potential to be great. Aside from the large amount of money spent on homes, citizens of Antioch just want to feel safe. With that in mind, I respectfully request for the addition of security fencing and gates to be required in Wildflower Townhomes 2 Project. I also respectfully request for you to consider making it a requirement for future developers to add security fencing/gates to future residential communities.

Kindly,

Calvin Douglas



## Villagrana, Monique

---

**From:** Cat Ramos <[REDACTED]>  
**Sent:** Friday, February 2, 2024 3:57 PM  
**To:** Planning Division  
**Subject:** Wildflower Station Place Antioch CA

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

**Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016**

Catherine Ramos

[REDACTED]  
Antioch ca 94531

Hello Commissioners,

I'm a resident owner/renter at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

We also have a lack of safety due to potential of increased crime, car break-ins, non-community residents speeding through our street to cut through traffic on Hillcrest Ave, unhoused individuals loitering, doing drugs.

Catherine Ramos



## Villagrana, Monique

---

**From:** Earnesta Lewis <[REDACTED]>  
**Sent:** Friday, February 2, 2024 9:09 AM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Earnesta and Anthony Lewis

[REDACTED]  
Antioch. CA. 94531

February 2, 2024

Dear Antioch Staff, Commissioners, Council Members, and Mayor;

Subject: We are seeking your support in our request to make the approval of the Wildflower Station 2 residential project contingent on the “required” construction of a fence and gate around both the new development and the first and current Wildflower Station 1 development.

I’ve been a resident of The Landing at Wildflower Station Place for 3 years, and was the first resident to move in. We have seen and been the victims of theft 3 times and have witnessed the issues of the community. Some of the issues we face are:

- 1 Trespassing
- 2 Unhoused individuals destroying property, stealing property, and sleeping on private patios.
- 3 Porch property theft
- 4 Vehicle thefts and break-ins
- 5 Home burglaries including forced entry.
- 6 High-speed vehicles endangering residents.
- 7 Vandalism
- 8 Common hiding, organizing, and spectating location for car sideshows.

These incidents have not only compromised our sense of security but have also detracted from the quality of life that our community member services. The problems our community face will likely become exacerbated with the increased ability to hide, and opportunity provided by more homes. The proposed fencing and gated access would provide a much-needed barrier against these issues, enhancing the safety and privacy of all residents within the involved properties.

In conclusion, we plead with the City to consider our request as a necessary condition for the approval of the new residential project.



Thank you for considering our concerns and request. We look forward to a positive response and the opportunity to speak to you.

Thanks,

Earnesta and Anthony Lewis



## **Villagrana, Monique**

---

**From:** Ellaine Joyce Bullecer <[REDACTED]>  
**Sent:** Friday, February 2, 2024 8:19 PM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

**This is a public comment for the project:**

**Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016**

Hello Commissioners,

I'm a resident owner at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

It's been very rough for everyone who lives in the community dealing with homeless people and non residents speeding through our street and worse doing car break ins and loitering. I myself have been a victim of property theft last year.

Please help us require DeNova Homes to keep the current and future community residents by providing us with the fence and gate system.

Kindly,

Ellaine Bullecer



**Villagrana, Monique**

---

**From:** Elsie Baluyut <[REDACTED]>  
**Sent:** Friday, February 2, 2024 10:19 AM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

**This is a public comment for the project:**

**Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016**

Elsie M. Baluyut  
[REDACTED]  
Antioch, CA 94531

Hello Commissioners,

I'm a resident owner at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

Since moving in on November 2021, there have been multiple occasions where a transient has sat on my porch for a few hours while I was not home and came back late in the night to sleep on my porch. Another person also peeked through my glass front door even when the indoor lights were on. Non-community residents speeding through our street to cut through traffic on Hillcrest. We have had multiple car break-ins and thefts. There are safety concerns from our entire community.

Kindly,

Elsie M. Baluyut



## Villagrana, Monique

---

**From:** Emerson Lagpacan <[REDACTED]>  
**Sent:** Friday, February 2, 2024 3:22 PM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

**This is a public comment for the project:**

**Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016**

Emerson Lagpacan  
[REDACTED]

Antioch CA 94531

Hello Commissioners,

**I'm a resident owner/renter at the original Wildflower Condos. I am writing to you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.**

As a resident, I am deeply concerned about the escalating security issues we are facing due to the influx of homeless individuals and thieves in our area.

The safety and security of our community should be a top priority for all of us. Unfortunately, recent events have highlighted the urgent need for additional measures to protect our homes, families, and property. Incidents of theft and vandalism have become all too common, leaving many of us feeling vulnerable and anxious about the safety of our neighborhood.

By gating our community, we can significantly reduce the risk of unauthorized access and deter criminal activity. Gated communities have been proven to experience lower crime rates compared to open neighborhoods, as they create a physical barrier that makes it more difficult for criminals to enter and escape unnoticed. This added layer of security will not only help to safeguard our property but also enhance the overall quality of life for residents by providing a greater sense of peace and tranquility. Moreover, gating our community will also serve as a deterrent to homeless individuals who may seek shelter or engage in disruptive behavior within our vicinity. While we sympathize with the plight of the homeless, it is essential to prioritize the safety and well-being of our residents. Implementing gating will help to ensure that our community remains a safe and welcoming environment for everyone.

I understand that there may be concerns about the potential impact of gating on traffic flow and accessibility. However, I believe that these concerns can be effectively addressed through careful planning and consultation with traffic engineers and city officials. Measures such as installing access control systems or implementing designated entry and exit points can help to minimize any inconvenience while maximizing security.

In conclusion, I urge the city council to support the HOA's request to make our condominium community gated. The safety and security of our residents should always come first, and gating is a proactive step



towards achieving that goal. Together, we can create a safer, more secure environment for everyone to enjoy.

Thank you for your attention to this matter.

**Kindly,**

Emerson Lagpacan



## Villagrana, Monique

---

**From:** Hugo Fernando Melgar Zúñiga <[REDACTED]>  
**Sent:** Friday, February 2, 2024 1:11 PM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

This is a public comment for the project:

***Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016***

**Hugo Melgar**

[REDACTED]  
Antioch, CA 94531

Hello Commissioners,

I'm a resident owner at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

The city of Antioch is responsible for public safety and currently our condominium is constantly invaded by Homeless, using the common areas of the buildings as accommodation and to consume liquor and drugs, affecting the safety and physical integrity of our families. They also remove garbage from the containers daily and break the bags, scattering their contents, which causes contamination and sources of infection. Reason why it is necessary to implement a fence and a gate to limit access to guarantee the comprehensive security and general well-being of the condominium residents.

Sincerely,

**Hugo Fernando Melgar Zúñiga**  
Electronic & Systems Engineer

United States of America  
Phone: [REDACTED]



## Villagrana, Monique

---

**From:** Jacqueline Estrada [REDACTED] >  
**Sent:** Sunday, February 4, 2024 12:08 PM  
**To:** Planning Division  
**Subject:** Fencing both communities by Denova

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Jacqueline R. Estrada  
[REDACTED]

Hello Commissioners,

I'm a resident owner/renter at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

[Optional - add in additional comments of why it's important to you. For example: lack of safety due to potential of increased crime, car break-ins, non-community residents speeding through our street to cut through traffic on Hillcrest ave., unhoused individuals loitering, doing drugs, etc]

Kindly,

Jacqueline R. Estrada



## Villagrana, Monique

---

**From:** Kathy Rufus <[REDACTED]>  
**Sent:** Sunday, February 4, 2024 12:42 PM  
**To:** Planning Division  
**Subject:** Public comment for wildflower townhomes

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello commissioners:

I'm a resident homeowner at the original Wildflower Station Condos. I am writing to you today (02/03/2024) to further reinforce the public comment submitted by our Collaborative Community Management Solutions HOA and the Board requesting the fence and gate to be required as part of the new project approval.

**Wildflower Townhouses 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-015-016**

The Landing At Wildflower Station Community has been experiencing a homeless population entering our community and walking into our patios, stealing and trashing our new community. We have cars driving very fast through our community using it as a shortcut and causing traffic and concern for our families and children's safety.

We are recommending a gate/fence to be installed for our safety and to protect our families.

I look forward to hearing from you.

Thank you.  
Stanley and Kathy Rufus

[REDACTED]  
Antioch, CA 94531



## Villagrana, Monique

---

**From:** [REDACTED]  
**Sent:** Friday, February 2, 2024 7:09 AM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

**This is a public comment for the project:**

**Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016**

Hello Commissioners,

I'm a resident owner at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval of Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016.

Since my family moved into the Wildflower Station community, our car catalytic converter was stolen, and in another incident, our car was broken into. These incidents caused us undue stress, lost workdays, and costly repairs. These are not isolated cases as our neighbors have also been victims of theft and car break-ins. Moreover, I am really concerned of the safety in the community with non-residents loitering around, and/or speeding through our street to cut through traffic on Hillcrest Ave.

In this regard, I petition the City to consider our request as a necessary condition for the approval of the new residential project.

Thank you for your time and consideration of our concerns and request. I look forward to your favorable response.

Sincerely,  
Leonor Arriola

[REDACTED]  
Antioch, CA 94531-5007



## Villagrana, Monique

---

**From:** Lynnette Warfield [REDACTED]  
**Sent:** Saturday, February 3, 2024 11:52 AM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhouses 2 - East Of Hillcrest Avenue, north side of Wildflower

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello Commissioners:

I'm a resident homeowner at the original Wildflower Station Condos. I am writing to you today (02/03/2024) to further reinforce the public comment submitted by our Collaborative Community Management Solutions HOA and the Board requesting the fence and gate to be required as part of the new project approval.

**Wildflower Townhouses 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-015-016**

The Landing At Wildflower Station Community has been experiencing a homeless population entering our community and walking into our patios, stealing and trashing our new community. We have cars driving very fast through our community using it as a shortcut and causing traffic and concern for our families and children's safety.

We are recommending a gate/fence to be installed for our safety and to protect our families.

I look forward to hearing from you.

Thank you.  
Lynnette Warfield

[REDACTED]  
Antioch, CA 94531



## Villagrana, Monique

---

**From:** Ulloa Tribe [REDACTED] >  
**Sent:** Thursday, February 1, 2024 5:50 PM  
**To:** Planning Division  
**Subject:** PUBLIC COMMENT FOR Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive  
**Attachments:** 2024-02-01 Letter to the City Regarding Townhomes.pdf  
**Follow Up Flag:** Follow up  
**Flag Status:** Completed

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello,

Please submit the attached PDF as a **Public Comment** for the following project:

Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016

Thank you for considering our concerns and request. If possible please share a planned date this project will come before the Planning Commission.

I look forward to a positive response and am available for any further discussion or clarification needed.

Sincerely,

Mauricio Ulloa  
The Landing at Wildflower Station Condo Association Board President  
[REDACTED]



## Villagrana, Monique

---

**From:** Mia Salvosa <[REDACTED]>  
**Sent:** Friday, February 2, 2024 7:30 AM  
**To:** Planning Division  
**Subject:** Fwd: Public Comment for Wildflower Townhomes 2

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

>  
>  
> Wildflower Townhomes 2- East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016  
>  
> Mia Balamiento  
> [REDACTED]  
>  
> Hello Commissioners,  
>  
> I'm a resident owner at the original Wildflower Condos. I am writing to you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.  
>  
> There have been a number of incidents where we have felt unsafe, scared, and frustrated. Many of us have had our cars broken into, homeless people sleeping around on our doorsteps, and my son had his wheels stolen from his car. As a resident of the original Wildflower Condos. I want our community to be able to feel safe and sleep at ease. We shouldn't have to worry about what goes on in our front yard every night.  
>  
> Kindly,  
>  
> Mia Balamiento  
>



## Villagrana, Monique

---

**From:** Red Nata [REDACTED] >  
**Sent:** Friday, February 2, 2024 10:03 AM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

This is a public comment for the project: Wildflower Townhomes 2-East of Hillcrest Avenue, north side of Wildflower drive/APNs:  
052-140-013; 052-140-015; 052-140-016

Ms.Natasha Rozenoer  
[REDACTED]  
Antioch, CA 94531  
[REDACTED]

Hello, Commissioners,

I am a resident owner at the original Wildflower condos.

I am writing to you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

This approval is extremely important to me as being a senior person with hearing problems I don't feel safe at home due to potential of increased crime and many homeless individuals loitering and doing drugs around our complex. A year ago someone cut off the catalytic converter from my car and it brought me a lot of troubles with repair. I still get up few times in the middle of the night to look at my car.

Kindly,

Natasha Rozenoer

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## Villagrana, Monique

---

**From:** ramon ramirez [REDACTED] >  
**Sent:** Friday, February 2, 2024 10:15 AM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2 3.

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello Commissioners,

**This is a public comment for the project:**

**Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016**

**From: Ramón Ramirez**

[REDACTED]  
**Antioch Ca 94531**

I'm a resident owner at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

I am asking for it due lack of safety due to potential of increased crime, car break-ins, non-community residents speeding through our street to cut through traffic on Hillcrest ave., unhoused individuals loitering, doing drugs.

Kindly

Ramón Ramirez



## Villagrana, Monique

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**From:** Salve Maduli <[REDACTED]>  
**Sent:** Thursday, February 1, 2024 11:41 PM  
**To:** Planning Division

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

This is a public comment for the project:

Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016

Salve Maduli  
[REDACTED]

Antioch Ca.94531

Hello Commissioners,

I'm a resident owner at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

It is very important that this community needs a gate for the main reason that is lack of safety due to potential of increased crime, car break-ins, non-community residents speeding through our street to cut through traffic on Hillcrest ave., unhoused individuals loitering, doing drugs, etc

Kindly,

Salve Maduli  
Sent from my iPhone



## Villagrana, Monique

---

**From:** Shakeel Hai <[REDACTED]>  
**Sent:** Friday, February 2, 2024 8:45 AM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

This is a public comment for the project:

Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-01

Hello Commissioners,

I'm a resident owner at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

The lack of security/safety due to potential of increased crime, car break-ins, non-community residents speeding through our street to cut through traffic on Hillcrest ave., unhoused individuals loitering, doing drugs, etc. has left us wanting more to keep our families safe.

Thank you for your time and consideration

Kindly,  
Shakeel Hai ([REDACTED])



## Villagrana, Monique

---

**From:** Tajinder Singh <[REDACTED]>  
**Sent:** Saturday, February 3, 2024 1:54 PM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

This is a public comment for the project:

Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016

**Name:** Tajinder Singh  
**Address:** [REDACTED], Antioch, California - 94531

Hello Commissioners,

I'm a resident owner at the original Wildflower Condos. I am writing to you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

I just want to emphasize that this is really important to me and my family. Recently we are observing an increased number of crimes in the community and surrounding area, including people speeding through our streets when kids are playing outside, increased number of stolen packages, theft etc. This is really concerning to us and hence would really appreciate it if you include fencing+gate into consideration when reviewing/approving the project.

Thank you so much.

Best,  
Tajinder Singh



## Villagrana, Monique

---

**From:** Vicki Williams [REDACTED] >  
**Sent:** Friday, February 2, 2024 4:36 PM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

This is a public comment for the project:

Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016

Vicki Williams  
[REDACTED]

Antioch, CA 94531

Hello Commissioners,

I'm a resident owner/renter at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

A fence is critical to keeping our community as well as the new community safe from increased crime, car break-ins, non-community residents speeding through to cut traffic on Hillcrest ave, unhoused individuals loitering and doing drugs as well as illegal dumping that has costed us thousands.

Our area is repeatedly targeted and DeNova needs to take these critical steps to ensure safety as well as profits when it comes to their developments which they have not this far.

Kindly,

Vicki



## Villagrana, Monique

---

**From:** Vicki Williams <[REDACTED]>  
**Sent:** Friday, February 2, 2024 4:39 PM  
**To:** Planning Division  
**Subject:** Re: Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello,

In addition, I want to add that our property is being used among crowds for illegal sideshows and vandalized and abandoned cars are also left on the property which is why there is an urgent need to have these communities gate.

Thank you.

Vicki

On Feb 2, 2024, at 4:36 PM, Vicki Williams <[REDACTED]> wrote:

This is a public comment for the project:

Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016

Vicki Williams

[REDACTED]  
Antioch, CA 94531

Hello Commissioners,

I'm a resident owner/renter at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

A fence is critical to keeping our community as well as the new community safe from increased crime, car break-ins, non-community residents speeding through to cut traffic on Hillcrest ave, unhoused individuals loitering and doing drugs as well as illegal dumping that has costed us thousands.

Our area is repeatedly targeted and DeNova needs to take these critical steps to ensure safety as well as profits when it comes to their developments which they have not this far.

Kindly,

Vicki



## Villagrana, Monique

---

**From:** Victoria Vargas <[REDACTED]>  
**Sent:** Friday, February 2, 2024 10:05 AM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

This is a public comment for the project:

**Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016**

Victoria Vargas  
[REDACTED]

Hello Commissioners,

I'm a resident owner at the original Wildflower Condos. I am writing you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

I bought my condo at the end of 2021 and since then I have experienced three car break-ins, suspicious people coming to my door and have felt unsafe at times to just even go throw away my trash. Because this property is open to the public from two sides, people constantly drive through our community at fast speeds, they loiter, and dump their trash into our bins causing a mess and money for our community. When recent sideshows occurred in the hillcrest/deer valley intersection, tens of cars came and parked in our parking lot to see the sideshow and were drinking. Additionally, suspicious people have been seen looking through windows to try and steal, transients doing drugs on the sidewalks, and break-ins into our gym. I do not feel safe leaving early in the morning while it's still dark to go to work and I shouldn't have to feel that way.

I assure you that the new residents of this proposed community will want a gated community, especially if they knew all the events that have transpired since this community was built. I wouldn't want them feeling unsafe or feeling like it's not a place to raise their families. I hope you consider this comment when determining whether a fenced community is necessary for this new project.

Sincerely,

**Victoria Vargas**



Mauricio Ulloa

Antioch, CA 94531

February 1, 2024

City of Antioch Planning Commission  
P.O. Box 5007  
Antioch, CA 94531-5007

Dear Antioch Staff, Commissioners, Council Members, and Mayor,

**Subject: We are seeking your support in our request to make the approval of the Wildflower Station 2 residential project contingent on the “required” construction of a fence and gate around both the new development and the first and current Wildflower Station 1 development.**

I am writing on behalf of the 98 families/residents of Wildflower Station Condominium Association, located at the corner of Wildflower and Hillcrest. Our community, initially part of a mixed-use residential-commercial plan, is now facing significant changes with the rezoning of the adjacent land from commercial to residential use and the proposed development of another townhome condominium association. Our situation is further complicated due to the shared private roadway including entrances previously constructed with the intent of sharing with commercial property.

We understand that the applicant has filled their project leveraging the processes under California Senate Bill 330 and understand time is of the essence. Under the stipulations of California Senate Bill 330, we understand that the city still has the authority to impose certain conditions on new housing developments, particularly concerning safety and privacy. We respectfully request that the approval of the new residential project adjacent to our community be made contingent on the construction of a comprehensive fencing system, complete with automated electronic access gates, encompassing both properties.

Our request stems from a series of safety and privacy concerns that have escalated over time, including:

- Trespassing
- Unhoused Individuals destroying property, stealing property, and sleeping on private patios.
- Porch property theft
- Vehicle thefts and break-ins
- Home burglaries including forced entry
- High-speed vehicles endangering residents
- Vandalism
- Common hiding, organizing, and spectating location for car sideshows

These incidents have not only compromised our sense of security but have also detracted from the quality of life that our community members deserve. The problems our community face will likely



become exacerbated with the increased ability to hide and opportunity provided by more homes. The proposed fencing and gated access would provide a much-needed barrier against these issues, enhancing the safety and privacy of all residents within the involved properties.

We believe this measure is in line with the city's commitment to ensuring the safety and wellbeing of its residents. The addition of a fence and gate system would serve as a proactive step in preventing future incidents, promoting a secure and peaceful living environment.

In conclusion, we plead with the City to consider our request as a necessary condition for the approval of the new residential project.

Thank you for considering our concerns and request. We look forward to a positive response and are available for any further discussion or clarification needed.

Sincerely,

A handwritten signature in black ink, appearing to read 'M. Ulloa', with a long horizontal flourish extending to the right.

Mauricio Ulloa

The Landing at Wildflower Station Condo Association Board President





## Villagrana, Monique

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**From:** Xiang Ding <[REDACTED]>  
**Sent:** Thursday, February 1, 2024 9:30 PM  
**To:** Planning Division  
**Subject:** Public Comment for Wildflower Townhomes 2

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

This is a public comment for the project:

Wildflower Townhomes 2 - East of Hillcrest Avenue, north side of Wildflower Drive/APNs: 052-140-013; 052-140-014; 052-140-015; 052-140-016

Xiang Ding  
[REDACTED]

Hello Commissioners,

I'm a resident owner/renter at the original Wildflower Condos. I am writing to you today to further reinforce the public comment submitted by our HOA Board requesting the fence and gate be a required part of the new project approval.

There has been lack of safety due to increased crime, car break-ins, non-community residents speeding through our street to cut through traffic on Hillcrest ave., unhoused individuals loitering and doing drugs.

Thank you for your time and consideration.

Kind regards,  
Xiang Ding



## ATTACHMENT "E"

### ADAMS BROADWELL JOSEPH & CARDOZO

A PROFESSIONAL CORPORATION

#### ATTORNEYS AT LAW

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SOUTH SAN FRANCISCO, CA 94080-7037

TEL: (650) 589-1660  
FAX: (650) 589-5062

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SACRAMENTO, CA 95814-4721

TEL: (916) 444-6201  
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THOMAS A. ENSLOW  
KELILAH D. FEDERMAN  
RICHARD M. FRANCO  
ANDREW J. GRAF  
TANYA A. GULESSERIAN  
DARION N. JOHNSTON  
RACHAEL E. KOSS  
AIDAN P. MARSHALL  
ALAURO R. MCGUIRE  
ISABEL TAHIR

#### *Of Counsel*

DANIEL L. CARDOZO  
MARC D. JOSEPH

July 15, 2025

#### **Via Email and Overnight Mail**

City of Antioch Planning Commission  
Kevin Riley, Chair  
Seth Webber, Vice-Chair  
Commissioners Jennifer Perez,  
Robert Martin, Ramesh Suman,  
Cortney L. Jones  
City of Antioch  
200 H Street  
Antioch, CA 94531  
Email: [planning@antiochca.gov](mailto:planning@antiochca.gov)

#### **Via Email Only**

Kevin Scudero, Acting Director  
Community Development Department  
200 H Street  
Antioch, CA 94531  
Email: [planning@antiochca.gov](mailto:planning@antiochca.gov)  
  
Zoe Merideth, Senior Planner  
Email: [zmerideth@antiochca.gov](mailto:zmerideth@antiochca.gov)

Re: **Antioch Planning Commission Hearing, Agenda Item 6-1;  
Wildflower Station Townhomes 2 Multifamily Residential  
Project (TM-02, AR-23-05)**

Dear Chair Riley, Vice-Chair Webber, Commissioners, Mr. Scudero, and Ms. Merideth:

We are writing on behalf of Contra Costa Residents for Responsible Development ("Contra Costa Residents") to provide comments on Agenda Item 6-1, the Wildflower Station Townhomes 2 Multifamily Residential Project (TM-02, AR-23-05) ("Wildflower Townhomes Project" or "Project") proposed by DeNova Homes, Inc. ("Applicant"). The Project consists of a vesting tentative map to create 19 residential lots for 19 townhome buildings, containing 159 residential units total. The Project site is a 10.35 acre undeveloped site located east of Hillcrest Avenue and west of Wildflower Station Place in the City of Antioch ("City").

The City contends that the Project previously has been analyzed under the California Environmental Quality Act<sup>1</sup> ("CEQA") and that further evaluation is not

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<sup>1</sup> Pub. Res. Code ("PRC") §§ 21000 et seq.; 14 Cal. Code Regs. §§ 15000 et seq. ("CEQA Guidelines").



required pursuant to CEQA Guidelines section 15183.<sup>2</sup> Specifically, the City contends that the Project was adequately analyzed in the Antioch Housing, Environmental Hazards, and Environmental Justice Elements Project Draft Environmental Impact Report (“Housing Element EIR”) adopted by the City in February 2023, and that additional environmental review is therefore not required for the Project pursuant to section 15183. These conclusions are set forth in the 15183 Consistency Memorandum, which purports to “determine if project-specific impacts would occur that are not adequately covered in [the Housing Element EIR]. To the extent the Housing Element policies and/or actions substantially mitigate a particular project impact, the impact shall not be considered peculiar, pursuant to 15183(f), thus, eliminating the need for further environmental review.”<sup>3</sup>

The City’s conclusion is not supported by substantial evidence in the record and further CEQA review is required. The City’s reliance on section 15183 to avoid any project-specific environmental review is misplaced, as none of the Project’s specific impacts were studied in the Housing Element EIR or the 15183 Consistency Memorandum, and the 15183 Consistency Memorandum does not provide evidence that the single Housing Element policy applicable to this Project will substantially mitigate any Project-level impact. Moreover, the Project will result in new or more significant impacts that are peculiar to the Project site. As a result, the Planning Commission lacks substantial evidence to approve the Project.

In particular, Contra Costa Residents’ noise expert found that the Project’s construction and operational noise impacts were not analyzed and would exceed applicable significance thresholds. These impacts are peculiar to the Project, were not analyzed in the Housing Element EIR and will not be mitigated by any uniformly applied policies or standards. The City also lacks substantial evidence to conclude that the Project would not result in transportation impacts peculiar to the Project site. Neither the Housing Element EIR nor the 15183 Consistency Memorandum conducts a VMT analysis or provides evidence that the Project possesses characteristics exempting it from a detailed VMT analysis. Finally, neither the Consistency Memorandum nor the Housing Element EIR performed any emissions modeling to determine the scope of potential air quality and public health impacts from the Project’s construction and operational emissions, in violation of CEQA. The City therefore may not properly rely on CEQA Guidelines section 15183 to avoid further environmental review.

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<sup>2</sup> March 2024 Wildflower Townhomes Section 15183 Consistency Memorandum (“15183 Consistency Memorandum”), pg. 1.

<sup>3</sup> *Id.* at pg. 9.



We prepared these comments with the assistance of acoustics, noise, and vibration expert Jack Meighan of Wilson Ihrig.<sup>4</sup> As explained below, the Project will have potentially significant air quality, public health, noise and transportation impacts that are peculiar to the project and were not analyzed at a project-level in the Housing Element EIR, or are more severe than previously analyzed by the City. These impacts are not reduced to less than significant levels by the mitigation measures in the Housing Element EIR or any other standard conditions of approval, and therefore require disclosure and mitigation in a project-level Environmental Impact Report (“EIR”) before the City can consider approval of the Project.

## **I. STATEMENT OF INTEREST**

Contra Costa Residents is an unincorporated association of individuals and labor organizations that may be adversely affected by the potential public and worker health and safety hazards, and the environmental and public service impacts of the Project. The coalition includes the International Brotherhood of Electrical Workers Local 302, Plumbers & Steamfitters Local 159, Sheet Metal Workers Local 104, Sprinkler Fitters Local 483, along with their members, their families, and other individuals who live and work in the City of Antioch and Contra Costa County.

Contra Costa Residents’ individual members live, work, recreate, and raise their families in the City of Antioch and surrounding communities. Accordingly, they would be directly affected by the Project’s environmental, health, and safety impacts. Individual members may also work on the Project itself. They will be first in line to be exposed to any health and safety hazards that exist on site.

Contra Costa Residents also has an interest in enforcing environmental laws that encourage sustainable development and ensure a safe working environment for its members. Environmentally detrimental projects can jeopardize future jobs by making it more difficult and more expensive for businesses and industries to expand in the region, and by making the area less desirable for new businesses and new residents. Indeed, continued environmental degradation can, and has, caused construction moratoriums and other restrictions on growth that, in turn, reduce future employment opportunities.

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<sup>4</sup> Mr. Meighan’s Comments (“Meighan Comments”) and CV are attached hereto as **Attachment A**.



## II. LEGAL BACKGROUND

CEQA has two basic purposes, neither of which has the City satisfied in this case. First, CEQA is designed to inform decision makers and the public about the potential, significant environmental impacts of a project before harm is done to the environment.<sup>5</sup> The EIR is the “heart” of this requirement,<sup>6</sup> and has been described as “an environmental ‘alarm bell’ whose purpose it is to alert the public and its responsible officials to environmental changes before they have reached ecological points of no return.”<sup>7</sup> To fulfill this purpose, the discussion of impacts in an EIR must be detailed, complete, and “reflect a good faith effort at full disclosure.”<sup>8</sup> An adequate EIR must contain facts and analysis, not just an agency’s conclusions.<sup>9</sup>

Second, CEQA directs public agencies to avoid or reduce environmental damage when possible by requiring imposition of mitigation measures and by requiring the consideration of environmentally superior alternatives.<sup>10</sup> CEQA imposes an affirmative obligation on agencies to avoid or reduce environmental harm by adopting feasible project alternatives or mitigation measures to address all potentially significant impacts identified in the agency’s CEQA analysis.<sup>11</sup> Without an adequate analysis and description of feasible mitigation measures, it would be impossible for agencies relying upon an EIR or other environmental document to meet this obligation.

Following preliminary review of a project to determine whether an activity is subject to CEQA, a lead agency is required to prepare an initial study to determine whether to prepare an EIR or negative declaration, identify whether a program EIR, tiering, or other appropriate process can be used for analysis of the project’s environmental effects, or determine whether a previously prepared EIR could be used with the project, among other purposes.<sup>12</sup> CEQA requires an agency to

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<sup>5</sup> Cal. Code Regs., tit. 14, § 15002, subd. (a)(1) (“CEQA Guidelines”); *Berkeley Keep Jets Over the Bay v. Bd. of Port Comm’rs.* (2001) 91 Cal.App.4th 1344, 1354 (“*Berkeley Jets*”); *County of Inyo v. Yorty* (1973) 32 Cal.App.3d 795, 810.

<sup>6</sup> *No Oil, Inc. v. City of Los Angeles* (1974) 13 Cal.3d 68, 84.

<sup>7</sup> *County of Inyo v. Yorty* (1973) 32 Cal.App.3d 795, 810.

<sup>8</sup> CEQA Guidelines, § 15151; *San Joaquin Raptor/Wildlife Rescue Center v. County of Stanislaus* (1994) 27 Cal.App.4th 713, 721-722.

<sup>9</sup> *See Citizens of Goleta Valley v. Bd. of Supervisors* (1990) 52 Cal.3d 553, 568.

<sup>10</sup> CEQA Guidelines, § 15002, subd. (a)(2) and (3); *Berkeley Jets*, 91 Cal.App.4th, at p. 1354; *Laurel Heights Improvement Ass’n v. Regents of the University of Cal.* (1998) 47 Cal.3d 376, 400.

<sup>11</sup> Pub. Resources Code, §§ 21002-21002.1.

<sup>12</sup> CEQA Guidelines, §§ 15060, 15063, subd. (c).



analyze the potential environmental impacts of its proposed actions in an EIR except in certain limited circumstances.<sup>13</sup> A CEQA exemption may be invoked only if expressly authorized by the CEQA statute or guidelines and if there is no possibility of a significant effect on the environment. Exemptions must be narrowly construed and are not to be expanded beyond the scope of their plain language.<sup>14</sup>

CEQA Guidelines Section 15183 provides an exemption for projects which are consistent with the development density established by existing zoning, community plan or general plan policies for which an EIR was certified, *except as necessary to evaluate whether there are project-specific significant impacts which are peculiar to the project or project site*.<sup>15</sup> In relying on section 15183 to approve a project, a lead agency may not forgo further analysis of potentially significant impacts unless it makes certain findings. An agency is required to perform further analysis as to impacts that (1) are peculiar to the proposed project or parcel, (2) were not analyzed as significant effects in a prior EIR for the zoning, community or general plan with which the project is consistent, (3) are potentially significant off-site or cumulative impacts that were not discussed in the prior EIR, or (4) are previously identified significant impacts which, due to substantial new information not known at the time the EIR was certified, are determined to have a more severe impact than discussed in the prior EIR.<sup>16</sup>

Under section 15183(f), an effect of a project on the environment is not considered peculiar to the project or project site if “uniformly applied development policies or standards have been previously adopted ...with a finding that the development policies or standards will substantially mitigate the environmental effect when applied to future projects, unless substantial new information shows that the policies or standards will not substantially mitigate the environmental effect.”<sup>17</sup>

Agency determinations under Guidelines section 15183 are reviewed under the substantial evidence standard.<sup>18</sup> In determining whether an agency’s findings concerning the use of a statutory exemption from CEQA may be upheld, courts

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<sup>13</sup> See, e.g., Pub. Resources Code, § 21100.

<sup>14</sup> *Castaic Lake Water Agency v. City of Santa Clarita* (1995) 41 Cal.App.4th 1257.

<sup>15</sup> 14 CCR § 15183(a).

<sup>16</sup> 14 CCR § 15183(b)(1)-(4).

<sup>17</sup> 14 CCR § 15183(f).

<sup>18</sup> *Lucas v. City of Pomona* (2023) 92 Cal.App.5th 508, 538, citing *Concerned Dublin Citizens v. City of Dublin* (2103) 214 Cal.App.4th 1301, 1311; see also, *Hilltop Group v. County of San Diego* (2024) 99 Cal.App.5th 890, 909-10.



review the administrative record to see that substantial evidence supports each element of the exemption.<sup>19</sup> This includes the determination that “uniformly applied development policies or standards” will substantially mitigate the project’s environmental effects.<sup>20</sup> Agency findings must specifically address the effect of uniform policies and standards on potential environmental impacts.<sup>21</sup>

Section 15168’s two-step inquiry of a program EIR’s applicability to later activities holds that “if a later activity would have effects that were not examined in the program EIR, a new initial study would need to be prepared leading to either an EIR or a negative declaration.” The City insists that, pursuant to sections 15162 and 15183, the Project is within the scope of the program EIR and no subsequent EIR is required. “Whether a later activity is within the scope of a program EIR is a factual question that the lead agency determines based on substantial evidence in the record.”

Here, the Housing Element EIR analyzed impacts at a program level, and did not analyze quantify, or disclose Project-level impacts for issues including transportation, air quality and public health, and noise.

### **III. THE PROJECT IS NOT EXEMPT FROM FURTHER CEQA REVIEW AND AN EIR IS REQUIRED**

The City contends that the Housing Element EIR provides the basis for its determination that no further environmental review of the Project’s impacts is required. The 15183 Consistency Memorandum notes that the Project’s density of 20.05 dwelling units per acre (“du/ac”) is consistent with the development density established in the Housing Element EIR, i.e., 20-25 du/ac, and purports to evaluate whether the Project will have any effects peculiar to the Project or Project site.<sup>22</sup> It goes on to state that “[t]o the extent that the Housing Element policies and/or actions substantially mitigate a particular project impact, the impact shall not be considered peculiar, pursuant to [CEQA Guidelines section] 15183(f), thus, eliminating the requirement for further environmental review.”<sup>23</sup>

However, while the Consistency Memorandum recites the requirements of section 15183, it does not actually analyze whether the Project will have any effects

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<sup>19</sup> *Lucas*, 92 Cal.App.5th at 538.

<sup>20</sup> 14 CCR § 15183(f).

<sup>21</sup> *Hilltop Group*, 99 Cal.App.5th at 918.

<sup>22</sup> 15183 Consistency Memorandum, pg. 9.

<sup>23</sup> *Id.* at pgs. 8-9.



peculiar to the Project or the Project site. Neither the Housing Element EIR nor the 15183 Consistency Memorandum examine the Project-level effects on environmental impacts such as air quality, health risks, transportation and noise. Nor does the Consistency Memorandum identify any “Housing Element policies and/or practices” that apply to the Project to substantially mitigate the Project’s impacts. As discussed below, the City lacks substantial evidence to support the necessary findings to exempt the Project from CEQA review, and the City must prepare and circulate for public review an EIR that analyzes the Project’s potentially significant impacts.

**A. The City Lacks Substantial Evidence to Support its Conclusions with Respect to the Project’s Transportation Impacts**

CEQA requires analysis of a project’s transportation impacts via analysis of the project’s vehicle miles traveled (“VMT”).<sup>24</sup> The Housing Element EIR evaluated the VMT impacts of all of the potential new housing sites in the City (including the site for the Wildflower Townhomes Project) and found a significant impact on VMT.<sup>25</sup> To address these impacts, the Housing Element EIR adopted Mitigation Measure TRANS-1, which provides that individual housing development projects (like this one) *that do not screen out from VMT impacts analysis shall provide a quantitative VMT analysis*.<sup>26</sup> Individual projects which result in a significant VMT impact are required to implement travel demand management measures and physical measures to reduce VMT to a less-than-significant level.<sup>27</sup> The Housing Element EIR lists seven criteria that are used to screen projects out of conducting project-level VMT analysis: (1) CEQA-exempt projects, (2) small projects, (3) local-serving uses, (4) proximity to a major transit stop, (5) projects located in low VMT areas, (6) affordable housing, and (7) transportation projects.<sup>28</sup> These criteria screen out projects from performing a full VMT analysis because projects meeting these criteria are presumed to have less-than-significant VMT impacts absent substantial evidence to the contrary.<sup>29</sup>

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<sup>24</sup> 14 CCR § 15064.3.

<sup>25</sup> Housing Element EIR, pg. IV.B-27.

<sup>26</sup> *Id.*

<sup>27</sup> *Id.*

<sup>28</sup> *Id.*, pgs. IV.B-21—IV.B-22.

<sup>29</sup> *Id.*



The Housing Element EIR makes clear that it did not analyze VMT impacts from individual housing projects like this one. Nor does the 15183 Consistency Memorandum for this Project analyze the Project's VMT impacts. Instead, the City asserts that because the 15183 Consistency Memoranda concludes that the Project qualifies for the Guidelines section 15183 exemption, the Project "screens out" from having to perform a quantitative VMT analysis as required under Housing Element EIR mitigation measure TRANS-1. Specifically, the 15183 Consistency Memorandum states:

"As demonstrated through this 15183 Consistency Memorandum, the proposed project would not result in significant impact that is peculiar to the project or project site, a significant effect that was not identified in the Housing Element EIR, or a substantially more severe significant effect related to transportation beyond what was identified in the Housing Element EIR. Therefore, pursuant to Section 15183 of the CEQA Guidelines, the proposed project qualifies for exemption from further environmental review under CEQA. Because the proposed project would be considered exempt from CEQA, Mitigation Measure TRANS-1 is not applicable."

The City's position is legally and logically flawed. The City's argument employs circular reasoning by claiming that because the Project is exempt from CEQA, it does not need to conduct a VMT analysis, citing the Housing Element EIR screening criteria. This reasoning is fundamentally flawed because the argument's premise ("the Project is exempt from CEQA") *assumes* the conclusion rather than supporting it. The City argues that an exemption from CEQA means an exemption from VMT analysis, but the CEQA exemption itself is predicated on the absence of significant environmental impacts peculiar to the Project, including transportation impacts which a VMT analysis is designed to determine. In other words, the City claims that it need not evaluate the Project's potentially significant VMT impacts because the Project is exempt from CEQA, but the exemption determination itself rests on unsupported assumptions regarding the lack of Project-specific peculiar impacts. The City lacks any evidence to support the conclusion that the Project will not have significant VMT impacts peculiar to the Project or Project site because it performed no Project-specific VMT analysis. By bypassing the VMT analysis this way, the City avoids an analysis that could reveal significant impacts, and preclude the use of the section 15183 exemption. This approach undermines the purpose of CEQA, which is to ensure that potential environmental impacts are identified, disclosed and mitigated.



While the Consistency Memorandum relies solely on the “CEQA Exemption” screening criterion, the Project does not qualify for any of the other screening criteria set out in the Housing Element EIR. The Project is not a “Small Project,” defined as having 10,000 square feet or less<sup>30</sup> of non-residential space or 10 residential units or less. The Project will not consist of “Local-Serving Uses,” as this screening criteria is intended to apply to commercial uses and is not relevant to residential projects.<sup>31</sup> The Project does not qualify for the “Proximity to a Major Transit Stop” VMT screening criteria, as this criteria is limited to the 0.5 mile (walking radius) surrounding the Antioch BART and Antioch Amtrak stations, and the Housing Element EIR found that none of the housing sites analyzed fall within this boundary.<sup>32</sup> The Project does not include any affordable housing, and therefore does not screen out from VMT analysis on that basis. Nor is the Project a “Transportation Project.” Finally, neither the 15183 Consistency Memoranda nor the Housing Element EIR evaluates whether the Project is in a “Low VMT Area,” defined as having home-based VMT per resident at or below 85% of the Antioch citywide average.<sup>33</sup> This determination requires TAZ-level screening using Contra Costa Countywide VMT maps and Travel Demand Model (“CCTA Model”) results.<sup>34</sup> The Housing Element EIR, due to its programmatic scope, did not conduct this screening.<sup>35</sup> It does acknowledge, however, that such analysis may be necessary for individual projects.<sup>36</sup> Similarly, the 15183 Consistency Memoranda omits TAZ-based screening. Therefore, the City has not provided substantial evidence to support a finding that the Project is located in a low VMT area.

As discussed above, the City’s failure to perform a quantitative VMT analysis prevents an understanding of the extent to which the Project’s VMT is expected to exceed the significance threshold, or the nature of mitigation required to reduce such impacts to below the threshold. The City’s conclusion lacks the support of substantial evidence because it neither includes a site-specific VMT analysis nor demonstrates that the Project qualifies for screening under any recognized exemption. Accordingly, the City must prepare a Project-specific EIR that includes a quantitative VMT analysis and appropriate mitigation.

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<sup>30</sup> *Id.*

<sup>31</sup> *Id.* at pg. IV.B-24.

<sup>32</sup> *Id.*

<sup>33</sup> *Id.* at pg. IV.B-22.

<sup>34</sup> *Id.* at pg. IV.B-24.

<sup>35</sup> *Id.*

<sup>36</sup> *Id.*



**B. The Project Will Have Significant, Unmitigated Air Quality and Public Health Impacts That are More Severe than Previously Analyzed**

The City has not performed any emissions modeling to determine potential impacts of Project construction or operations; neither the 15183 Consistency Memorandum nor the Housing Element EIR analyzed any Project site-specific air quality impacts or identified any sensitive receptors near the Project site. Under CEQA Guidelines section 15168(c), these air quality and public health impacts are effects that were not examined in the Housing Element EIR, requiring a new initial study leading to either an EIR or negative declaration. The 15183 Consistency Memorandum includes a cursory discussion of the Project's potential air quality impacts, as follows:

“The proposed project would be consistent with the Housing Element and, thus, was anticipated by the City and considered under the Housing Element EIR analysis. Accordingly, the proposed project would not result in any new significant effects related to air quality. However, the Housing Element EIR requires mitigation measures related to construction emissions of criteria air pollutant emissions from future housing developments (AIR-1), operational emissions of criteria air pollutant emissions from future housing developments (AIR-2), and health risks related to the generation of toxic air contaminants (TACs) and particulate matter (PM) 2.5 microns in diameter (PM<sub>2.5</sub>) during construction and operation of future housing developments (AIR-3a and AIR-3b).”<sup>37</sup>

However, the Consistency Memorandum goes on to state that only the Housing Element EIR's air quality mitigation measure AIR-3a is applicable to this Project.<sup>38</sup> But because the measure is structured so that development projects choose one option or the other, only part of Mitigation Measure AIR-3a is applicable to the project.<sup>39</sup> Specifically, the proposed project will be required to equip all off-road diesel equipment with Tier 4 engines and the Applicant must prepare a Construction Emissions Minimization Plan for all identified reduction measures.<sup>40</sup> Based on the Project's size, location and characteristics, none of the other Housing

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<sup>37</sup> 15183 Consistency Memorandum, pg. 10.

<sup>38</sup> *Id.* at pg. 11.

<sup>39</sup> *Id.*

<sup>40</sup> *Id.*



Element EIR's air quality mitigation measures would apply to this Project. The City then concludes, without any supporting evidence, that "[i]mplementation of Mitigation measure AIR-3a would ensure the proposed project would not result in a significant impact that is peculiar to the project or the project site, would not be identified as a significant effect in the Housing Element EIR, and would not result in a more severe adverse impact than the significant effects previously identified within the Housing Element EIR."<sup>41</sup> This conclusion lacks the support of any evidence, let alone substantial evidence as required by CEQA.

The Housing Element EIR expressly recognized that the use of construction equipment during construction of housing developments like the Project can pose health risks related to the generation of TACs and PM<sub>2.5</sub>.<sup>42</sup> DPM is a known toxic air contaminant ("TAC") carcinogen that contains numerous harmful compounds. Diesel exhaust has been linked to a range of serious health problems including an increase in respiratory disease, lung damage, cancer, and premature death.<sup>43,44,45</sup> Fine DPM is deposited deep in the lungs in the smallest airways and can result in increased respiratory symptoms and disease; decreased lung function, particularly in children and individuals with asthma; alterations in lung tissue and respiratory tract defense mechanisms; and premature death.<sup>46</sup> Exposure to DPM increases the risk of lung cancer. It also causes non-cancer effects including chronic bronchitis, inflammation of lung tissue, thickening of the alveolar walls, immunological allergic reactions, and airway constriction.<sup>47</sup> DPM is a TAC that is recognized by state and federal agencies as causing severe health risk because it

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<sup>41</sup> *Id.*

<sup>42</sup> Housing Element EIR, pgs. IV.C-21—23.

<sup>43</sup> California Air Resources Board, Initial Statement of Reasons for Rulemaking, Proposed Identification of Diesel Exhaust as a Toxic Air Contaminant, Staff Report, June 1998; see also California Air Resources Board, Overview: Diesel Exhaust & Health, <https://ww2.arb.ca.gov/resources/overview-diesel-exhaust-and-health#:~:text=Diesel%20Particulate%20Matter%20and%20Health&text=In%201998%2C%20CARB%20identified%20DPM,and%20other%20adverse%20health%20effects.>

<sup>44</sup> U.S. EPA, Health Assessment Document for Diesel Engine Exhaust, Report EPA/600/8-90/057F, May 2002.

<sup>45</sup> Environmental Defense Fund, Cleaner Diesel Handbook, Bring Cleaner Fuel and Diesel Retrofits into Your Neighborhood, April 2005; [http://www.edf.org/documents/4941\\_cleanerdieselhandbook.pdf](http://www.edf.org/documents/4941_cleanerdieselhandbook.pdf), accessed July 5, 2020.

<sup>46</sup> California Air Resources Board, Initial Statement of Reasons for Rulemaking, Proposed Identification of Diesel Exhaust as a Toxic Air Contaminant, Staff Report, June 1998.

<sup>47</sup> Findings of the Scientific Review Panel on The Report on Diesel Exhaust as adopted at the Panel's April 22, 1998, Meeting.



contains toxic materials, unlike PM<sub>2.5</sub> and PM<sub>10</sub>.<sup>48</sup> Despite the Housing Element EIR's express recognition of the health risks from construction equipment emissions of TACs from construction of projects like this one, the City failed to conduct a quantitative health risk analysis ("HRA") and omitted a comparison of the Project's health risk impacts to the Bay Area Air Quality Management District's ("BAAQMD") threshold of 10 in one million.<sup>49</sup>

The City's omission of an HRA also conflicts with the Office of Environmental Health Hazard Assessment's ("OEHHA") *Risk Assessment Guidelines: Guidance Manual for Preparation of Health Risk Assessments*. These guidelines recommend that all short-term projects lasting at least 2 months assess cancer risks and that exposure from projects lasting more than 6 months should be evaluated for the duration of the project.<sup>50</sup> Here, the Project's construction is expected to take longer than six months.<sup>51</sup> Because the anticipated duration of the Project's construction would exceed the 2-month and 6-month requirements set forth by OEHHA, a quantified HRA under OEHHA guidance should have been prepared to evaluate the Project for its entire duration.

Therefore, based on the current record, the City cannot conclude that implementation of AIR-3a will prevent significant new air quality impacts associated with the Project. As a result, the City may not rely on the section 15183 exemption to approve this Project, and an EIR must be prepared and circulated for public review.

### **C. The City Lacks Substantial Evidence to Support its Conclusions with Respect to the Project's Noise Impacts**

The City has not performed any site-specific analysis of the Project's potential noise impacts. Neither the Housing Element EIR nor the 15183 Consistency

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<sup>48</sup> Health & Safety Code § 39655(a) (defining "toxic air contaminant" as air pollutants "which may cause or contribute to an increase in mortality or in serious illness, or which may pose a present or potential hazard to human health. A substance that is listed as a hazardous air pollutant pursuant to subsection (b) of Section 112 of the federal act (42 U.S.C. Sec. 7412 (b)) is a toxic air contaminant.")

<sup>49</sup> BAAQMD, Regulation 11-18 Reduction of Risk From Air Toxic Emissions at Existing Facilities, p. 5.

<sup>50</sup> OEHHA, *Risk Assessment Guidelines: Guidance Manual for Preparation of Health Risk Assessments*, pgs. 8-18, available at: <https://oehha.ca.gov/air/cmr/notice-adoption-air-toxics-hot-spots-program-guidance-manual-preparation-health-risk-0>.

<sup>51</sup> 15183 Consistency Memo, pg. 11.



Memorandum includes any analysis of ambient noise in the area of the Project site, modeling of the Project's construction or operational noise impacts, or identification of sensitive receptors near the Project site. The 15183 Consistency Memorandum contains no discussion whatsoever regarding whether the Project may have peculiar noise impacts necessitating further CEQA review. Under CEQA Guidelines section 15168(c), these Project-specific noise impacts were not examined in the Housing Element EIR, requiring a new initial study leading to either an EIR or negative declaration.

The Housing Element EIR recognizes that for individual projects like this one, "construction activities could generate exterior noise levels that exceed the City's noise objectives established under General Plan Policy 11.8.2."<sup>52</sup> The Housing Element EIR also states that "[i]ndividual housing developments...would result in a potentially significant impact if they cause a new exceedance of the General Plan noise objectives, or an audible (3.0 dBA) increase in areas where the General Plan noise objectives are already exceeded as the result of existing development."<sup>53</sup> General Plan Policy 11.8.2 (f) requires a detailed noise attenuation study to be prepared by a qualified acoustical engineer to determine appropriate mitigation and ways to incorporate such mitigation into project design and intervention. Finally, the Housing Element EIR points out that General Plan Policy 11.8.2 requires development adjacent to occupied noise sensitive land uses to implement a construction-related noise mitigation plan that should depict the location of construction equipment and how the noise from this equipment will be mitigated during construction through the use of noise reduction methods listed in Policy 11.8.2(o).<sup>54</sup>

Therefore, while the City expressly recognizes that individual housing projects like this one may have significant noise impacts on existing nearby sensitive receptors and requires studies and mitigation to reduce noise impacts, the 15183 Consistency Memo simply assumes without any analysis or evidence that the proposed Project "would not result in new significant impacts or substantially more significant impacts related to" impacts including noise.<sup>55</sup> It does not analyze or even consider whether the Project would "cause a new exceedance of the General Plan noise objectives, or an audible (3.0 dBA) increase in areas where the General Plan noise objectives are already exceeded." Nor does it consider whether Housing

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<sup>52</sup> Housing Element EIR, pg. IV.L-12.

<sup>53</sup> *Id.* at pg. IV.L-10.

<sup>54</sup> *Id.* at pg. IV.L-12.

<sup>55</sup> 15183 Consistency Memorandum, pg. 13.



Element policies and/or actions might substantially mitigate the Project's noise impacts. At a minimum, to demonstrate consistency with the Housing Element EIR, it must consider Project impacts in relation to General Plan Policy noise objectives, and must prepare a construction-related noise mitigation plan depicting the location of construction equipment and how the noise from this equipment will be mitigated during construction. Without any actual analysis, or consideration of any applicable uniformly applied development policies or standards, there is no support whatsoever for the conclusion that the Project will not have peculiar noise impacts.

Moreover, Mr. Meighan provides substantial evidence that the Project will cause new potentially significant noise impacts. First, the 15183 Consistency Memorandum lacks any measurement or disclosure of ambient noise conditions in the area of the Project site. This violates CEQA's requirement that a lead agency consider both the "absolute noise level" associated with a project as well as the increase in the level of noise that will result from a project.<sup>56</sup> This also fails to disclose a potentially significant operational noise impact. Based on information provided in the 15183 Consistency Memoranda, Mr. Meighan estimates the Project's nighttime operational noise levels to be 53 dBA at the Townhomes to the Northeast of the Project site.<sup>57</sup> Mr. Meighan goes on to explain that nighttime noise levels in suburban areas can be as quiet as 40 dBA.<sup>58</sup> When this ambient level is compared to the Project's estimated noise levels, the Project would far exceed the City's significance threshold of 3 dBA over ambient levels.<sup>59</sup> This is a new significant impact that was not addressed in the Housing Element EIR or the 15183 Consistency Memoranda and would not be mitigated even if the Housing Element EIR and General Plan noise policies and standards are applied.<sup>60</sup> In order to mitigate these impacts, Mr. Meighan suggest shielding HVAC units' noise emissions to the southeast.<sup>61</sup>

Second, Mr. Meighan provides substantial evidence that the Project will have significant construction noise impacts. As explained in Mr. Meighan's comments, the City fails to set its own construction noise significance threshold, leaving the

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<sup>56</sup> *Gardiner Farms, LLC v. County of Kern* (2020) 45 CA5th 814, 887, 893; *Keep Our Mountains Quiet v. County of Santa Clara* (2015) 236 CA4th 714, 733.

<sup>57</sup> Meighan Comments, pg. 6.

<sup>58</sup> *Id.*

<sup>59</sup> *Id.*

<sup>60</sup> *Id.* at pgs. 2-3.

<sup>61</sup> *Id.* at pg. 6.



City's conclusion unsupported by any meaningful standard of evaluation.<sup>62</sup> Because of this, Mr. Meighan's analysis assumes a significance threshold of 75 dBA, which comes from the Los Angeles Municipal Code.<sup>63</sup> Based on information provided in the 15183 Consistency Memoranda, Mr. Meighan estimates the Project's construction noise levels to range from 80 dBA to 85 dBA.<sup>64</sup> This exceeds the significance threshold by at least 5 dBA, which is a new significant impact that the Housing Element EIR and the 15183 Consistency Memoranda did not address. To reduce this impact, Mr. Meighan proposes the use of a temporary soundwall for the Project.<sup>65</sup>

Third, Mr. Meighan explains that the City fails to conduct a screening level construction vibration analysis as required by the Housing Element EIR.<sup>66</sup> The Housing Element EIR states that "if sensitive receptors are located within these buffer distances [about 115 feet], future developments under the Project should prepare a screening level vibration analysis for City review in accordance with General Plan Policy 11.8.2(k)."<sup>67</sup> Despite the closest structures from the Project site being only 90 feet away, the City does not conduct this analysis. Mr. Meighan explains that a thorough evaluation of the Project's potential vibration impacts is "vital" because, if those impacts are significant, they could interfere with people's daily lives and potentially damage nearby homes.<sup>68</sup>

For the above reasons, the City cannot rely on the 15183 CEQA exemption. The City must prepare an EIR that adequately analyzes the Project's potentially significant noise impacts by establishing ambient noise levels for the Project site, comparing them against applicable noise significance thresholds, and proposing mitigation for any significant impacts found.

#### **IV. THE CITY LACKS SUBSTANTIAL EVIDENCE TO MAKE THE REQUIRED FINDINGS TO APPROVE THE PROJECT'S VESTING TENTATIVE SUBDIVISION MAP**

The Project requires the City to approve a Vesting Tentative Subdivision Map ("VTSM") for condominium purposes that would subdivide the project site for

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<sup>62</sup> *Id.*

<sup>63</sup> *Id.* at pg. 5.

<sup>64</sup> *Id.*

<sup>65</sup> *Id.*

<sup>66</sup> *Id.* at pg. 6.

<sup>67</sup> *Id.*

<sup>68</sup> *Id.*



the development of 19 townhome buildings, containing a total of 159 residential units.<sup>69</sup> However, as discussed above, the City fails to adequately analyze or mitigate several new project-specific environmental impacts that were not addressed by the Housing Element EIR. As a result, the City cannot make the requisite findings to approve the Project's VTSM.

California's Subdivision Map Act precludes the approval of a tentative map where the design or improvement of the proposed subdivision is not consistent with the applicable general plan, is likely to cause substantial environmental damage, or is likely to cause serious public health problems.<sup>70</sup>

Additionally, Antioch Municipal Code Section 9-4.323 states that a VTSM may be made conditional or denied if any of the following is determined:

- A failure to do so would place the residents of the subdivision or the immediate community, or both, in a condition dangerous to their health or safety, or both; or
- The condition or denial is required in order to comply with state or federal laws.

As detailed in our comments and those of our noise expert, there is substantial evidence that the Project may result in several potentially significant environmental impacts, including: (1) construction and operational noise, (2) VMT, and (3) air quality and related health risks. These impacts remain unaddressed and could pose serious risks to public health and safety—both for future subdivision residents and the surrounding community. Therefore, the City cannot make the required findings under the Subdivision Map Act and Antioch's Municipal Code to approve the VTSM until all of the Project's potentially significant impacts are thoroughly analyzed and effectively mitigated.

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<sup>69</sup> Staff Report for the Antioch Planning Commission Regular Meeting of July 16, 2025, pg. 1

<sup>70</sup> Government Code § 66474(b), (e) and (f).



## V. CONCLUSION

As discussed herein, the City lacks substantial evidence to rely on a CEQA Guidelines section 15183 exemption for Project approval. The Project will result in potentially significant project-level impacts which are peculiar to the Project and Project site and will require mitigation. Therefore, the Project cannot be approved until the City complies with CEQA by preparing an EIR.

Sincerely,



Alaura McGuire

Attachment  
ARM:acp



# **EXHIBIT A**





WI #24-001.32

July 12, 2025

Alaura R. McGuire  
Adams Broadwell Joseph & Cardozo  
601 Gateway Boulevard, Suite 1000  
South San Francisco, CA 94080

**SUBJECT: Wildflower 2 Section 15183 Consistency Memorandum  
Antioch, CA  
Review and Comments on the Initial Study Noise Analysis**

Dear Ms. McGuire,

As requested, we have reviewed the information and noise impact analysis for the Wildflower 2 Townhomes Project in Antioch, CA. The project consists of construction and operation/occupancy of 168 townhome units along Hillcrest Avenue, northeast of the intersection with Davidson Drive and Deer Valley Road. This letter is based on the Section 15183 Consistency Memorandum prepared by Raney Planning and Management, dated March 2024. The site is surrounded by noise-sensitive receivers, most notably existing townhomes to the northeast of the site along Wildflower Station Place.

Wilson Ihrig is an acoustical consulting firm that has practiced exclusively in the field of acoustics since 1966. During our 58 years of operation, we have prepared hundreds of noise studies for Environmental Impact Reports and Statements. We have one of the largest technical laboratories in the acoustical consulting industry. We also utilize industry-standard acoustical programs such as Roadway Construction Noise Model (RCNM), SoundPLAN, and CadnaA. In short, we are well qualified to prepare environmental noise studies and review studies prepared by others.

### Adverse Effects of Noise<sup>1</sup>

Although the health effects of noise are not taken as seriously in the United States as they are in other countries, they are real and, in many parts of the country, pervasive.

**Noise-Induced Hearing Loss.** If a person is repeatedly exposed to loud noises, he or she may experience noise-induced hearing impairment or loss. In the United States, both the Occupational Health and Safety Administration (OSHA) and the National Institute for Occupational Safety and

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<sup>1</sup> More information on these and other adverse effects of noise may be found in *Guidelines for Community Noise*, eds B Berglund, T Lindvall, and D Schwela, World Health Organization, Geneva, Switzerland, 1999. (<https://www.who.int/publications/i/item/a68672>)



Health (NIOSH) promote standards and regulations to protect the hearing of people exposed to high levels of industrial noise.

**Speech Interference.** Another common problem associated with noise is speech interference. In addition to the obvious issues that may arise from misunderstandings, speech interference also leads to problems with concentration fatigue, irritation, decreased working capacity, and automatic stress reactions. For complete speech intelligibility, the sound level of the speech should be 15 to 18 dBA higher than the background noise. Typical indoor speech levels are 45 to 50 dBA at 1 meter, so any noise above 30 dBA begins to interfere with speech intelligibility. The common reaction to higher background noise levels is to raise one's voice. If this is required persistently for long periods of time, stress reactions and irritation will likely result.

**Sleep Disturbance.** Noise can disturb sleep by making it more difficult to fall asleep, by waking someone after they are asleep, or by altering their sleep stage, e.g., reducing the amount of rapid eye movement (REM) sleep. Noise exposure for people who are sleeping has also been linked to increased blood pressure, increased heart rate, increase in body movements, and other physiological effects. Not surprisingly, people whose sleep is disturbed by noise often experience secondary effects such as increased fatigue, depressed mood, and decreased work performance.

**Cardiovascular and Physiological Effects.** Human's bodily reactions to noise are rooted in the "fight or flight" response that evolved when many noises signaled imminent danger. These include increased blood pressure, elevated heart rate, and vasoconstriction. Prolonged exposure to acute noises can result in permanent effects such as hypertension and heart disease.

**Impaired Cognitive Performance.** Studies have established that noise exposure impairs people's abilities to perform complex tasks (tasks that require attention to detail or analytical processes) and it makes reading, paying attention, solving problems, and memorizing more difficult. This is why there are standards for classroom background noise levels and why offices and libraries are designed to provide quiet work environments.

## Introduction

Under Section 15183 of the California Code, a project that is consistent with development density established by a General Plan for which an EIR has been certified is exempt from CEQA review except for project-specific impacts peculiar to the project or project site. Per 15183(f), an impact is not considered peculiar if uniformly applied development standards or policies have been previously adopted by the City with a finding that the development standards or policies would substantially mitigate the impact when applied to future projects, unless substantial new information shows otherwise.

According to the Consistency Memorandum, the City considered uniformly applied development standards and policies in the Draft Environmental Impact Report entitled Antioch Housing, Environmental Hazards, and Environmental Justice (EJ) Elements (Housing Element DEIR)<sup>2</sup>. None of these policies is mentioned in the Consistency Memorandum, and there is no evidence that they would mitigate the Project's noise impacts in any event. The Housing Element DEIR's operational

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<sup>2</sup> [https://www.antiochca.gov/fc/community-development/planning/housing-element/DHEIR-DEIR\\_22\\_0902.pdf](https://www.antiochca.gov/fc/community-development/planning/housing-element/DHEIR-DEIR_22_0902.pdf)



noise section states “General Plan Policy 11.8.2 (f) requires a detailed noise attenuation study to be prepared by a qualified acoustical engineer to determine appropriate mitigation and ways to incorporate such mitigation into project design and implementation.” And that “compliance with Code of Ordinance 9-5.1901 (A) and General Plan Policy 11.8.2 (f) would ensure that future development under the Project would not result in a substantial temporary or permanent increase in ambient noise levels from stationary sources, and this impact would be less than significant” (Housing Element DEIR page IV.L-13).

Similarly, for construction noise, General Plan Policy 11.8.2 “requires development adjacent to occupied noise sensitive land uses to implement a construction-related noise mitigation plan and requires that all construction equipment utilize noise reduction features” Additionally, the construction-related noise mitigation plan should “depict the location of construction equipment and how the noise from this equipment will be mitigated during construction through the use of noise reduction methods” that are listed in General Plan Policy 11.8.2 (m) (DEIR page IV.L-12).

General Plan 11.8.2 (f) and (m) proceed to list several design guidelines that reduce noise. None of these are guaranteed to reduce noise, depending on the unique characteristics of each site. In Section m, mufflers are already included in construction noise source models of the cited FTA database, which takes its source values from measurements of modern equipment already equipped with mufflers. Nighttime construction restrictions do not mitigate daytime noise levels. Strategic staging will reduce the length of unnecessary noise impacts, but will not mitigate the worst-case construction noise scenarios when necessary activities occur adjacent to sensitive uses. Similarly for section f, most of these best practices do not mitigate worst-case noise and are already included in modeling assumptions. Strategic project design and orientation will reduce some potential impacts. However, this does not preclude there being operational noise impacts, either due to the constraints set by the geometry of each individual site plan, or project orientation that was set by considerations other than efficiency of reducing on-site noise.

The Housing Element DEIR establishes that the general plan will reduce noise to less than significant if followed, but the same DEIR cites the general plan requirements that projects which can result in the “development of proposed uses could result in a significant increase in noise a detailed noise attenuation study to be prepared by a qualified acoustical engineer to determine appropriate mitigation and ways to incorporate such mitigation into project design and implementation” (DEIR page IV.L-13). Those steps have not been taken here, and we believe that this project has the potential to result in a significant increase in noise and vibration, and thus a detailed analysis is required, as detailed in this letter.

### **Construction Noise Impacts are Potentially Significant.**

To estimate construction noise, the Federal Highway Administration’s Roadway Construction Noise Model (RCNM)<sup>3</sup> was used for this analysis. Typically, multiple pieces of equipment are used in a construction noise analysis, based on a realistic estimation of a construction environment where multiple activities occur simultaneously. Up to three pieces of equipment were modeled at once as a conservative estimate, based on typical construction procedures and timelines. The one exception to this was pile driving, which is a typically more intense procedure than most construction methods.

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<sup>3</sup> [https://www.fhwa.dot.gov/ENVIRonment/noise/construction\\_noise/rcnm/rcnmcover.cfm](https://www.fhwa.dot.gov/ENVIRonment/noise/construction_noise/rcnm/rcnmcover.cfm)



Program default usage factors, or the percentage of time the equipment generally operates, were used for all pieces of analyzed equipment. Source levels typically used in a construction noise analysis are shown in Table 1.

**Table 1: Typical Construction Equipment Noise Levels**

Equipment	Lmax Sound Level at 50 feet (dBA)	Utilization %
Backhoe	77.6	40%
Compactor (ground)	80.0	20%
Compressor (air)	78.0	40%
Concrete Mixer Truck	79.0	40%
Concrete Pump Truck	81.0	20%
Concrete Saw	89.6	20%
Crane	81.0	16%
Dozer	81.7	40%
Excavator	80.7	40%
Forklift	75.0	10%
Pneumatic Tools	85.0	50%
Generator	81.0	50%
Mounted Impact Hammer (hoe ram)	90.0	20%
Impact Pile Driver	95.0	20%
Front End Loader	79.0	40%
Paver	77.0	50%
Roller	80.0	20%
Tractor	84.0	40%
Welder / Torch	73.0	40%
Source: RCNM 1.1		

The results of this analysis at the closest sensitive receiver, which are the townhomes approximately 90 feet northeast of the project boundary<sup>4</sup>, are shown in Table 2.

**Table 2: Modeled Noise Levels from the Proposed Project and Nearest Sensitive Receiver**

Noise Source(s)	Modeled Noise Level (dBA)
Impact Pile Driver Only	83
Impact Pile Driver + Pneumatic Tools + Concrete Saw	85
Concrete Saw + Pneumatic Tools	80

City of Antioch General Plan<sup>5</sup> section 11.6.2i-n addresses construction noise. It limits construction hours to 7am to 7pm Monday through Saturday. However, during daytime hours, no limits are set.

<sup>4</sup> This is measured via google earth to the project site

<sup>5</sup> [https://www.antiochca.gov/fc/community-development/planning/Antioch\\_Adopted\\_General\\_Plan.pdf](https://www.antiochca.gov/fc/community-development/planning/Antioch_Adopted_General_Plan.pdf)



Even if the city does not set its own construction noise daytime limits, the IS should set thresholds based on other similar documents. For example, the County of Los Angeles code has a construction noise limit of 75 dBA<sup>6</sup>. Certainly, that has no jurisdiction for this project, but other government agencies and other municipalities do have daytime construction noise limits. It is the responsibility of the project applicant to find an applicable guideline to use and determine if noise levels will create an adverse impact on the community. If the applicant finds, chooses and properly cites another threshold that they feel is more appropriate, it is within their right to do so. Many such thresholds are based on ambient noise levels, which are not present here. Either way, the IS must be revised to include such a threshold to evaluate, identify, and potentially mitigate construction noise impacts.

All three modeled scenarios yield noise levels that match or exceed the 75 dBA construction noise guideline. As it currently stands, this is an exceedance of the recommended construction noise threshold which would require mitigation, such as a temporary soundwall. A study should be developed, consistent with the General Plan Policy 11.8.2, that shows how implementation of recommended barriers reduce noise levels below significance limits.

### Operational Noise Impacts are Potentially Significant.

Operational noise from the project may represent long-term changes in the surrounding noise environment for nearby receptors. Typically, operational noise sources can include rooftop mechanical units and noise from outdoor spaces and balconies. In order to model the noise, the following assumptions were used:

- Mechanical equipment, such as HVAC units, can generate sound power levels as high as 91 dBA<sup>7</sup>.
- The HVAC units would be located on building rooftops and are 10 feet behind the edge of the building.
- The distance from the new buildings to the residence to the southeast is 180 feet.
- Three units were assumed, 10 feet apart
- 4 dB of reduction was assumed from a rooftop parapet
- HVAC systems may run constantly, especially in the City of Antioch where summer average heat can reach as high as 90 degrees Fahrenheit<sup>8</sup>.

Based on the above assumptions, the potential nighttime operational noise levels are shown in Table 3. No rooftop amenity space was indicated in the Initial Study, so it was not studied. If more detailed plans show balconies or rooftop amenity space, those should be analyzed as well.

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<sup>6</sup> [https://codelibrary.amlegal.com/codes/los\\_angeles/latest/lamc/0-0-0-193925](https://codelibrary.amlegal.com/codes/los_angeles/latest/lamc/0-0-0-193925)

<sup>7</sup> [https://www.americanstandardair.com/content/dam/Trane/en-engineer/products/rooftopunits/Voyager/Voyager%20III/RT-PRC022-E4\\_0513.pdf](https://www.americanstandardair.com/content/dam/Trane/en-engineer/products/rooftopunits/Voyager/Voyager%20III/RT-PRC022-E4_0513.pdf), page 23

<sup>8</sup> <https://weatherspark.com/y/1111/Average-Weather-in-Antioch-California-United-States-Year-Round>



**Table 3: Predicted Project Operational Noise Levels**

Receiver	Modeled Noise Level (dBA)	Noise Criteria	Exceedance
Townhomes to the Northeast	53	n/a <sup>a</sup>	Unknown
<sup>a</sup> Without measured ambient levels, increases over the ambient are impossible to determine.			

Since there is no ambient measurement, there is no criteria, because nighttime criteria levels are typically based on ambient levels. Suburban areas at night can be as quiet as 40 dBA<sup>9</sup>. Based on the housing element DEIR, a significance threshold is used in the city of 3 dBA over ambient limits, of which this scenario exceeds. Due to the proximity of highway SR-4, ambient levels may be higher than this, but this is why it is important to measure ambient noise levels to establish a baseline. The available Project plans are unclear as to the placement of HVAC units. More developed plans may show an HVAC arrangement that is less intrusive, or ambient measurements may present a higher criterion. But the potential for significant impacts do exist, and more thorough study is needed.

The project has the potential to result in significant operational noise levels, which should be considered in a noise study, as required by General Plan Policy 11.8.2. This noise study should determine potential mitigation measures, such as shielding of the HVAC units' noise emissions to the southeast, to reduce community exposure to excessive noise. The noise study must also measure ambient noise levels and determine if the increase over ambient levels would be significant.

### Construction Vibration Impacts are Potentially Significant.

The DEIR on pages IV.L-14 and IV.L-15 describes the construction vibration analysis used for housing with the city limits of Antioch, based on procedures and thresholds found in the Caltrans *Transportation and Construction Vibration Guidance Manual* and the Federal Transit Administration *Transit Noise and Vibration Impact Assessment Manual*. The DEIR states that “a buffer distance of about 115 feet” is required “to protect vibration sensitive structures (below the 0.12 in/sec criterion in Table IV.L-4).” The closest structure for this project is 90 feet away. The DEIR continues, “if sensitive receptors are located within these buffer distances, future developments under the Project should prepare a screening level vibration analysis for City review in accordance with General Plan Policy 11.8.2 (k).” This was not completed for this project as required by the City and should be part of a noise study associated with this project. The DEIR continues, stating that if the “screening level analysis shows that construction has the potential to result in damage to structures” then a “detailed vibration impact assessment must be prepared by a structural engineer or other appropriate professional to determine appropriate design means and methods of construction to avoid the potential damage” (DEIR pages IV.L-14 and IV.L-1). This is vital to study, as a potential impact would not just impact people's day to day lives, but could result in damage to nearby housing, and thus should be studied more thoroughly.

<sup>9</sup> <https://ehs.yale.edu/sites/default/files/files/decibel-level-chart.pdf>



## **Baseline Noise not Established.**

CEQA requires evaluation of whether a project would cause a “substantial temporary or permanent increase in ambient noise levels.” Without knowing how loud the environment is, it is impossible to determine if the new project will increase noise in the surrounding community. Baseline noise measurements are the preferred way to determine background noise sources. These measurements serve as a crucial reference point for evaluating the potential noise impacts of proposed projects or activities. Without establishing the baseline noise conditions before any new development occurs, decision-makers cannot effectively determine whether the project complies with noise regulations nor identify any potential adverse effects on the surrounding environment and communities. Given the proximity to both local streets and highway SR-4 along with noise from nearby residences, as well as shielding from nearby structures, noise levels should be physically measured to be accurately determined.

The Federal Transit Administration’s 2018 Transit Noise and Vibration Impact Assessment Manual<sup>10</sup> (FTA Manual) Appendix E recommends a minimum of three one-hour Equivalent Sound Level (Leq) noise measurements (peak-hour roadway traffic, typical midday conditions, and typical nighttime conditions) to estimate the Day-Night Sound Level (Ldn) at site, which can be used to establish baseline noise conditions for the project, including the Community Noise Equivalent Level (CNEL). An EIR should be prepared with these baseline noise measurements to properly describe the noise environment.

## **Conclusions**

Considering the potentially significant impacts from construction and operational noise and vibration on the surrounding community, it is imperative that an EIR be conducted to disclose and analyze these potentially significant impacts. Failure to evaluate these impacts would be a violation of CEQA’s core purpose of providing a transparent and comprehensive assessment of a project’s environmental effects.

Very truly yours,  
WILSON IHRIG

A handwritten signature in blue ink, appearing to read "Jack Meighan", is written over a horizontal line.

Jack Meighan  
Associate

meighan - updated wildflower noise analysis.docx

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<sup>10</sup> [https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/research-innovation/118131/transit-noise-and-vibration-impact-assessment-manual-fta-report-no-0123\\_0.pdf](https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/research-innovation/118131/transit-noise-and-vibration-impact-assessment-manual-fta-report-no-0123_0.pdf)





## **JACK MEIGHAN**

*Associate*

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Jack joined Wilson Ihrig in 2021 and is an experienced acoustics engineer with expertise in projects involving rail transit systems, highways, CEQA analysis, environmental noise reduction, mechanical drawing reviews, and construction noise and vibration mitigation. He has hands-on experience with project management, including client coordination and presentations, as well as in designing, developing, and testing MATLAB code used in acoustics applications. Additionally, his expertise includes taking field measurements, developing test plans and specifying, purchasing, setting up and repairing acoustic measurement equipment. He has experience in using Traffic Noise Model (TNM), CadnaA, EASE, Visual Basic, LabView, and CAD software.

### **Education**

- B.S. in Mechanical Engineering, University of Southern California, Los Angeles, CA
- 

### **Project Experience**

#### ***Metro Regional Connector, Los Angeles CA***

Planned, took, and processed measurements as part of a team to determine the effectiveness of floating slab trackwork for a new subway in downtown Los Angeles that travels below the Walt Disney Concert Hall and the Colburn School of Music.

#### ***Rodeo Credit Enterprise CEQA Analysis for New Construction, Palmdale, CA***

Wrote an accepted proposal and executed it for a noise study project to determine noise mitigation requirements on a new housing development. Led all aspects of the project and managed the budget during all phases of project completion. Completed 5 separate projects of this type for this developer.

#### ***Blackhall Studios, Santa Clarita, CA***

Led the vibration measurement effort for a new soundstage directly adjacent to an existing freight and commuter rail line. Tested equipment, processed data, and analyzed results to determine the vibration propagation through the soil to the proposed soundstage locations, and was part of the team that developed mitigation techniques for the office spaces directly next to the rail line.

#### ***Octavia Residential Condos CEQA Study, San Francisco, CA***

Calculated the STC ratings for the proposed windows to meet Title 24 requirements, modeled the acoustic performance of floor and ceiling structures, researched noise codes, helped with a mechanical design review, and wrote a report summarizing the results for a new Condominium project being developed in San Francisco.

#### ***San Diego International Airport Terminal I Replacement, CA***

Conducted interior noise and vibration measurements, analyzed measurement data to help determine project criteria, modeled the existing and future terminals in CadnaA, and was part of a team that did a complete HVAC analysis of the entire terminal, as part of a CEQA analysis where a new terminal for the airport is being designed.



***Five Points Apartments Noise Study, Whittier, CA***

Took measurements, researched sound data and solutions, and recommended mitigation for a new apartment complex that was located next to an existing car wash, as part of a CEQA review.

***USC Ellison Vibration Survey, Los Angeles, CA***

Conducted vibration measurements as part of a survey to determine the effectiveness of vibration isolation platforms that are used to insulate cell growth in a cancer research facility. Determined the effectiveness and presented this information to the client. Researched and recommended a permanent monitoring system so the client could view data in real time.

***TEN50 Condos 'Popping' Noise Investigation, Los Angeles, CA***

Was part of a team that investigated the noise source of an unwanted popping noise in luxury condos in Downtown Los Angeles. Helped isolate the noise source location with accelerometers to determine where vibrations were occurring first and used an acoustic camera to determine where in the condo the noise was coming from.

***2000 University Project, Berkely, CA***

Wrote a construction noise monitoring plan based on environmental noise calculations, wrote a report summarizing the results, and attending a meeting with the client to discuss options.

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***Bay Area Rapid Transit (BART) On-Track, CA, San Francisco Bay Area, CA\****

Day to day project manager, responsible for meetings, presentations, and coordination with the client for an ongoing noise study on the BART system. Developed MATLAB code to process measurements and determine areas where high corrugation was present, contributing to excessively high in-car noise levels. Performed noise measurements inside both the right of way and the vehicle cabin, in addition to rail corrugation measurements.

***California I-605/SR-60 Interchange Improvement, Los Angeles, CA\****

Developed a noise model of the area that predicted sound levels for abatement design, in addition to conducting noise measurements and analysis. Led the Team in use of the FHWA Traffic Noise Model Software for the project, involving three major highways and two busy interchanges extending over 17 miles in southern California.

***Sound Transit On-Track, Seattle, WA\****

Took measurements, fixed equipment, and developed software in MATLAB to process Corrugation Analysis Trolley measurements as part of an ongoing noise study on the Sound Transit Link system. Tested vibration data to determine the best measurement and processing techniques to store the data in an online database for in-car measurements.

***LA Metro CRRC Railcar Testing, Los Angeles, CA\****

Led the effort to plan the measurements, determine measurement locations and finalize the test plan. Formulated a method to capture speed data directly from legacy train vehicles. Executed noise and vibration specification measurements for new rail cars delivered by CRRC.

***City of Los Angeles, Pershing Square Station Rehabilitation Noise Monitoring, CA\****

Built noise models, wrote a construction noise plan, and assisted in on-site construction noise issues as they arose for a renovation of the Pershing Square metro station in downtown Los



Angeles. Trained construction personnel in techniques for noise reduction and how to conduct noise monitoring measurements to meet project specifications.

***City of Orange Metrolink Parking Garage Construction Monitoring, CA\****

Wrote an adaptive management vibration monitoring plan, set up equipment to monitor live vibration levels, and generated weekly reports as part of an effort to build a new parking garage. Designed, planned, and completed measurements to predict and mitigate pile driving construction impacts at three historic building locations adjacent to the construction site. Coordinated with the client whenever an on-site problem arose.

***LA Metro Westside Subway Construction, Los Angeles, CA\****

Planned, organized, and processed noise measurements for the Purple Line extension construction. Implemented both long term microphones to measure noise levels and accelerometers to measure vibration levels in existing subway tunnels. Oversaw noise monitoring at sensitive construction sites for the project and worked with the contractor to find ways to reduce construction noise levels by approximately 10dB.

***Montreal Réseau Express Métropolitain, Canada\****

Conducted vibration propagation measurements used to create models to predict operational vibration levels for an under-construction transit line. Managed equipment, solved problems in the field, and wrote parts of the report summarizing the findings of the acoustic study.

***NHCRP Barrier\****

Took on-highway measurements and wrote, designed, developed, and tested MATLAB code to identify specific spectrograms to use for analyses for a project evaluating barrier reflected highway traffic noise differences in the presence of a single absorptive or reflective noise barrier.

***Siemens Railcar Testing for Sound Transit, Seattle, WA\****

Measured in-car noise and vibration for new rail cars delivered by Siemens. Developed new internal techniques for measurements based on the written specifications. Contributed to the team that helped identify issues that new cars had in meeting the Sound Transit specifications for noise and vibration. Participated in developing the test plan and specified then acquired new equipment for the measurement.

***Toronto/Ontario Eglinton Crosstown Light Rail, Final Design, Canada\****

Assisted in vibration propagation measurements, analysis, and recommendations for mitigation for a 12-mile light-rail line both on and under Eglinton Avenue. Set up and ran equipment for at-grade measurements with an impact hammer for underground measurements with an impact load cell that was used during pre-construction borehole drilling.

*\* Work done prior to working for Wilson Ihrig*



# **RESPONSES TO COMMENTS**

## **INTRODUCTION**

This Responses to Comments document contains comments received on the Wildflower Townhomes Project (proposed project) for which a 15183 Consistency Memorandum was prepared. Unlike for an Environmental Impact Report (EIR), comments received regarding the 15183 Consistency Memorandum are not required to be attached to the document, nor must the lead agency provide specific written responses to public comments. Nonetheless, the lead agency has chosen to provide responses to the comments for consideration by the City decision makers.

## **COMMENT LETTERS**

The City of Antioch received one comment letter on the proposed project that includes comments regarding the 15183 Consistency Memorandum. The comment letter was received from Adams Broadwell Joseph & Cardozo on behalf of the Contra Costa Residents for Responsible Development. The comment letter is included in the Responses to Comments section below.

## **RESPONSES TO COMMENTS**

The Responses to Comments section below includes the comment letter received regarding the proposed project, as well as responses to each comment. The bracketed comment letter is followed by numbered responses to each bracketed comment.



Letter 1

ADAMS BROADWELL JOSEPH & CARDOZO

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KELILAH D. FEDERMAN  
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July 15, 2025

**Via Email and Overnight Mail**

City of Antioch Planning Commission  
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Seth Webber, Vice-Chair  
Commissioners Jennifer Perez,  
Robert Martin, Ramesh Suman,  
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Zoe Merideth, Senior Planner  
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Re: **Antioch Planning Commission Hearing, Agenda Item 6-1;  
Wildflower Station Townhomes 2 Multifamily Residential  
Project (TM-02, AR-23-05)**

Dear Chair Riley, Vice-Chair Webber, Commissioners, Mr. Scudero, and Ms. Merideth:

We are writing on behalf of Contra Costa Residents for Responsible Development ("Contra Costa Residents") to provide comments on Agenda Item 6-1, the Wildflower Station Townhomes 2 Multifamily Residential Project (TM-02, AR-23-05) ("Wildflower Townhomes Project" or "Project") proposed by DeNova Homes, Inc. ("Applicant"). The Project consists of a vesting tentative map to create 19 residential lots for 19 townhome buildings, containing 159 residential units total. The Project site is a 10.35 acre undeveloped site located east of Hillcrest Avenue and west of Wildflower Station Place in the City of Antioch ("City").

The City contends that the Project previously has been analyzed under the California Environmental Quality Act<sup>1</sup> ("CEQA") and that further evaluation is not

<sup>1</sup> Pub. Res. Code ("PRC") §§ 21000 et seq.; 14 Cal. Code Regs. §§ 15000 et seq. ("CEQA Guidelines").





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required pursuant to CEQA Guidelines section 15183.<sup>2</sup> Specifically, the City contends that the Project was adequately analyzed in the Antioch Housing, Environmental Hazards, and Environmental Justice Elements Project Draft Environmental Impact Report (“Housing Element EIR”) adopted by the City in February 2023, and that additional environmental review is therefore not required for the Project pursuant to section 15183. These conclusions are set forth in the 15183 Consistency Memorandum, which purports to “determine if project-specific impacts would occur that are not adequately covered in [the Housing Element EIR]. To the extent the Housing Element policies and/or actions substantially mitigate a particular project impact, the impact shall not be considered peculiar, pursuant to 15183(f), thus, eliminating the need for further environmental review.”<sup>3</sup>


The City’s conclusion is not supported by substantial evidence in the record and further CEQA review is required. The City’s reliance on section 15183 to avoid any project-specific environmental review is misplaced, as none of the Project’s specific impacts were studied in the Housing Element EIR or the 15183 Consistency Memorandum, and the 15183 Consistency Memorandum does not provide evidence that the single Housing Element policy applicable to this Project will substantially mitigate any Project-level impact. Moreover, the Project will result in new or more significant impacts that are peculiar to the Project site. As a result, the Planning Commission lacks substantial evidence to approve the Project.

In particular, Contra Costa Residents’ noise expert found that the Project’s construction and operational noise impacts were not analyzed and would exceed applicable significance thresholds. These impacts are peculiar to the Project, were not analyzed in the Housing Element EIR and will not be mitigated by any uniformly applied policies or standards. The City also lacks substantial evidence to conclude that the Project would not result in transportation impacts peculiar to the Project site. Neither the Housing Element EIR nor the 15183 Consistency Memorandum conducts a VMT analysis or provides evidence that the Project possesses characteristics exempting it from a detailed VMT analysis. Finally, neither the Consistency Memorandum nor the Housing Element EIR performed any emissions modeling to determine the scope of potential air quality and public health impacts from the Project’s construction and operational emissions, in violation of CEQA. The City therefore may not properly rely on CEQA Guidelines section 15183 to avoid further environmental review.

<sup>2</sup> March 2024 Wildflower Townhomes Section 15183 Consistency Memorandum (“15183 Consistency Memorandum”), pg. 1.

<sup>3</sup> *Id.* at pg. 9.

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We prepared these comments with the assistance of acoustics, noise, and vibration expert Jack Meighan of Wilson Ihrig.<sup>4</sup> As explained below, the Project will have potentially significant air quality, public health, noise and transportation impacts that are peculiar to the project and were not analyzed at a project-level in the Housing Element EIR, or are more severe than previously analyzed by the City. These impacts are not reduced to less than significant levels by the mitigation measures in the Housing Element EIR or any other standard conditions of approval, and therefore require disclosure and mitigation in a project-level Environmental Impact Report ("EIR") before the City can consider approval of the Project.

**I. STATEMENT OF INTEREST**

Contra Costa Residents is an unincorporated association of individuals and labor organizations that may be adversely affected by the potential public and worker health and safety hazards, and the environmental and public service impacts of the Project. The coalition includes the International Brotherhood of Electrical Workers Local 302, Plumbers & Steamfitters Local 159, Sheet Metal Workers Local 104, Sprinkler Fitters Local 483, along with their members, their families, and other individuals who live and work in the City of Antioch and Contra Costa County.

Contra Costa Residents' individual members live, work, recreate, and raise their families in the City of Antioch and surrounding communities. Accordingly, they would be directly affected by the Project's environmental, health, and safety impacts. Individual members may also work on the Project itself. They will be first in line to be exposed to any health and safety hazards that exist on site.

Contra Costa Residents also has an interest in enforcing environmental laws that encourage sustainable development and ensure a safe working environment for its members. Environmentally detrimental projects can jeopardize future jobs by making it more difficult and more expensive for businesses and industries to expand in the region, and by making the area less desirable for new businesses and new residents. Indeed, continued environmental degradation can, and has, caused construction moratoriums and other restrictions on growth that, in turn, reduce future employment opportunities.

<sup>4</sup> Mr. Meighan's Comments ("Meighan Comments") and CV are attached hereto as **Attachment A**.

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## II. LEGAL BACKGROUND

CEQA has two basic purposes, neither of which has the City satisfied in this case. First, CEQA is designed to inform decision makers and the public about the potential, significant environmental impacts of a project before harm is done to the environment.<sup>5</sup> The EIR is the “heart” of this requirement,<sup>6</sup> and has been described as “an environmental ‘alarm bell’ whose purpose it is to alert the public and its responsible officials to environmental changes before they have reached ecological points of no return.”<sup>7</sup> To fulfill this purpose, the discussion of impacts in an EIR must be detailed, complete, and “reflect a good faith effort at full disclosure.”<sup>8</sup> An adequate EIR must contain facts and analysis, not just an agency’s conclusions.<sup>9</sup>

Second, CEQA directs public agencies to avoid or reduce environmental damage when possible by requiring imposition of mitigation measures and by requiring the consideration of environmentally superior alternatives.<sup>10</sup> CEQA imposes an affirmative obligation on agencies to avoid or reduce environmental harm by adopting feasible project alternatives or mitigation measures to address all potentially significant impacts identified in the agency’s CEQA analysis.<sup>11</sup> Without an adequate analysis and description of feasible mitigation measures, it would be impossible for agencies relying upon an EIR or other environmental document to meet this obligation.

Following preliminary review of a project to determine whether an activity is subject to CEQA, a lead agency is required to prepare an initial study to determine whether to prepare an EIR or negative declaration, identify whether a program EIR, tiering, or other appropriate process can be used for analysis of the project’s environmental effects, or determine whether a previously prepared EIR could be used with the project, among other purposes.<sup>12</sup> CEQA requires an agency to

<sup>5</sup> Cal. Code Regs., tit. 14, § 15002, subd. (a)(1) (“CEQA Guidelines”); *Berkeley Keep Jets Over the Bay v. Bd. of Port Comm’rs.* (2001) 91 Cal.App.4th 1344, 1354 (“*Berkeley Jets*”); *County of Inyo v. Yorty* (1973) 32 Cal.App.3d 795, 810.

<sup>6</sup> *No Oil, Inc. v. City of Los Angeles* (1974) 13 Cal.3d 68, 84.

<sup>7</sup> *County of Inyo v. Yorty* (1973) 32 Cal.App.3d 795, 810.

<sup>8</sup> CEQA Guidelines, § 15151; *San Joaquin Raptor/Wildlife Rescue Center v. County of Stanislaus* (1994) 27 Cal.App.4th 713, 721-722.

<sup>9</sup> See *Citizens of Goleta Valley v. Bd. of Supervisors* (1990) 52 Cal.3d 553, 568.

<sup>10</sup> CEQA Guidelines, § 15002, subd. (a)(2) and (3); *Berkeley Jets*, 91 Cal.App.4th, at p. 1354; *Laurel Heights Improvement Ass’n v. Regents of the University of Cal.* (1998) 47 Cal.3d 376, 400.

<sup>11</sup> Pub. Resources Code, §§ 21002-21002.1.

<sup>12</sup> CEQA Guidelines, §§ 15060, 15063, subd. (c).

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analyze the potential environmental impacts of its proposed actions in an EIR except in certain limited circumstances.<sup>13</sup> A CEQA exemption may be invoked only if expressly authorized by the CEQA statute or guidelines and if there is no possibility of a significant effect on the environment. Exemptions must be narrowly construed and are not to be expanded beyond the scope of their plain language.<sup>14</sup>

CEQA Guidelines Section 15183 provides an exemption for projects which are consistent with the development density established by existing zoning, community plan or general plan policies for which an EIR was certified, *except as necessary to evaluate whether there are project-specific significant impacts which are peculiar to the project or project site.*<sup>15</sup> In relying on section 15183 to approve a project, a lead agency may not forgo further analysis of potentially significant impacts unless it makes certain findings. An agency is required to perform further analysis as to impacts that (1) are peculiar to the proposed project or parcel, (2) were not analyzed as significant effects in a prior EIR for the zoning, community or general plan with which the project is consistent, (3) are potentially significant off-site or cumulative impacts that were not discussed in the prior EIR, or (4) are previously identified significant impacts which, due to substantial new information not known at the time the EIR was certified, are determined to have a more severe impact than discussed in the prior EIR.<sup>16</sup>

Under section 15183(f), an effect of a project on the environment is not considered peculiar to the project or project site if “uniformly applied development policies or standards have been previously adopted ... with a finding that the development policies or standards will substantially mitigate the environmental effect when applied to future projects, unless substantial new information shows that the policies or standards will not substantially mitigate the environmental effect.”<sup>17</sup>

Agency determinations under Guidelines section 15183 are reviewed under the substantial evidence standard.<sup>18</sup> In determining whether an agency’s findings concerning the use of a statutory exemption from CEQA may be upheld, courts

<sup>13</sup> See, e.g., Pub. Resources Code, § 21100.

<sup>14</sup> *Castaic Lake Water Agency v. City of Santa Clarita* (1995) 41 Cal.App.4th 1257.

<sup>15</sup> 14 CCR § 15183(a).

<sup>16</sup> 14 CCR § 15183(b)(1)-(4).

<sup>17</sup> 14 CCR § 15183(f).

<sup>18</sup> *Lucas v. City of Pomona* (2023) 92 Cal.App.5th 508, 538, citing *Concerned Dublin Citizens v. City of Dublin* (2103) 214 Cal.App.4th 1301, 1311; see also, *Hilltop Group v. County of San Diego* (2024) 99 Cal.App.5th 890, 909-10.

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review the administrative record to see that substantial evidence supports each element of the exemption.<sup>19</sup> This includes the determination that “uniformly applied development policies or standards” will substantially mitigate the project’s environmental effects.<sup>20</sup> Agency findings must specifically address the effect of uniform policies and standards on potential environmental impacts.<sup>21</sup>

Section 15168’s two-step inquiry of a program EIR’s applicability to later activities holds that “if a later activity would have effects that were not examined in the program EIR, a new initial study would need to be prepared leading to either an EIR or a negative declaration.” The City insists that, pursuant to sections 15162 and 15183, the Project is within the scope of the program EIR and no subsequent EIR is required. “Whether a later activity is within the scope of a program EIR is a factual question that the lead agency determines based on substantial evidence in the record.”

Here, the Housing Element EIR analyzed impacts at a program level, and did not analyze quantify, or disclose Project-level impacts for issues including transportation, air quality and public health, and noise.

### III. THE PROJECT IS NOT EXEMPT FROM FURTHER CEQA REVIEW AND AN EIR IS REQUIRED

1-2

The City contends that the Housing Element EIR provides the basis for its determination that no further environmental review of the Project’s impacts is required. The 15183 Consistency Memorandum notes that the Project’s density of 20.05 dwelling units per acre (“du/ac”) is consistent with the development density established in the Housing Element EIR, i.e., 20-25 du/ac, and purports to evaluate whether the Project will have any effects peculiar to the Project or Project site.<sup>22</sup> It goes on to state that “[t]o the extent that the Housing Element policies and/or actions substantially mitigate a particular project impact, the impact shall not be considered peculiar, pursuant to [CEQA Guidelines section] 15183(f), thus, eliminating the requirement for further environmental review.”<sup>23</sup>

However, while the Consistency Memorandum recites the requirements of section 15183, it does not actually analyze whether the Project will have any effects

<sup>19</sup> *Lucas*, 92 Cal.App.5th at 538.

<sup>20</sup> 14 CCR § 15183(f).

<sup>21</sup> *Hilltop Group*, 99 Cal.App.5th at 918.

<sup>22</sup> 15183 Consistency Memorandum, pg. 9.

<sup>23</sup> *Id.* at pgs. 8-9.

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cont.

peculiar to the Project or the Project site. Neither the Housing Element EIR nor the 15183 Consistency Memorandum examine the Project-level effects on environmental impacts such as air quality, health risks, transportation and noise.

1-3

Nor does the Consistency Memorandum identify any “Housing Element policies and/or practices” that apply to the Project to substantially mitigate the Project’s impacts. As discussed below, the City lacks substantial evidence to support the necessary findings to exempt the Project from CEQA review, and the City must prepare and circulate for public review an EIR that analyzes the Project’s potentially significant impacts.

1-4

**A. The City Lacks Substantial Evidence to Support its Conclusions with Respect to the Project’s Transportation Impacts**

CEQA requires analysis of a project’s transportation impacts via analysis of the project’s vehicle miles traveled (“VMT”).<sup>24</sup> The Housing Element EIR evaluated the VMT impacts of all of the potential new housing sites in the City (including the site for the Wildflower Townhomes Project) and found a significant impact on VMT.<sup>25</sup> To address these impacts, the Housing Element EIR adopted Mitigation Measure TRANS-1, which provides that individual housing development projects (like this one) *that do not screen out from VMT impacts analysis shall provide a quantitative VMT analysis.*<sup>26</sup> Individual projects which result in a significant VMT impact are required to implement travel demand management measures and physical measures to reduce VMT to a less-than-significant level.<sup>27</sup> The Housing Element EIR lists seven criteria that are used to screen projects out of conducting project-level VMT analysis: (1) CEQA-exempt projects, (2) small projects, (3) local-serving uses, (4) proximity to a major transit stop, (5) projects located in low VMT areas, (6) affordable housing, and (7) transportation projects.<sup>28</sup> These criteria screen out projects from performing a full VMT analysis because projects meeting these criteria are presumed to have less-than-significant VMT impacts absent substantial evidence to the contrary.<sup>29</sup>

<sup>24</sup> 14 CCR § 15064.3.

<sup>25</sup> Housing Element EIR, pg. IV.B-27.

<sup>26</sup> *Id.*

<sup>27</sup> *Id.*

<sup>28</sup> *Id.*, pgs. IV.B-21—IV.B-22.

<sup>29</sup> *Id.*

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The Housing Element EIR makes clear that it did not analyze VMT impacts from individual housing projects like this one. Nor does the 15183 Consistency Memorandum for this Project analyze the Project's VMT impacts. Instead, the City asserts that because the 15183 Consistency Memoranda concludes that the Project qualifies for the Guidelines section 15183 exemption, the Project "screens out" from having to perform a quantitative VMT analysis as required under Housing Element EIR mitigation measure TRANS-1. Specifically, the 15183 Consistency Memorandum states:

"As demonstrated through this 15183 Consistency Memorandum, the proposed project would not result in significant impact that is peculiar to the project or project site, a significant effect that was not identified in the Housing Element EIR, or a substantially more severe significant effect related to transportation beyond what was identified in the Housing Element EIR. Therefore, pursuant to Section 15183 of the CEQA Guidelines, the proposed project qualifies for exemption from further environmental review under CEQA. Because the proposed project would be considered exempt from CEQA, Mitigation Measure TRANS-1 is not applicable."

1-4  
cont.

The City's position is legally and logically flawed. The City's argument employs circular reasoning by claiming that because the Project is exempt from CEQA, it does not need to conduct a VMT analysis, citing the Housing Element EIR screening criteria. This reasoning is fundamentally flawed because the argument's premise ("the Project is exempt from CEQA") *assumes* the conclusion rather than supporting it. The City argues that an exemption from CEQA means an exemption from VMT analysis, but the CEQA exemption itself is predicated on the absence of significant environmental impacts peculiar to the Project, including transportation impacts which a VMT analysis is designed to determine. In other words, the City claims that it need not evaluate the Project's potentially significant VMT impacts because the Project is exempt from CEQA, but the exemption determination itself rests on unsupported assumptions regarding the lack of Project-specific peculiar impacts. The City lacks any evidence to support the conclusion that the Project will not have significant VMT impacts peculiar to the Project or Project site because it performed no Project-specific VMT analysis. By bypassing the VMT analysis this way, the City avoids an analysis that could reveal significant impacts, and preclude the use of the section 15183 exemption. This approach undermines the purpose of CEQA, which is to ensure that potential environmental impacts are identified, disclosed and mitigated.

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cont.

While the Consistency Memorandum relies solely on the “CEQA Exemption” screening criterion, the Project does not qualify for any of the other screening criteria set out in the Housing Element EIR. The Project is not a “Small Project,” defined as having 10,000 square feet or less<sup>30</sup> of non-residential space or 10 residential units or less. The Project will not consist of “Local-Serving Uses,” as this screening criteria is intended to apply to commercial uses and is not relevant to residential projects.<sup>31</sup> The Project does not qualify for the “Proximity to a Major Transit Stop” VMT screening criteria, as this criteria is limited to the 0.5 mile (walking radius) surrounding the Antioch BART and Antioch Amtrak stations, and the Housing Element EIR found that none of the housing sites analyzed fall within this boundary.<sup>32</sup> The Project does not include any affordable housing, and therefore does not screen out from VMT analysis on that basis. Nor is the Project a “Transportation Project.” Finally, neither the 15183 Consistency Memoranda nor the Housing Element EIR evaluates whether the Project is in a “Low VMT Area,” defined as having home-based VMT per resident at or below 85% of the Antioch citywide average.<sup>33</sup> This determination requires TAZ-level screening using Contra Costa Countywide VMT maps and Travel Demand Model (“CCTA Model”) results.<sup>34</sup> The Housing Element EIR, due to its programmatic scope, did not conduct this screening.<sup>35</sup> It does acknowledge, however, that such analysis may be necessary for individual projects.<sup>36</sup> Similarly, the 15183 Consistency Memoranda omits TAZ-based screening. Therefore, the City has not provided substantial evidence to support a finding that the Project is located in a low VMT area.

As discussed above, the City’s failure to perform a quantitative VMT analysis prevents an understanding of the extent to which the Project’s VMT is expected to exceed the significance threshold, or the nature of mitigation required to reduce such impacts to below the threshold. The City’s conclusion lacks the support of substantial evidence because it neither includes a site-specific VMT analysis nor demonstrates that the Project qualifies for screening under any recognized exemption. Accordingly, the City must prepare a Project-specific EIR that includes a quantitative VMT analysis and appropriate mitigation.

<sup>30</sup> *Id.*

<sup>31</sup> *Id.* at pg. IV.B-24.

<sup>32</sup> *Id.*

<sup>33</sup> *Id.* at pg. IV.B-22.

<sup>34</sup> *Id.* at pg. IV.B-24.

<sup>35</sup> *Id.*

<sup>36</sup> *Id.*





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**B. The Project Will Have Significant, Unmitigated Air Quality and Public Health Impacts That are More Severe than Previously Analyzed**

The City has not performed any emissions modeling to determine potential impacts of Project construction or operations; neither the 15183 Consistency Memorandum nor the Housing Element EIR analyzed any Project site-specific air quality impacts or identified any sensitive receptors near the Project site. Under CEQA Guidelines section 15168(c), these air quality and public health impacts are effects that were not examined in the Housing Element EIR, requiring a new initial study leading to either an EIR or negative declaration. The 15183 Consistency Memorandum includes a cursory discussion of the Project's potential air quality impacts, as follows:

1-5

"The proposed project would be consistent with the Housing Element and, thus, was anticipated by the City and considered under the Housing Element EIR analysis. Accordingly, the proposed project would not result in any new significant effects related to air quality. However, the Housing Element EIR requires mitigation measures related to construction emissions of criteria air pollutant emissions from future housing developments (AIR-1), operational emissions of criteria air pollutant emissions from future housing developments (AIR-2), and health risks related to the generation of toxic air contaminants (TACs) and particulate matter (PM) 2.5 microns in diameter (PM<sub>2.5</sub>) during construction and operation of future housing developments (AIR-3a and AIR-3b)."<sup>37</sup>

However, the Consistency Memorandum goes on to state that only the Housing Element EIR's air quality mitigation measure AIR-3a is applicable to this Project.<sup>38</sup> But because the measure is structured so that development projects choose one option or the other, only part of Mitigation Measure AIR-3a is applicable to the project.<sup>39</sup> Specifically, the proposed project will be required to equip all off-road diesel equipment with Tier 4 engines and the Applicant must prepare a Construction Emissions Minimization Plan for all identified reduction measures.<sup>40</sup> Based on the Project's size, location and characteristics, none of the other Housing

1-6

<sup>37</sup> 15183 Consistency Memorandum, pg. 10.

<sup>38</sup> *Id.* at pg. 11.

<sup>39</sup> *Id.*

<sup>40</sup> *Id.*





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cont.

Element EIR's air quality mitigation measures would apply to this Project. The City then concludes, without any supporting evidence, that "[i]mplementation of Mitigation measure AIR-3a would ensure the proposed project would not result in a significant impact that is peculiar to the project or the project site, would not be identified as a significant effect in the Housing Element EIR, and would not result in a more severe adverse impact than the significant effects previously identified within the Housing Element EIR."<sup>41</sup> This conclusion lacks the support of any evidence, let alone substantial evidence as required by CEQA.

1-7

The Housing Element EIR expressly recognized that the use of construction equipment during construction of housing developments like the Project can pose health risks related to the generation of TACs and PM<sub>2.5</sub>.<sup>42</sup> DPM is a known toxic air contaminant ("TAC") carcinogen that contains numerous harmful compounds. Diesel exhaust has been linked to a range of serious health problems including an increase in respiratory disease, lung damage, cancer, and premature death.<sup>43,44,45</sup> Fine DPM is deposited deep in the lungs in the smallest airways and can result in increased respiratory symptoms and disease; decreased lung function, particularly in children and individuals with asthma; alterations in lung tissue and respiratory tract defense mechanisms; and premature death.<sup>46</sup> Exposure to DPM increases the risk of lung cancer. It also causes non-cancer effects including chronic bronchitis, inflammation of lung tissue, thickening of the alveolar walls, immunological allergic reactions, and airway constriction.<sup>47</sup> DPM is a TAC that is recognized by state and federal agencies as causing severe health risk because it

<sup>41</sup> *Id.*

<sup>42</sup> Housing Element EIR, pgs. IV.C-21—23.

<sup>43</sup> California Air Resources Board, Initial Statement of Reasons for Rulemaking, Proposed Identification of Diesel Exhaust as a Toxic Air Contaminant, Staff Report, June 1998; see also California Air Resources Board, Overview: Diesel Exhaust & Health, <https://ww2.arb.ca.gov/resources/overview-diesel-exhaust-and-health#:~:text=Diesel%20Particulate%20Matter%20and%20Health&text=In%201998%2C%20CARB%20identified%20DPM,and%20other%20adverse%20health%20effects>.

<sup>44</sup> U.S. EPA, Health Assessment Document for Diesel Engine Exhaust, Report EPA/600/8-90/057F, May 2002.

<sup>45</sup> Environmental Defense Fund, Cleaner Diesel Handbook, Bring Cleaner Fuel and Diesel Retrofits into Your Neighborhood, April 2005; [http://www.edf.org/documents/4941\\_cleanerdieselhandbook.pdf](http://www.edf.org/documents/4941_cleanerdieselhandbook.pdf), accessed July 5, 2020.

<sup>46</sup> California Air Resources Board, Initial Statement of Reasons for Rulemaking, Proposed Identification of Diesel Exhaust as a Toxic Air Contaminant, Staff Report, June 1998.

<sup>47</sup> Findings of the Scientific Review Panel on The Report on Diesel Exhaust as adopted at the Panel's April 22, 1998, Meeting.

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cont.

contains toxic materials, unlike PM<sub>2.5</sub> and PM<sub>10</sub>.<sup>48</sup> Despite the Housing Element EIR's express recognition of the health risks from construction equipment emissions of TACs from construction of projects like this one, the City failed to conduct a quantitative health risk analysis ("HRA") and omitted a comparison of the Project's health risk impacts to the Bay Area Air Quality Management District's ("BAAQMD") threshold of 10 in one million.<sup>49</sup>

The City's omission of an HRA also conflicts with the Office of Environmental Health Hazard Assessment's ("OEHHA") *Risk Assessment Guidelines: Guidance Manual for Preparation of Health Risk Assessments*. These guidelines recommend that all short-term projects lasting at least 2 months assess cancer risks and that exposure from projects lasting more than 6 months should be evaluated for the duration of the project.<sup>50</sup> Here, the Project's construction is expected to take longer than six months.<sup>51</sup> Because the anticipated duration of the Project's construction would exceed the 2-month and 6-month requirements set forth by OEHHA, a quantified HRA under OEHHA guidance should have been prepared to evaluate the Project for its entire duration.

Therefore, based on the current record, the City cannot conclude that implementation of AIR-3a will prevent significant new air quality impacts associated with the Project. As a result, the City may not rely on the section 15183 exemption to approve this Project, and an EIR must be prepared and circulated for public review.

1-8

**C. The City Lacks Substantial Evidence to Support its  
Conclusions with Respect to the Project's Noise Impacts**

The City has not performed any site-specific analysis of the Project's potential noise impacts. Neither the Housing Element EIR nor the 15183 Consistency

<sup>48</sup> Health & Safety Code § 39655(a) (defining "toxic air contaminant" as air pollutants "which may cause or contribute to an increase in mortality or in serious illness, or which may pose a present or potential hazard to human health. A substance that is listed as a hazardous air pollutant pursuant to subsection (b) of Section 112 of the federal act (42 U.S.C. Sec. 7412 (b)) is a toxic air contaminant.")

<sup>49</sup> BAAQMD, Regulation 11-18 Reduction of Risk From Air Toxic Emissions at Existing Facilities, p. 5.

<sup>50</sup> OEHHA, *Risk Assessment Guidelines: Guidance Manual for Preparation of Health Risk Assessments*, pgs. 8-18, available at: <https://oehha.ca.gov/air/cmr/notice-adoption-air-toxics-hot-spots-program-guidance-manual-preparation-health-risk-0>.

<sup>51</sup> 15183 Consistency Memo, pg. 11.

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Memorandum includes any analysis of ambient noise in the area of the Project site, modeling of the Project's construction or operational noise impacts, or identification of sensitive receptors near the Project site. The 15183 Consistency Memorandum contains no discussion whatsoever regarding whether the Project may have peculiar noise impacts necessitating further CEQA review. Under CEQA Guidelines section 15168(c), these Project-specific noise impacts were not examined in the Housing Element EIR, requiring a new initial study leading to either an EIR or negative declaration.

1-9

The Housing Element EIR recognizes that for individual projects like this one, "construction activities could generate exterior noise levels that exceed the City's noise objectives established under General Plan Policy 11.8.2."<sup>52</sup> The Housing Element EIR also states that "[i]ndividual housing developments... would result in a potentially significant impact if they cause a new exceedance of the General Plan noise objectives, or an audible (3.0 dBA) increase in areas where the General Plan noise objectives are already exceeded as the result of existing development."<sup>53</sup> General Plan Policy 11.8.2 (f) requires a detailed noise attenuation study to be prepared by a qualified acoustical engineer to determine appropriate mitigation and ways to incorporate such mitigation into project design and intervention. Finally, the Housing Element EIR points out that General Plan Policy 11.8.2 requires development adjacent to occupied noise sensitive land uses to implement a construction-related noise mitigation plan that should depict the location of construction equipment and how the noise from this equipment will be mitigated during construction through the use of noise reduction methods listed in Policy 11.8.2(o).<sup>54</sup>

Therefore, while the City expressly recognizes that individual housing projects like this one may have significant noise impacts on existing nearby sensitive receptors and requires studies and mitigation to reduce noise impacts, the 15183 Consistency Memo simply assumes without any analysis or evidence that the proposed Project "would not result in new significant impacts or substantially more significant impacts related to" impacts including noise.<sup>55</sup> It does not analyze or even consider whether the Project would "cause a new exceedance of the General Plan noise objectives, or an audible (3.0 dBA) increase in areas where the General Plan noise objectives are already exceeded." Nor does it consider whether Housing


<sup>52</sup> Housing Element EIR, pg. IV.L-12.

<sup>53</sup> *Id.* at pg. IV.L-10.

<sup>54</sup> *Id.* at pg. IV.L-12.

<sup>55</sup> 15183 Consistency Memorandum, pg. 13.

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Element policies and/or actions might substantially mitigate the Project's noise impacts. At a minimum, to demonstrate consistency with the Housing Element EIR, it must consider Project impacts in relation to General Plan Policy noise objectives, and must prepare a construction-related noise mitigation plan depicting the location of construction equipment and how the noise from this equipment will be mitigated during construction. Without any actual analysis, or consideration of any applicable uniformly applied development policies or standards, there is no support whatsoever for the conclusion that the Project will not have peculiar noise impacts.

1-10

Moreover, Mr. Meighan provides substantial evidence that the Project will cause new potentially significant noise impacts. First, the 15183 Consistency Memorandum lacks any measurement or disclosure of ambient noise conditions in the area of the Project site. This violates CEQA's requirement that a lead agency consider both the "absolute noise level" associated with a project as well as the increase in the level of noise that will result from a project.<sup>56</sup> This also fails to disclose a potentially significant operational noise impact. Based on information provided in the 15183 Consistency Memoranda, Mr. Meighan estimates the Project's nighttime operational noise levels to be 53 dBA at the Townhomes to the Northeast of the Project site.<sup>57</sup> Mr. Meighan goes on to explain that nighttime noise levels in suburban areas can be as quiet as 40 dBA.<sup>58</sup> When this ambient level is compared to the Project's estimated noise levels, the Project would far exceed the City's significance threshold of 3 dBA over ambient levels.<sup>59</sup> This is a new significant impact that was not addressed in the Housing Element EIR or the 15183 Consistency Memoranda and would not be mitigated even if the Housing Element EIR and General Plan noise policies and standards are applied.<sup>60</sup> In order to mitigate these impacts, Mr. Meighan suggest shielding HVAC units' noise emissions to the southeast.<sup>61</sup>

1-11

Second, Mr. Meighan provides substantial evidence that the Project will have significant construction noise impacts. As explained in Mr. Meighan's comments, the City fails to set its own construction noise significance threshold, leaving the

<sup>56</sup> *Gardiner Farms, LLC v. County of Kern* (2020) 45 CA5th 814, 887, 893; *Keep Our Mountains Quiet v. County of Santa Clara* (2015) 236 CA4th 714, 733.

<sup>57</sup> Meighan Comments, pg. 6.

<sup>58</sup> *Id.*

<sup>59</sup> *Id.*

<sup>60</sup> *Id.* at pgs. 2-3.

<sup>61</sup> *Id.* at pg. 6.

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City's conclusion unsupported by any meaningful standard of evaluation.<sup>62</sup> Because of this, Mr. Meighan's analysis assumes a significance threshold of 75 dBA, which comes from the Los Angeles Municipal Code.<sup>63</sup> Based on information provided in the 15183 Consistency Memoranda, Mr. Meighan estimates the Project's construction noise levels to range from 80 dBA to 85 dBA.<sup>64</sup> This exceeds the significance threshold by at least 5 dBA, which is a new significant impact that the Housing Element EIR and the 15183 Consistency Memoranda did not address. To reduce this impact, Mr. Meighan proposes the use of a temporary soundwall for the Project.<sup>65</sup>

1-12

Third, Mr. Meighan explains that the City fails to conduct a screening level construction vibration analysis as required by the Housing Element EIR.<sup>66</sup> The Housing Element EIR states that "if sensitive receptors are located within these buffer distances [about 115 feet], future developments under the Project should prepare a screening level vibration analysis for City review in accordance with General Plan Policy 11.8.2(k)."<sup>67</sup> Despite the closest structures from the Project site being only 90 feet away, the City does not conduct this analysis. Mr. Meighan explains that a thorough evaluation of the Project's potential vibration impacts is "vital" because, if those impacts are significant, they could interfere with people's daily lives and potentially damage nearby homes.<sup>68</sup>

For the above reasons, the City cannot rely on the 15183 CEQA exemption. The City must prepare an EIR that adequately analyzes the Project's potentially significant noise impacts by establishing ambient noise levels for the Project site, comparing them against applicable noise significance thresholds, and proposing mitigation for any significant impacts found.

1-13

**IV. THE CITY LACKS SUBSTANTIAL EVIDENCE TO MAKE THE  
REQUIRED FINDINGS TO APPROVE THE PROJECT'S VESTING  
TENTATIVE SUBDIVISION MAP**

The Project requires the City to approve a Vesting Tentative Subdivision Map ("VTSM") for condominium purposes that would subdivide the project site for

<sup>62</sup> *Id.*

<sup>63</sup> *Id.* at pg. 5.

<sup>64</sup> *Id.*

<sup>65</sup> *Id.*

<sup>66</sup> *Id.* at pg. 6.

<sup>67</sup> *Id.*

<sup>68</sup> *Id.*





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cont.**

the development of 19 townhome buildings, containing a total of 159 residential units.<sup>69</sup> However, as discussed above, the City fails to adequately analyze or mitigate several new project-specific environmental impacts that were not addressed by the Housing Element EIR. As a result, the City cannot make the requisite findings to approve the Project's VTSM.

California's Subdivision Map Act precludes the approval of a tentative map where the design or improvement of the proposed subdivision is not consistent with the applicable general plan, is likely to cause substantial environmental damage, or is likely to cause serious public health problems.<sup>70</sup>

Additionally, Antioch Municipal Code Section 9-4.323 states that a VTSM may be made conditional or denied if any of the following is determined:


- A failure to do so would place the residents of the subdivision or the immediate community, or both, in a condition dangerous to their health or safety, or both; or
- The condition or denial is required in order to comply with state or federal laws.

As detailed in our comments and those of our noise expert, there is substantial evidence that the Project may result in several potentially significant environmental impacts, including: (1) construction and operational noise, (2) VMT, and (3) air quality and related health risks. These impacts remain unaddressed and could pose serious risks to public health and safety—both for future subdivision residents and the surrounding community. Therefore, the City cannot make the required findings under the Subdivision Map Act and Antioch's Municipal Code to approve the VTSM until all of the Project's potentially significant impacts are thoroughly analyzed and effectively mitigated.

<sup>69</sup> Staff Report for the Antioch Planning Commission Regular Meeting of July 16, 2025, pg. 1

<sup>70</sup> Government Code § 66474(b), (e) and (f).

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V. CONCLUSION

As discussed herein, the City lacks substantial evidence to rely on a CEQA Guidelines section 15183 exemption for Project approval. The Project will result in potentially significant project-level impacts which are peculiar to the Project and Project site and will require mitigation. Therefore, the Project cannot be approved until the City complies with CEQA by preparing an EIR.


Sincerely,



Alaura McGuire

Attachment  
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**Letter 1 cont.**

## **EXHIBIT A**





Letter 1 cont.



WILSON IHRIG  
ACOUSTICS, NOISE & VIBRATION

CALIFORNIA  
WASHINGTON  
NEW YORK

WI #24-001.32

July 12, 2025

Alaura R. McGuire  
Adams Broadwell Joseph & Cardozo  
601 Gateway Boulevard, Suite 1000  
South San Francisco, CA 94080

**SUBJECT: Wildflower 2 Section 15183 Consistency Memorandum  
Antioch, CA  
Review and Comments on the Initial Study Noise Analysis**

Dear Ms. McGuire,

As requested, we have reviewed the information and noise impact analysis for the Wildflower 2 Townhomes Project in Antioch, CA. The project consists of construction and operation/occupancy of 168 townhome units along Hillcrest Avenue, northeast of the intersection with Davidson Drive and Deer Valley Road. This letter is based on the Section 15183 Consistency Memorandum prepared by Raney Planning and Management, dated March 2024. The site is surrounded by noise-sensitive receivers, most notably existing townhomes to the northeast of the site along Wildflower Station Place.

1-14

Wilson Ihrig is an acoustical consulting firm that has practiced exclusively in the field of acoustics since 1966. During our 58 years of operation, we have prepared hundreds of noise studies for Environmental Impact Reports and Statements. We have one of the largest technical laboratories in the acoustical consulting industry. We also utilize industry-standard acoustical programs such as Roadway Construction Noise Model (RCNM), SoundPLAN, and CadnaA. In short, we are well qualified to prepare environmental noise studies and review studies prepared by others.

#### Adverse Effects of Noise<sup>1</sup>

Although the health effects of noise are not taken as seriously in the United States as they are in other countries, they are real and, in many parts of the country, pervasive.

**Noise-Induced Hearing Loss.** If a person is repeatedly exposed to loud noises, he or she may experience noise-induced hearing impairment or loss. In the United States, both the Occupational Health and Safety Administration (OSHA) and the National Institute for Occupational Safety and

<sup>1</sup> More information on these and other adverse effects of noise may be found in *Guidelines for Community Noise*, eds B Berglund, T Lindvall, and D Schwela, World Health Organization, Geneva, Switzerland, 1999. (<https://www.who.int/publications/i/item/a68672>)





Letter 1 cont.

WILSON IHRIG  
Slatten Ranch Townhomes  
Comments on Noise Analysis

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cont.

Health (NIOSH) promote standards and regulations to protect the hearing of people exposed to high levels of industrial noise.

**Speech Interference.** Another common problem associated with noise is speech interference. In addition to the obvious issues that may arise from misunderstandings, speech interference also leads to problems with concentration fatigue, irritation, decreased working capacity, and automatic stress reactions. For complete speech intelligibility, the sound level of the speech should be 15 to 18 dBA higher than the background noise. Typical indoor speech levels are 45 to 50 dBA at 1 meter, so any noise above 30 dBA begins to interfere with speech intelligibility. The common reaction to higher background noise levels is to raise one's voice. If this is required persistently for long periods of time, stress reactions and irritation will likely result.

**Sleep Disturbance.** Noise can disturb sleep by making it more difficult to fall asleep, by waking someone after they are asleep, or by altering their sleep stage, e.g., reducing the amount of rapid eye movement (REM) sleep. Noise exposure for people who are sleeping has also been linked to increased blood pressure, increased heart rate, increase in body movements, and other physiological effects. Not surprisingly, people whose sleep is disturbed by noise often experience secondary effects such as increased fatigue, depressed mood, and decreased work performance.

**Cardiovascular and Physiological Effects.** Human's bodily reactions to noise are rooted in the "fight or flight" response that evolved when many noises signaled imminent danger. These include increased blood pressure, elevated heart rate, and vasoconstriction. Prolonged exposure to acute noises can result in permanent effects such as hypertension and heart disease.

**Impaired Cognitive Performance.** Studies have established that noise exposure impairs people's abilities to perform complex tasks (tasks that require attention to detail or analytical processes) and it makes reading, paying attention, solving problems, and memorizing more difficult. This is why there are standards for classroom background noise levels and why offices and libraries are designed to provide quiet work environments.

### Introduction

Under Section 15183 of the California Code, a project that is consistent with development density established by a General Plan for which an EIR has been certified is exempt from CEQA review except for project-specific impacts peculiar to the project or project site. Per 15183(f), an impact is not considered peculiar if uniformly applied development standards or policies have been previously adopted by the City with a finding that the development standards or policies would substantially mitigate the impact when applied to future projects, unless substantial new information shows otherwise.

According to the Consistency Memorandum, the City considered uniformly applied development standards and policies in the Draft Environmental Impact Report entitled Antioch Housing, Environmental Hazards, and Environmental Justice (EJ) Elements (Housing Element DEIR)<sup>2</sup>. None of these policies is mentioned in the Consistency Memorandum, and there is no evidence that they would mitigate the Project's noise impacts in any event. The Housing Element DEIR's operational

<sup>2</sup> [https://www.antiochca.gov/fc/community-development/planning/housing-element/DHEEIR-DEIR\\_22\\_0902.pdf](https://www.antiochca.gov/fc/community-development/planning/housing-element/DHEEIR-DEIR_22_0902.pdf)





Letter 1 cont.

WILSON IHRIG  
Slatten Ranch Townhomes  
Comments on Noise Analysis

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cont.

noise section states “General Plan Policy 11.8.2 (f) requires a detailed noise attenuation study to be prepared by a qualified acoustical engineer to determine appropriate mitigation and ways to incorporate such mitigation into project design and implementation.” And that “compliance with Code of Ordinance 9-5.1901 (A) and General Plan Policy 11.8.2 (f) would ensure that future development under the Project would not result in a substantial temporary or permanent increase in ambient noise levels from stationary sources, and this impact would be less than significant” (Housing Element DEIR page IV.L-13).

Similarly, for construction noise, General Plan Policy 11.8.2 “requires development adjacent to occupied noise sensitive land uses to implement a construction-related noise mitigation plan and requires that all construction equipment utilize noise reduction features” Additionally, the construction-related noise mitigation plan should “depict the location of construction equipment and how the noise from this equipment will be mitigated during construction through the use of noise reduction methods” that are listed in General Plan Policy 11.8.2 (m) (DEIR page IV.L-12).

General Plan 11.8.2 (f) and (m) proceed to list several design guidelines that reduce noise. None of these are guaranteed to reduce noise, depending on the unique characteristics of each site. In Section m, mufflers are already included in construction noise source models of the cited FTA database, which takes its source values from measurements of modern equipment already equipped with mufflers. Nighttime construction restrictions do not mitigate daytime noise levels. Strategic staging will reduce the length of unnecessary noise impacts, but will not mitigate the worst-case construction noise scenarios when necessary activities occur adjacent to sensitive uses. Similarly for section f, most of these best practices do not mitigate worst-case noise and are already included in modeling assumptions. Strategic project design and orientation will reduce some potential impacts. However, this does not preclude there being operational noise impacts, either due to the constraints set by the geometry of each individual site plan, or project orientation that was set by considerations other than efficiency of reducing on-site noise.

The Housing Element DEIR establishes that the general plan will reduce noise to less than significant if followed, but the same DEIR cites the general plan requirements that projects which can result in the “development of proposed uses could result in a significant increase in noise a detailed noise attenuation study to be prepared by a qualified acoustical engineer to determine appropriate mitigation and ways to incorporate such mitigation into project design and implementation” (DEIR page IV.L-13). Those steps have not been taken here, and we believe that this project has the potential to result in a significant increase in noise and vibration, and thus a detailed analysis is required, as detailed in this letter.

**Construction Noise Impacts are Potentially Significant.**

To estimate construction noise, the Federal Highway Administration’s Roadway Construction Noise Model (RCNM)<sup>3</sup> was used for this analysis. Typically, multiple pieces of equipment are used in a construction noise analysis, based on a realistic estimation of a construction environment where multiple activities occur simultaneously. Up to three pieces of equipment were modeled at once as a conservative estimate, based on typical construction procedures and timelines. The one exception to this was pile driving, which is a typically more intense procedure than most construction methods.

<sup>3</sup> [https://www.fhwa.dot.gov/ENVIRONMENT/noise/construction\\_noise/rcnm/rcnmcover.cfm](https://www.fhwa.dot.gov/ENVIRONMENT/noise/construction_noise/rcnm/rcnmcover.cfm)





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Program default usage factors, or the percentage of time the equipment generally operates, were used for all pieces of analyzed equipment. Source levels typically used in a construction noise analysis are shown in Table 1.

Table 1: Typical Construction Equipment Noise Levels

Equipment	Lmax Sound Level at 50 feet (dBA)	Utilization %
Backhoe	77.6	40%
Compactor (ground)	80.0	20%
Compressor (air)	78.0	40%
Concrete Mixer Truck	79.0	40%
Concrete Pump Truck	81.0	20%
Concrete Saw	89.6	20%
Crane	81.0	16%
Dozer	81.7	40%
Excavator	80.7	40%
Forklift	75.0	10%
Pneumatic Tools	85.0	50%
Generator	81.0	50%
Mounted Impact Hammer (hoe ram)	90.0	20%
Impact Pile Driver	95.0	20%
Front End Loader	79.0	40%
Paver	77.0	50%
Roller	80.0	20%
Tractor	84.0	40%
Welder / Torch	73.0	40%

Source: RCNM 1.1

The results of this analysis at the closest sensitive receiver, which are the townhomes approximately 90 feet northeast of the project boundary<sup>4</sup>, are shown in Table 2.

Table 2: Modeled Noise Levels from the Proposed Project and Nearest Sensitive Receiver

Noise Source(s)	Modeled Noise Level (dBA)
Impact Pile Driver Only	83
Impact Pile Driver + Pneumatic Tools + Concrete Saw	85
Concrete Saw + Pneumatic Tools	80

City of Antioch General Plan<sup>5</sup> section 11.6.2i-n addresses construction noise. It limits construction hours to 7am to 7pm Monday through Saturday. However, during daytime hours, no limits are set.

<sup>4</sup> This is measured via google earth to the project site

<sup>5</sup> [https://www.antiochca.gov/fc/community-development/planning/Antioch\\_Adopted\\_General\\_Plan.pdf](https://www.antiochca.gov/fc/community-development/planning/Antioch_Adopted_General_Plan.pdf)





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cont.

Even if the city does not set its own construction noise daytime limits, the IS should set thresholds based on other similar documents. For example, the County of Los Angeles code has a construction noise limit of 75 dBA<sup>6</sup>. Certainly, that has no jurisdiction for this project, but other government agencies and other municipalities do have daytime construction noise limits. It is the responsibility of the project applicant to find an applicable guideline to use and determine if noise levels will create an adverse impact on the community. If the applicant finds, chooses and properly cites another threshold that they feel is more appropriate, it is within their right to do so. Many such thresholds are based on ambient noise levels, which are not present here. Either way, the IS must be revised to include such a threshold to evaluate, identify, and potentially mitigate construction noise impacts.

All three modeled scenarios yield noise levels that match or exceed the 75 dBA construction noise guideline. As it currently stands, this is an exceedance of the recommended construction noise threshold which would require mitigation, such as a temporary soundwall. A study should be developed, consistent with the General Plan Policy 11.8.2, that shows how implementation of recommended barriers reduce noise levels below significance limits.

**Operational Noise Impacts are Potentially Significant.**

Operational noise from the project may represent long-term changes in the surrounding noise environment for nearby receptors. Typically, operational noise sources can include rooftop mechanical units and noise from outdoor spaces and balconies. In order to model the noise, the following assumptions were used:

- Mechanical equipment, such as HVAC units, can generate sound power levels as high as 91 dBA<sup>7</sup>.
- The HVAC units would be located on building rooftops and are 10 feet behind the edge of the building.
- The distance from the new buildings to the residence to the southeast is 180 feet.
- Three units were assumed, 10 feet apart
- 4 dB of reduction was assumed from a rooftop parapet
- HVAC systems may run constantly, especially in the City of Antioch where summer average heat can reach as high as 90 degrees Fahrenheit<sup>8</sup>.

Based on the above assumptions, the potential nighttime operational noise levels are shown in Table 3. No rooftop amenity space was indicated in the Initial Study, so it was not studied. If more detailed plans show balconies or rooftop amenity space, those should be analyzed as well.

<sup>6</sup> [https://codelibrary.amlegal.com/codes/los\\_angeles/latest/lamc/0-0-0-193925](https://codelibrary.amlegal.com/codes/los_angeles/latest/lamc/0-0-0-193925)

<sup>7</sup> [https://www.americanstandardair.com/content/dam/Trane/en-engineer/products/rooftopunits/Voyager/Voyager%20III/RT-PRC022-E4\\_0513.pdf](https://www.americanstandardair.com/content/dam/Trane/en-engineer/products/rooftopunits/Voyager/Voyager%20III/RT-PRC022-E4_0513.pdf), page 23

<sup>8</sup> <https://weatherspark.com/y/1111/Average-Weather-in-Antioch-California-United-States-Year-Round>





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Table 3: Predicted Project Operational Noise Levels

Receiver	Modeled Noise Level (dBA)	Noise Criteria	Exceedance
Townhomes to the Northeast	53	n/a <sup>a</sup>	Unknown
<sup>a</sup> Without measured ambient levels, increases over the ambient are impossible to determine.			

Since there is no ambient measurement, there is no criteria, because nighttime criteria levels are typically based on ambient levels. Suburban areas at night can be as quiet as 40 dBA<sup>9</sup>. Based on the housing element DEIR, a significance threshold is used in the city of 3 dBA over ambient limits, of which this scenario exceeds. Due to the proximity of highway SR-4, ambient levels may be higher than this, but this is why it is important to measure ambient noise levels to establish a baseline. The available Project plans are unclear as to the placement of HVAC units. More developed plans may show an HVAC arrangement that is less intrusive, or ambient measurements may present a higher criterion. But the potential for significant impacts do exist, and more thorough study is needed.

The project has the potential to result in significant operational noise levels, which should be considered in a noise study, as required by General Plan Policy 11.8.2. This noise study should determine potential mitigation measures, such as shielding of the HVAC units' noise emissions to the southeast, to reduce community exposure to excessive noise. The noise study must also measure ambient noise levels and determine if the increase over ambient levels would be significant.

**Construction Vibration Impacts are Potentially Significant.**

The DEIR on pages IV.L-14 and IV.L-15 describes the construction vibration analysis used for housing with the city limits of Antioch, based on procedures and thresholds found in the Caltrans *Transportation and Construction Vibration Guidance Manual* and the Federal Transit Administration *Transit Noise and Vibration Impact Assessment Manual*. The DEIR states that "a buffer distance of about 115 feet" is required "to protect vibration sensitive structures (below the 0.12 in/sec criterion in Table IV.L-4)." The closest structure for this project is 90 feet away. The DEIR continues, "if sensitive receptors are located within these buffer distances, future developments under the Project should prepare a screening level vibration analysis for City review in accordance with General Plan Policy 11.8.2 (k)." This was not completed for this project as required by the City and should be part of a noise study associated with this project. The DEIR continues, stating that if the "screening level analysis shows that construction has the potential to result in damage to structures" then a "detailed vibration impact assessment must be prepared by a structural engineer or other appropriate professional to determine appropriate design means and methods of construction to avoid the potential damage" (DEIR pages IV.L-14 and IV.L-1). This is vital to study, as a potential impact would not just impact people's day to day lives, but could result in damage to nearby housing, and thus should be studied more thoroughly.

<sup>9</sup> <https://ehs.yale.edu/sites/default/files/files/decibel-level-chart.pdf>





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cont.

**Baseline Noise not Established.**

CEQA requires evaluation of whether a project would cause a “substantial temporary or permanent increase in ambient noise levels.” Without knowing how loud the environment is, it is impossible to determine if the new project will increase noise in the surrounding community. Baseline noise measurements are the preferred way to determine background noise sources. These measurements serve as a crucial reference point for evaluating the potential noise impacts of proposed projects or activities. Without establishing the baseline noise conditions before any new development occurs, decision-makers cannot effectively determine whether the project complies with noise regulations nor identify any potential adverse effects on the surrounding environment and communities. Given the proximity to both local streets and highway SR-4 along with noise from nearby residences, as well as shielding from nearby structures, noise levels should be physically measured to be accurately determined.

The Federal Transit Administration’s 2018 Transit Noise and Vibration Impact Assessment Manual<sup>10</sup> (FTA Manual) Appendix E recommends a minimum of three one-hour Equivalent Sound Level (Leq) noise measurements (peak-hour roadway traffic, typical midday conditions, and typical nighttime conditions) to estimate the Day-Night Sound Level (Ldn) at site, which can be used to establish baseline noise conditions for the project, including the Community Noise Equivalent Level (CNEL). An EIR should be prepared with these baseline noise measurements to properly describe the noise environment.

**Conclusions**

Considering the potentially significant impacts from construction and operational noise and vibration on the surrounding community, it is imperative that an EIR be conducted to disclose and analyze these potentially significant impacts. Failure to evaluate these impacts would be a violation of CEQA’s core purpose of providing a transparent and comprehensive assessment of a project’s environmental effects.

Very truly yours,  
WILSON IHRIG



Jack Meighan  
Associate

meighan - updated wildflower noise analysis.docx

<sup>10</sup> [https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/research-innovation/118131/transit-noise-and-vibration-impact-assessment-manual-fta-report-no-0123\\_0.pdf](https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/research-innovation/118131/transit-noise-and-vibration-impact-assessment-manual-fta-report-no-0123_0.pdf)





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## JACK MEIGHAN

Associate

Jack joined Wilson Ihrig in 2021 and is an experienced acoustics engineer with expertise in projects involving rail transit systems, highways, CEQA analysis, environmental noise reduction, mechanical drawing reviews, and construction noise and vibration mitigation. He has hands-on experience with project management, including client coordination and presentations, as well as in designing, developing, and testing MATLAB

code used in acoustics applications. Additionally, his expertise includes taking field measurements, developing test plans and specifying, purchasing, setting up and repairing acoustic measurement equipment. He has experience in using Traffic Noise Model (TNM), CadnaA, EASE, Visual Basic, LabView, and CAD software.

### Education

- B.S. in Mechanical Engineering, University of Southern California, Los Angeles, CA

### Project Experience

#### ***Metro Regional Connector, Los Angeles CA***

Planned, took, and processed measurements as part of a team to determine the effectiveness of floating slab trackwork for a new subway in downtown Los Angeles that travels below the Walt Disney Concert Hall and the Colburn School of Music.

#### ***Rodeo Credit Enterprise CEQA Analysis for New Construction, Palmdale, CA***

Wrote an accepted proposal and executed it for a noise study project to determine noise mitigation requirements on a new housing development. Led all aspects of the project and managed the budget during all phases of project completion. Completed 5 separate projects of this type for this developer.

#### ***Blackhall Studios, Santa Clarita, CA***

Led the vibration measurement effort for a new soundstage directly adjacent to an existing freight and commuter rail line. Tested equipment, processed data, and analyzed results to determine the vibration propagation through the soil to the proposed soundstage locations, and was part of the team that developed mitigation techniques for the office spaces directly next to the rail line.

#### ***Octavia Residential Condos CEQA Study, San Francisco, CA***

Calculated the STC ratings for the proposed windows to meet Title 24 requirements, modeled the acoustic performance of floor and ceiling structures, researched noise codes, helped with a mechanical design review, and wrote a report summarizing the results for a new Condominium project being developed in San Francisco.

#### ***San Diego International Airport Terminal I Replacement, CA***

Conducted interior noise and vibration measurements, analyzed measurement data to help determine project criteria, modeled the existing and future terminals in CadnaA, and was part of a team that did a complete HVAC analysis of the entire terminal, as part of a CEQA analysis where a new terminal for the airport is being designed.

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cont.





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**Five Points Apartments Noise Study, Whittier, CA**

Took measurements, researched sound data and solutions, and recommended mitigation for a new apartment complex that was located next to an existing car wash, as part of a CEQA review.

**USC Ellison Vibration Survey, Los Angeles, CA**

Conducted vibration measurements as part of a survey to determine the effectiveness of vibration isolation platforms that are used to insulate cell growth in a cancer research facility. Determined the effectiveness and presented this information to the client. Researched and recommended a permanent monitoring system so the client could view data in real time.

**TEN50 Condos 'Popping' Noise Investigation, Los Angeles, CA**

Was part of a team that investigated the noise source of an unwanted popping noise in luxury condos in Downtown Los Angeles. Helped isolate the noise source location with accelerometers to determine where vibrations were occurring first and used an acoustic camera to determine where in the condo the noise was coming from.

**2000 University Project, Berkely, CA**

Wrote a construction noise monitoring plan based on environmental noise calculations, wrote a report summarizing the results, and attending a meeting with the client to discuss options.

**Bay Area Rapid Transit (BART) On-Track, CA, San Francisco Bay Area, CA\***

Day to day project manager, responsible for meetings, presentations, and coordination with the client for an ongoing noise study on the BART system. Developed MATLAB code to process measurements and determine areas where high corrugation was present, contributing to excessively high in-car noise levels. Performed noise measurements inside both the right of way and the vehicle cabin, in addition to rail corrugation measurements.

**California I-605/SR-60 Interchange Improvement, Los Angeles, CA\***

Developed a noise model of the area that predicted sound levels for abatement design, in addition to conducting noise measurements and analysis. Led the Team in use of the FHWA Traffic Noise Model Software for the project, involving three major highways and two busy interchanges extending over 17 miles in southern California.

**Sound Transit On-Track, Seattle, WA\***

Took measurements, fixed equipment, and developed software in MATLAB to process Corrugation Analysis Trolley measurements as part of an ongoing noise study on the Sound Transit Link system. Tested vibration data to determine the best measurement and processing techniques to store the data in an online database for in-car measurements.

**LA Metro CRRC Railcar Testing, Los Angeles, CA\***

Led the effort to plan the measurements, determine measurement locations and finalize the test plan. Formulated a method to capture speed data directly from legacy train vehicles. Executed noise and vibration specification measurements for new rail cars delivered by CRRC.

**City of Los Angeles, Pershing Square Station Rehabilitation Noise Monitoring, CA\***

Built noise models, wrote a construction noise plan, and assisted in on-site construction noise issues as they arose for a renovation of the Pershing Square metro station in downtown Los

\* Work done prior to working for Wilson Ihrig





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Angeles. Trained construction personnel in techniques for noise reduction and how to conduct noise monitoring measurements to meet project specifications.

***City of Orange Metrolink Parking Garage Construction Monitoring, CA\****

Wrote an adaptive management vibration monitoring plan, set up equipment to monitor live vibration levels, and generated weekly reports as part of an effort to build a new parking garage. Designed, planned, and completed measurements to predict and mitigate pile driving construction impacts at three historic building locations adjacent to the construction site. Coordinated with the client whenever an on-site problem arose.

***LA Metro Westside Subway Construction, Los Angeles, CA\****

Planned, organized, and processed noise measurements for the Purple Line extension construction. Implemented both long term microphones to measure noise levels and accelerometers to measure vibration levels in existing subway tunnels. Oversaw noise monitoring at sensitive construction sites for the project and worked with the contractor to find ways to reduce construction noise levels by approximately 10dB.

***Montreal Réseau Express Métropolitain, Canada\****

Conducted vibration propagation measurements used to create models to predict operational vibration levels for an under-construction transit line. Managed equipment, solved problems in the field, and wrote parts of the report summarizing the findings of the acoustic study.

***NHCRP Barrier\****

Took on-highway measurements and wrote, designed, developed, and tested MATLAB code to identify specific spectrograms to use for analyses for a project evaluating barrier reflected highway traffic noise differences in the presence of a single absorptive or reflective noise barrier.

***Siemens Railcar Testing for Sound Transit, Seattle, WA\****

Measured in-car noise and vibration for new rail cars delivered by Siemens. Developed new internal techniques for measurements based on the written specifications. Contributed to the team that helped identify issues that new cars had in meeting the Sound Transit specifications for noise and vibration. Participated in developing the test plan and specified then acquired new equipment for the measurement.

***Toronto/Ontario Eglinton Crosstown Light Rail, Final Design, Canada\****

Assisted in vibration propagation measurements, analysis, and recommendations for mitigation for a 12-mile light-rail line both on and under Eglinton Avenue. Set up and ran equipment for at-grade measurements with an impact hammer for underground measurements with an impact load cell that was used during pre-construction borehole drilling.

*\* Work done prior to working for Wilson Ihrig*





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**LETTER 1: CONTRA COSTA RESIDENTS FOR RESPONSIBLE  
DEVELOPMENT (ADAMS BROADWELL JOSEPH &  
CARDOZO)**

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**Response to Comment 1-1**

The comment provides background information and is introductory to the remainder of the comment letter. Responses to the specific concerns summarized within the comment are addressed in detail below.

**Response to Comment 1-2**

The Housing Element EIR is a program-level EIR. According to CEQA Guidelines Section 15168(a), a program-level EIR is an EIR that may be prepared on a series of actions that could be characterized as one large project and are related either: 1) geographically; 2) as logical parts in the chain of contemplated actions; 3) in connection with issuance of rules, regulations, plans, or other general criteria to govern the conduct of a continuing program; or 4) as individual activities carried out under the same authorizing statutory or regulatory authority and having generally similar environmental effects which can be mitigated in similar ways.

The programmatic level of CEQA analysis was appropriate based on criterion 1 through 3 above, as well as due to the fact that site-specific details, including development proposals or technical studies, were not available. Nonetheless, the Housing Element EIR does provide a project-level analysis where possible, including for issues related to project-level construction, operational emissions, and VMT.

CEQA Guidelines Section 15183 does not require a “project-level” analysis but, rather, an examination of whether a proposed project would result in environmental effects peculiar to the project or project site from what was addressed in the Housing Element EIR. Because the Housing Element EIR addresses air quality, health risks, transportation, and noise impacts, the 15183 Consistency Memorandum appropriately addresses whether the proposed project would result in any peculiar effects related to such. In addition, contrary to the comment, the 15183 Consistency Memorandum does include specific sections discussing potential impacts related to air quality, including health risks, and transportation on pages 12 and 13, respectively.

**Response to Comment 1-3**

Contrary to the comment, the 15183 Consistency Memorandum clearly describes the applicable policies and mitigation measures from the Housing Element EIR. For example, as discussed on page 11, the City would require compliance with Mitigation Measure AIR-3a of the Housing Element EIR as a Condition of Approval, which would ensure impacts related to exposing sensitive receptors to substantial pollutant concentrations would not be peculiar. In addition, page 13 of the 15183 Consistency Memorandum includes the following:

According to the Housing Element EIR, implementation of General Plan policies, such as Policy 11.7.2(c) and 11.7.2(d) (which were updated as part of the Housing Element Update), would comply with Options A and B of the BAAQMD’s recommended plan-level thresholds of significance and ensure that future development would not result in a cumulatively considerable contribution to global climate change. Finally, implementation of existing General Plan Policies 7.4.2, 10.6.2, 10.7.2, 10.8.2, and Housing Element Policy 4.1 would help to reduce GHG emissions from transportation, energy use, and water use.





### **Response to Comment 1-4**

In response to the comment, page 12 of the 15183 Consistency Memorandum is hereby revised as follows:

#### **Transportation**

Pursuant to Mitigation Measure TRANS-1 as set forth in the Housing Element EIR, individual housing project development proposals that do not screen out from a VMT impact analysis are required to provide a quantitative VMT analysis; however, the Housing Element EIR provides that any project that meets the screening criteria set forth in the Housing Element EIR ~~is exempt from CEQA~~ is not required to conduct a VMT analysis pursuant to Mitigation Measure TRANS-1. The screening criteria set forth in the Housing Element EIR include, but are not limited to, proximity to a major transit stop and projects located in low VMT areas. As shown in Figure IV.B-6 of the Housing Element EIR, the project site is located within a Transit Priority Area, defined by the Housing Element EIR as the 0.5-mile (walking radius) surrounding the City's Bay Area Rapid Transit (BART) and Amtrak stations. In addition, the project site is identified as Sites 158 through 161 by the Housing Element, which are shown in Figure IV.B-6 of the Housing Element EIR as having VMT below the significance threshold under the 2020 and 2040 scenarios, which include buildout of the City in addition to the development anticipated by the Housing Element through the horizon years 2020 and 2040. Based on such, the proposed project would be considered to screen out from a VMT impact analysis and would not be anticipated to result in a significant VMT impact. As demonstrated through this 15183 Consistency Memorandum, the proposed project would not result in a significant impact that is peculiar to the project or project site, a significant effect that was not previously identified in the Housing Element EIR, or a substantially more severe significant effect related to transportation beyond what was identified in the Housing Element EIR. Therefore, pursuant to Section 15183 of the CEQA Guidelines, the proposed project qualifies for exemption from further environmental review under CEQA. Because the proposed project would meet the screening criteria set forth in the Housing Element EIR and not be anticipated to result in a significant VMT impact, be considered exempt, Mitigation Measure TRANS-1 is not applicable. Thus, the proposed project would not result in a significant impact that is peculiar to the project or the project site, was not identified as a significant effect in the Housing Element EIR, and would not result in a more severe adverse impact than the significant effects previously identified within the Housing Element EIR.

The above changes do not modify the overall conclusion of the 15183 Consistency Memorandum that, pursuant to CEQA Guidelines Section 15183, additional environmental review under CEQA is not required for the proposed project.

### **Response to Comment 1-5**

Please see Response to Comment 1-2 above.

### **Response to Comment 1-6**

Mitigation Measures AIR-1 and AIR-2 are not required for all future housing proposals, but, rather, only for projects that exceed the Bay Area Air District's (BAAD) screening criteria. It should be noted that BAAD was formerly known as the Bay Area Air Quality Management District (BAAQMD) and is referred to as such in both the Housing Element EIR and the 15183 Consistency Memorandum. As discussed on page IV.C-14 and IV.C-15 of the Housing Element EIR, "...BAAQMD recommends using their project-level thresholds of significance to identify levels at which individual projects could cause significant air quality impacts related to emissions of ozone precursors (ROG and NO<sub>x</sub>), PM<sub>10</sub>, PM<sub>2.5</sub>, and TACs." The screening criteria set forth by BAAD include the number of units for typical residential development types that, if exceeded,





would result in emissions that would exceed the BAAD's project-level thresholds of significance. Because, as explained in the 15183 Consistency Memorandum, the project is below the screening criteria, the project's emissions are expected to be below the applicable threshold of significance and, thus, implementation of Mitigation Measures AIR-1 and AIR-2 of the Housing Element EIR is not required.

Mitigation Measure AIR-3(b) is specific to residential emergency generators, as, according to the Housing Element EIR, generators could substantially contribute to the existing poor air quality in the City and expose sensitive receptors to substantial pollutant concentrations. As stated in the 15183 Consistency Memorandum, the proposed project would not include emergency generators and, thus, is not subject to Mitigation Measure AIR-3(b).

Mitigation Measure AIR-3(a) is only required for projects that involve a construction duration greater than six months that are located in a BAAD Planning Healthy Place Map area. The mapped areas are described by the Housing Element EIR on page IV.C-21 as areas with elevated levels of toxic air contaminants (TACs) and/or particulate matter pollution. As stated in the 15183 Consistency Memorandum, a portion of the project site is located within a BAAD Planning Healthy Place Map area defined as needing "Best Practices," and project construction is anticipated to take longer than six months. Thus, the proposed project would be subject to Mitigation Measure AIR-3a, which would be included as a Condition of Approval for the project imposed by the City. AIR-3a, as stated in the 15183 Consistency Memorandum and presented on page IV.C-22 of the Housing Element EIR, requires that one of two measures be implemented, as well as the preparation of a Construction Emissions Minimization Plan. The Housing Element EIR specifically concluded on page IV.C-23 that "implementation of Mitigation Measures AIR-3a and AIR-3b would ensure that impacts from future residential development under the Project would be less than significant related to plan- and project-level generation of TACs and PM<sub>2.5</sub>." Mitigation Measure AIR-3a would be considered a uniformly applicable development policy or standard identified by the Housing Element EIR to substantially mitigate impacts associated with buildout of the Housing Element. With compliance with Mitigation Measure AIR-3a, pursuant to CEQA Guidelines Section 15183(f), the project's impacts associated with exposure of sensitive receptors to substantial pollutant concentrations would not be considered peculiar. Because impacts related to exposure of sensitive receptors to substantial pollutant concentrations are not peculiar to the project site or project, in accordance with Section 15183(c) of the CEQA Guidelines, the proposed project qualifies for exemption from further environmental review under CEQA. The comment does not provide any substantial new information to suggest that, pursuant to CEQA Guidelines Section 15183(b)(4), the project's impacts related to exposure of sensitive receptors to substantial pollutant concentrations would be more severe than discussed in the Housing Element EIR.

Page 11 of the 15183 Consistency Memorandum also notes: "In addition, the proposed project would be subject to the same regulations governing criteria air pollutants and emissions as were identified for the development projects evaluated in the Housing Element EIR." Such regulations include the BAAD's dust control measures to be implemented during construction, as enforced by General Plan Policy 10.6.2(a), and measures sourced from the BAAD 2017 Clean Air Plan. As noted on page IV.C-18 of the Housing Element EIR, BAAD considers implementation of best management practices (BMPs) to control dust during construction sufficient to reduce potential impacts to a less-than-significant level. Thus, the 15183 Consistency Memorandum provides sufficient supporting evidence that the project's compliance with all applicable regulations would ensure that the proposed project would not result in a significant impact that is peculiar to the project or the project site and would not result in a more severe adverse impact than the significant effects previously identified within the Housing Element EIR.





### **Response to Comment 1-7**

Please see Response to Comment 1-6.

### **Response to Comment 1-8**

Please see Response to Comment 1-2. The proposed project is consistent with the General Plan land use and zoning designations for the project site and, thus, was considered as part of the Housing Element EIR's analysis. The Housing Element EIR concluded that compliance with General Plan Policy 11.8.2 would be sufficient to reduce potentially significant noise impacts to less-than-significant levels. Thus, General Plan Policy 11.8.2 is a uniformly applicable development policy identified by the Housing Element EIR as being sufficient to substantially mitigate impacts associated with buildout of the Housing Element related to noise. The proposed project would be subject to all measures and standards within Policy 11.8.2. For example, as discussed in further detail in Response to Comment 1-11, construction noise would be mitigated by Policy 11.8.2's requirements related to limited hours, noise reduction features, and the preparation and implementation of a construction-related noise mitigation plan. A noise attenuation study is not required, as the City has determined that the project would not result in a significant increase in noise and because the project is not located in an area exceeding the General Plan noise standards. Therefore, consistent with the conclusions of the 15183 Consistency Memorandum and pursuant to CEQA Guidelines Section 15183(f), noise associated with the proposed project is not peculiar. With respect to specific aspects of noise impacts, such as operational noise generation, construction noise and vibration, please see Responses to Comments 1-10 through 1-12 below.

### **Response to Comment 1-9**

Please see Response to Comment 1-8. With respect to specific aspects of noise impacts, such as operational noise generation, construction noise and vibration, please see Responses to Comments 1-10 through 1-12 below.

### **Response to Comment 1-10**

The proposed project is consistent with the General Plan land use and zoning designations for the project site and, thus, was considered as part of the Housing Element EIR's analysis. Residential uses are not typically considered significant noise-generating uses. The proposed project does not include any operational noise-generating uses outside of typical residential uses. Typical residential uses include heating, ventilation, and air conditioning (HVAC) units. The Housing Element EIR identifies that, according to the City's General Plan EIR, areas within 50 feet of studied roadways could be exposed to noise levels of approximately 60 dBA CNEL. Because Hillcrest Avenue is located adjacent to the project site, a noise level associated with potential HVAC units on the project site of 53 dB, as cited by the comment, would reasonably not be expected to result in a substantial increase in noise levels in the project vicinity. In addition, any proposed HVAC units would be shielded to reduce noise, as required by the City's adopted Multi-family Residential Objective Design Standards.

The proposed project would be subject to the aforementioned standard, as well as the requirements within General Plan Policy 11.8.2. General Plan Policy 11.8.2 is a uniformly applicable development policy identified by the Housing Element EIR as being sufficient to substantially mitigate impacts associated with buildout of the Housing Element related to noise. For example, as discussed in further detail in Response to Comment 1-11, construction noise would be mitigated by Policy 11.8.2's requirements related to limited hours, noise reduction features, and the preparation of a construction-related noise mitigation plan. Therefore, consistent





with the conclusions of the 15183 Consistency Memorandum and pursuant to CEQA Guidelines Section 15183(f), noise associated with the proposed project is not peculiar.

### **Response to Comment 1-11**

The Housing Element EIR concluded that implementation of the City of Antioch Code of Ordinances and General Plan policies would ensure that the construction of individual residential developments under the Housing Element would not result in a substantial temporary increase in ambient noise levels related to construction activities, and impacts were determined to be less than significant. Specifically, as noted on pages IV.L-11 and IV.L-12 of the Housing Element EIR and pursuant to General Plan Policy 11.8.2, temporary construction periods would be subject to various measures, including the following:

- Construction activities shall be regulated to hours of operation within Section 5-17.05(B) of the City of Antioch Code of Ordinances in order to avoid or mitigate noise impacts on adjacent noise-sensitive land uses;
- Proposed development adjacent to occupied noise sensitive land uses shall implement a construction-related noise mitigation plan that depicts the location of construction equipment storage and maintenance areas, and documents methods to be employed to minimize noise impacts on adjacent noise sensitive land uses; and
- All construction equipment shall use noise reduction features (e.g., mufflers and engine shrouds) that are at least as effective as those originally installed by the manufacturer.

With respect to the construction-related noise mitigation plan, as noted within General Plan Policy 11.8.2(o), the City shall condition approval of subdivisions located adjacent to any developed/occupied noise-sensitive land uses to submit such a plan for review and approval prior to the issuance of any grading plans. The plan would depict the location of construction equipment and how such equipment noise would be mitigated, including through temporary noise-attenuation fences, equipment mufflers, staging area locations, and limiting construction-related activities (including truck deliveries) to the hours of 7:00 AM and 7:00 PM, Monday through Saturday. Lastly, the construction-related noise mitigation plan shall incorporate any other restrictions imposed by the City. General Plan Policy 11.8.2(o) is a uniformly applicable development policy identified by the Housing Element EIR as being sufficient to substantially mitigate impacts associated with buildout of the Housing Element related to construction noise. The comment does not provide any substantial new information that shows that compliance with General Plan Policy 11.8.2 would not substantially mitigate construction noise. General Plan Policy 11.8.2(o) specifically states, “The construction contractor shall use temporary noise-attenuation fences, where feasible, to reduce construction noise impacts on adjacent noise sensitive land uses.” The proposed project would be subject to such measures, which the Housing Element EIR concluded would be sufficient to reduce project construction noise to a less-than-significant level. Therefore, pursuant to CEQA Guidelines Section 15183(f), construction noise associated with the proposed project is not peculiar and the 15183 Consistency Memorandum’s conclusion is appropriate.

It should be noted that Comment 1-14 uses the Los Angeles County construction noise limit threshold of 75 dBA, which is inapplicable to the proposed project (located in the City of Antioch within Contra Costa County). The City does not have an established daytime noise level threshold for construction, but does limit the hours of construction within Section 5-17.05(B) of the Antioch Code of Ordinances, as enforced by General Plan Policy 11.8.2.





Because the proposed project is consistent with the type of on-site development anticipated by the Housing Element EIR, which concluded that compliance with the standards contained within the City of Antioch Code of Ordinances and with the applicable General Plan policies would be sufficient to ensure potential impacts related to noise would be less than significant, the 15183 Consistency Memorandum's conclusion that noise associated with the proposed project would not be peculiar remains adequate.

**Response to Comment 1-12**

As discussed on page IV.L-15 of the Housing Element EIR, the requirement for a screening level vibration analysis is based specifically on the use of impact pile drivers during construction. The use of pile drivers is generally required for the construction of deep foundations, bridges, docks, or skyscrapers, or excavations associated with temporary retaining walls. Pile drivers are not typically used for standard residential developments; therefore, the requirements of Policy 11.8.2(k) would not be required for the proposed project. Thus, consistent with the conclusions within the 15183 Consistency Memorandum, the proposed project would not result in any peculiar impacts related to noise.

**Response to Comment 1-13**

Please see Responses to Comments 1-2 through 1-12 above.

**Response to Comment 1-14**

Please see Responses to Comments 1-8 through 1-12 above.





## ATTACHMENT "F"

### CITY OF ANTIOCH PLANNING COMMISSION REGULAR MEETING

Regular Meeting  
6:30 p.m.

July 16, 2025  
City Council Chambers

#### 1. CALL TO ORDER

Chairperson Webber called the meeting to order at 6:30 P.M. on Wednesday, July 16, 2025, in Council Chambers. He welcomed Commissioner Spijker to the Planning Commission.

Planning Manager Merideth introduced Commissioner Spijker and welcomed him to the Planning Commission.

#### 2. ROLL CALL

Present: Commissioners Spijker, Martin, Riley, Perez, Vice Chairperson Jones and Chairperson Webber  
Absent: Commissioner Suman

Staff: Interim City Attorney, Derek Cole  
Planning Manager, Zoe Merideth  
Minutes Clerk, Kitty Eiden

#### 3. PLEDGE OF ALLEGIANCE

Chairperson Webber led the Pledge of Allegiance.

#### 4. PUBLIC COMMENT – None

#### 5. CONSENT CALENDAR

##### 5-1 Planning Commission Meeting Minutes June 4, 2025

*On motion by Commissioner Martin, seconded by Commissioner Riley the Planning Commission members present unanimously approved Item 5-1. The motion carried the following vote:*

**AYES:** Spijker, Martin, Riley, Perez, Jones and Webber  
**NOES:** None  
**ABSTAIN:** None  
**ABSENT:** Suman

#### 6. NEW PUBLIC HEARING

**6-1. Wildflower Station Townhomes 2 | TM-02, AR-23-05 | Northeast corner of Hillcrest Avenue and Wildflower Drive (APN: 052-140-013, 014, 015, and 016)** The applicant, Kathryn Watt on behalf of DeNova Homes, Inc., is seeking approval of a Vesting Tentative



Subdivision Map and Design Review for the creation of 19 residential lots and 17 common area parcels for 159 multifamily townhome units in 19 buildings on a +/- 7.93-acre site. The project includes an internal road network, open space, landscaping, stormwater facilities, and utilities to serve the project site.

Interim City Attorney Cole announced that the City and Commission had received correspondence from a law firm regarding this agenda item. He stated that staff, the consultant, and himself request additional time to review and respond to the issues raised. On their behalf, he asked the Planning Commission to continue the Public Hearing to August 20, 2025. He also noted that the applicant's general counsel did not oppose the continuance. He requested the Planning Commission allow for public comment on the agenda item as well as the recommended action to continue the Public Hearing.

Chairperson Webber opened the public comment period.

Mauricio Ulloa, Wildflower Station Place Homeowners Association (HOA), thanked DeNova for incorporating resident feedback into the revised proposal to help address ongoing challenges related to security. He confirmed that the HOA accepted the offer to contribute their fair share toward the maintenance of fencing and gates.

Chairperson Webber closed the public comment period.

***On motion by Commissioner Jones, seconded by Commissioner Martin the Planning Commission members present unanimously continued Public Hearing Item #6-1 to August 20, 2025. The motion carried the following vote:***

<b>AYES:</b>	<b><i>Spijker, Martin, Riley, Perez, Jones and Webber</i></b>
<b>NOES:</b>	<b><i>None</i></b>
<b>ABSTAIN:</b>	<b><i>None</i></b>
<b>ABSENT:</b>	<b><i>Suman</i></b>

**6-2. Slatten Ranch Town Homes | TM-01, AR-23-01 | North of Wicklow Way, East of Slatten Ranch Road, and West of Empire Avenue (APN: 056-120-098)** The applicant, Kathryn Watt on behalf of DeNova Homes, Inc., is seeking approval of a Vesting Tentative Subdivision Map and Design Review for the creation of 17 residential lots and 13 common area parcels for 129 multifamily townhome units in 17 buildings on a +/- 6.41-acre site. The project includes an internal road network, open space, landscaping, stormwater facilities, and utilities to serve the project site.

Interim City Attorney Cole announced that the City and Commission had received correspondence from a law firm regarding this agenda item. He stated that staff, the consultant, and himself requested additional time to review and respond to the issues raised. On their behalf, he asked the Planning Commission to continue the Public Hearing to August 20, 2025. He requested the Planning Commission allow for public comment on the agenda item as well as the recommended action to continue the Public Hearing.



Chairperson Webber opened and closed the public comment period with no members of the public requesting to speak.

***On motion by Commissioner Martin, seconded by Commissioner Jones the Planning Commission members present continued Public Hearing Item #6-2 to August 20, 2025. The motion carried the following vote:***

**AYES:** *Spijker, Martin, Riley, Perez, Jones and Webber*  
**NOES:** *None*  
**ABSTAIN:** *None*  
**ABSENT:** *Suman*

## **7. ORAL/WRITTEN COMMUNICATIONS**

Planning Manager Merideth reported on her attendance at a Planning Directors meeting, and she provided an update on new California Environmental Quality Act (CEQA) laws. She announced the City had received responses to their RFP for the General Plan Update and staff was in the process of evaluating and interviewing firms and anticipated bringing a contract to Council for consideration soon.

## **8. COMMITTEE REPORTS**

Commissioner Jones reported on her attendance at the TRANSPLAN Committee meeting.

## **9. NEXT MEETING: August 6, 2025**

Planning Manager Merideth announced that the August 6, 2025, Planning Commission meeting would likely be canceled, and the next Planning Commission meeting would be held on August 20, 2025.

## **10. ADJOURNMENT**

***On motion by Commissioner Martin, seconded by Commissioner Jones, the Planning Commission members present unanimously adjourned the meeting at 6:51 P.M. The motion carried the following vote:***

**AYES:** *Spijker, Martin, Riley, Perez, Jones and Webber*  
**NOES:** *None*  
**ABSTAIN:** *None*  
**ABSENT:** *Suman*

Respectfully submitted:

Kitty Eiden  
KITTY EIDEN, Minutes Clerk